

# Planning Committee Agenda



**Reigate & Banstead**  
**BOROUGH COUNCIL**  
Banstead | Horley | Redhill | Reigate

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22 October 2018

## To the Members of the PLANNING COMMITTEE

**Councillors:** S. Parnall (Chairman)

M. S. Blacker	J. M. Ellacott	J. M. Stephenson
Mrs. R. Absalom	V. H. Lewanski	C. Stevens
L. S. Ascough	S. McKenna	Ms. B. J. Thomson
R. Biggs	R. Michalowski	Mrs. R. S. Turner
Mrs. J. S. Bray	J. Paul	S. T. Walsh
G. P. Crome	M. J. Selby	C. T. H. Whinney

### Substitutes

#### Councillors:

**Conservatives:** T. Archer, M. A. Brunt, J. E. Durrant, J. S. Godden,  
Dr. L. R. Hack, A. C. J. Horwood, F. Kelly, G. J. Knight,  
G. Owen, D. T. Powell, T. Schofield and J. F. White

**Residents' Group:** R. Harper, N. D. Harrison, B. A. Stead and J. C. White

**Green Party:** H. Brown and J. C. S. Essex

For a meeting of the **PLANNING COMMITTEE** to be held on **WEDNESDAY, 31 OCTOBER 2018** at **7.30 pm** in the New Council Chamber - Town Hall, Reigate.

John Jory  
Chief Executive

**1. MINUTES**

(Pages 5 - 8)

To confirm as a correct record the Minutes of the previous meeting.

**2. APOLOGIES FOR ABSENCE**

To receive any apologies for absence.

**3. DECLARATIONS OF INTEREST**

To receive any declarations of interest.

**4. ADDENDUM TO THE AGENDA**

(To Be Tabled)

To note the addendum tabled at the meeting which provides an update on the agenda of planning applications before the Committee.

**PLANNING APPLICATIONS:**

**NOTES:**

1. The order in which the applications will be considered at the meeting may be subject to change.
2. Plans are reproduced in the agenda for reference purposes only and are not reproduced to scale. Accordingly dimensions should not be taken from these plans and the originals should be viewed for detailed information. Most drawings in the agenda have been scanned, and reproduced smaller than the original, thus affecting image quality.

To consider the following applications :

**5. 18/01637/F - THE WARWICK SCHOOL, NOKE DRIVE, REDHILL** (Pages 9 - 50)

Demolition of main teaching block and sports facilities. Construction of new main teaching block together with new sports hall, rationalisation of vehicle access, parking and on-site drop-off/pick-up areas; formal and informal play areas.



**6. 18/01180/F - FORMER PHILIPS RESEARCH LABORATORIES SOUTH SITE, CROSSOAK LANE, SALFORDS** (Pages 51 - 88)

The redevelopment of the site to include four employment buildings incorporating 5 units for open b1(b), b1(c), b2 and b8 use comprising 15,831sqm gea with associated parking and landscape planting.

**7. 18/01134/F - LAND TO REAR OF 19-29 SHELVERS WAY** (Pages 89 - 116)

Erection of 6 dwellings comprising 2 x pair of semi detached dwellings, and 2 x Detached dwellings along with access to Shelveys Way utilising the approved access from 17/00016/S73, and all associated landscaping, and ancillary work.

**8. 18/01361/F - REAR OF 86 - 90 PARTRIDGE MEAD BANSTEAD** (Pages 117 - 138)

Erection of Four Dwellings. As amended on 08/10/2018.

**9. 18/01901/F - ST MATTHEWS CHURCH, STATION ROAD, REDHILL** (Pages 139 - 150)

Proposed replacement of existing timber shed with new timber shed.

**10. 18/01818/F - THE CROFT RESIDENTIAL HOME, BUCKLAND ROAD, REIGATE** (Pages 151 - 176)

The proposed demolition of a former nursing home building and daycare outbuilding, and the erection of a single replacement building, comprising 8no. Apartments (1no. 1Bed; 3no. 2Bed and 4no.3Bed units) with 16no. Surface car parking spaces, cycle and refuse storage enclosure, with associated hard + soft landscaping enhancements. (Building reduced in size with a gross internal area of 997.03Sqm).

**11. PLANNING PERFORMANCE REPORT (Q2, 2018/19)** (Pages 177 - 184)

To inform members of the 2018/19 Q2 Development Management performance against a range of indicators.

## 12. ANY OTHER URGENT BUSINESS

To consider any item(s) which, in the opinion of the Chairman, should be considered as a matter of urgency.

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# Agenda Item 1

Planning Committee  
3 October 2018

Minutes

## **BOROUGH OF REIGATE AND BANSTEAD**

### **PLANNING COMMITTEE**

Minutes of a meeting of the Planning Committee held at the New Council Chamber - Town Hall, Reigate on 3 October 2018 at 7.30 pm.

Present: Councillors S. Parnall (Chairman), M. S. Blacker (Vice-Chair), Mrs. R. Absalom, R. Biggs, Mrs. J. S. Bray, G. P. Crome, S. McKenna, R. Michalowski, J. Paul, J. M. Stephenson, C. Stevens, Ms. B. J. Thomson, Mrs. R. S. Turner, C. T. H. Whinney, N. D. Harrison (Substitute), F. Kelly (Substitute), G. Owen (Substitute) and J. F. White (Substitute).

Also present: Councillor Dr. L. R. Hack.

#### **47. MINUTES**

**RESOLVED** that the minutes of the meeting held on 5 September 2018 be approved as a correct record and signed.

#### **48. APOLOGIES FOR ABSENCE**

Apologies for absence were received from Councillors Ascough, Ellacott (substitute: Councillor Kelly), Lewanski ( substitute: Councillor Owen), Selby (substitute: Councillor Harrison) and Walsh (substitute: Councillor J F White).

#### **49. DECLARATIONS OF INTEREST**

Councillor Blacker declared a pecuniary interest in agenda items 9 (18/01424/F) and 10 (18/01694/F) because he was the agent for the applicant in each case. Councillor Blacker left the meeting throughout the debate and vote on these items.

Councillor Paul declared a pecuniary interest in agenda item 11 (18/01721/H) because he was a close relative of the applicants. Councillor Paul left the meeting throughout the debate and vote on this item.

Councillor Kelly queried whether he should declare an interest in agenda item 9 because he had attended meetings of the Limes Action Group and supported its aims. In response to legal advice given by the planning solicitor, Councillor Kelly confirmed that he had no predetermined views on the planning application and would therefore remain in the meeting for this item, as his judgment would be based purely on the planning merits.

In respect of agenda item 10, and for the sake of full disclosure, Councillor McKenna advised that he had sought to have an oak tree in close proximity to the applicant site protected. This had no impact upon the current application, however, and he would remain in the meeting for the item.

**50. ADDENDUM TO THE AGENDA**

An addendum was tabled at the meeting, providing an update on matters arising and advising of any changes made to recommendations following publication of the agenda.

It was noted that agenda item 5 had been withdrawn.

The Chairman drew the Committee's attention to the advisory note in the addendum, about the Town and Country Planning (Pre-commencement Conditions) Regulations 2018 which came into effect on 1 October 2018.

The impact of this was that any pre-commencement conditions would now have to be agreed in writing with the applicant before being imposed upon a permission.

The Chairman clarified which applications members of the public were present for, and sought the approval of the Committee to bring these forward.

It was agreed that the applications should be considered in the following order: 6,7, 9, 12, 8, 10 and 11, although the minutes follow the agenda order for the sake of consistency.

**RESOLVED** that the addendum be noted.

**51. 17/01929/OUT: LAND TO THE NORTH OF ROCKSHAW ROAD, MERSTHAM**

It was noted that this item had been withdrawn by the applicant.

**52. 18/01313/F: REAR OF 35-49 WARREN ROAD, BANSTEAD**

The Committee considered an application for the demolition of 37 Warren Road and the construction of eight dwellings to the rear of 35-59 Warren Road.

Officers clarified that the appendices in the addendum should be referenced as follows:

Appendix A related to the scheme dismissed at appeal in 2015;  
Appendix B and the second and third plans in Appendix C showed the amended plans in relation to the current application which overcame the need to impose condition 6; and  
the first plan in Appendix C showed the refused scheme from 2017.

Following the debate on this item reasons for refusal were proposed and seconded.

**RESOLVED** that planning permission be **REFUSED** on the following grounds:

1. The proposed development would, by virtue of the height and scale of buildings and their elevated position due to the rising topography, give rise to an urbanised form of development which would be prominent within, and harmful to, the spacious character and street scene of Warren Road. The proposal would therefore be contrary to policies Ho9, Ho13 and Ho14 of the Reigate and Banstead Borough Local Plan 2005, Policy CS4 of the Reigate and Banstead Core Strategy, the Reigate and Banstead Local Distinctiveness Design Guide 2004 and the provisions of the National Planning Policy Framework in relation to 'Achieving well-designed places'.

**53. 18/00640/F: MOUNT PLEASANT, COPPICE LANE, REIGATE**

The Committee considered an application for demolition of the existing residential dwelling and the erection of three family dwellings together with associated hard and soft landscaping measures.

**RESOLVED** that planning permission be **GRANTED with conditions** as set out in the report and addendum.

**54. 18/01414/F: ROMANS INTERNATIONAL LTD, BRIGHTON ROAD, BANSTEAD**

The Committee considered an application for the erection of a row of eight garages to the rear of the site.

**RESOLVED** that planning permission be **GRANTED with conditions** as set out in the report and addendum.

**55. 18/01424/F: THE LIMES PUBLIC HOUSE, 58 ALBURY ROAD, MERSTHAM**

The Committee considered an application for the demolition of the existing public house and the construction of a new public house with flats over, together with associated parking on part of the site.

**RESOLVED** that planning permission be **GRANTED with conditions** as set out in the report and addendum.

Note: Councillor Blacker left the meeting throughout the debate and vote on this item.

**56. 18/01694/HHOLD: 63 BLETCHINGLEY ROAD, MERSTHAM**

The Committee considered an application for a vehicle crossover.

**RESOLVED** that planning permission be **GRANTED with conditions** as set out in the report.

Note: Councillor Blacker left the meeting throughout the debate and vote on this application.

**57. 18/01721/HHOLD: 48 CHAPEL ROAD, TADWORTH**

The Committee considered an application for a single storey rear extension with a depth of 4.5 metres.

**RESOLVED** that planning permission be **GRANTED with conditions** as set out in the report.

Note: Councillor Paul left the meeting throughout the debate and vote on this application.

**58. 18/01813/ADV: LAND PARCEL AT WINKWORTH ROAD, BANSTEAD**

The Committee considered an application for a village sign in Banstead.

**RESOLVED** that planning permission be **GRANTED with conditions** as set out in the report.

**59. ANY OTHER URGENT BUSINESS**


None.

The Meeting closed at 9.01 pm

# Agenda Item 5

Planning Committee  
31<sup>st</sup> October 2018

Agenda Item: 5  
18/01637/F

 <b>Reigate &amp; Banstead</b> BOROUGH COUNCIL Banstead   Horley   Redhill   Reigate	<b>TO:</b>	PLANNING COMMITTEE
	<b>DATE:</b>	31 <sup>st</sup> October 2018
	<b>REPORT OF:</b>	HEAD OF PLANNING
	<b>AUTHOR:</b>	Billy Clements
	<b>TELEPHONE:</b>	01737 276087
	<b>EMAIL:</b>	billy.clements@reigate-banstead.gov.uk
<b>AGENDA ITEM:</b>	5	<b>WARD:</b> Redhill East

<b>APPLICATION NUMBER:</b>	18/01637/F	<b>VALID:</b>	11 December 2017
<b>APPLICANT:</b>	Kier Construction	<b>AGENT:</b>	ECE Planning Ltd
<b>LOCATION:</b>	<b>THE WARWICK SCHOOL, NOKE DRIVE, REDHILL</b>		
<b>DESCRIPTION:</b>	<b>Demolition of main teaching block and sports facilities. Construction of new main teaching block together with new sports hall, rationalisation of vehicle access, parking and on-site drop-off/pick-up areas; formal and informal play areas.</b>		
<b>All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail.</b>			

## SUMMARY

This application seeks full planning permission for the demolition of an existing teaching block and sports hall and the erection of a new replacement main teaching block and sport hall. In addition, the proposals include creation of new hard and soft landscaped play areas and reconfiguration of internal parking and vehicle circulation. The works would be conducted in a phased manner, to prevent the need for temporary accommodation and to minimise disruption to the curriculum.

The site is within the urban area and, like a number of schools within the Borough, is designated as Urban Open Land. However, as the facilities would be constructed largely on the footprint of existing buildings or areas of hardstanding, it is not felt that they would erode the overall openness of the site. Furthermore, the proposals would deliver a significant enhancement in the quality and functionality of the school's facilities, thus consistent with the aims and exception in Policy Pc6. National planning policy also advises that "great weight" should be given to the need to alter and improve schools within planning decisions.

The buildings would consist of two blocks, part two, part three storeys, linked by a glazed first floor walkway. Whilst they would be relatively large, scale and massing is not considered to be harmful given the siting of the buildings and changing levels which means they would not appear unduly dominant within surrounding street scenes. The contemporary form, appearance and materiality is not felt to be objectionable given there is a reasonable degree of variety in the surrounding area and the buildings would not be widely visible from surrounding public vantage points. The proposals would result in the

school buildings being laid out around a loose cluster of more generous outdoor communal space, the landscaping of which would add to the overall quality of the school environment.

An assessment has been undertaken of the impact of the proposals on neighbours. Given the difference in levels, separation distances and the height and scale of the building, it is concluded that the teaching block would not give rise to unacceptable impacts on the amenity of the adjoining apartments at St Anne's Rise. In respect of the Sports Hall building, it is acknowledged that this would introduce built form on a part of the site which is presently given over to hardstanding and – in this regard – would represent a notable change for properties on Wordsworth Mead. However, the main bulk of the building would be in excess of 20m from the rear garden boundaries of these neighbours and, at this distance, it is not felt that it would be unduly overbearing. The building is considered to pass the established 25 and 45 degree rules in relation to neighbours on Wordsworth Mead and whilst there may be some shading effects on the rear gardens (or rear conservatories) of these properties, this would be limited to the winter months. Consideration has been given to the impact of the use of the Sports Hall in terms of noise and disturbance; however, it is considered that levels of break out noise would be sufficient low so as to not cause unacceptable disturbance. A condition limiting hours of use in acknowledgement of the fact that large groups/events leaving the hall in late hours may cause disturbance to neighbours is considered reasonable. Taking all of the considerations and conclusions into account, the proposals are not therefore considered to give rise to serious detriment to the amenities and living conditions of neighbouring properties.

There is no objection to the proposals from the County Highway Authority and the reconfiguration of the car parking area would give rise to a modest increase in formal designated car parking. There is also considered to be ample space within the re-organised layout to accommodate significant additional parking of the sort which may arise during a large event at the school. Additional cycle parking is proposed and a condition requiring a new Travel Plan is also recommended to encourage sustainable travel.

Impacts on trees and ecology are considered to be acceptable subject to appropriate management and mitigation measures which are contained within the recommended conditions. All proposed buildings would be within Flood Zone 1 and sustainable drainage measures would be secured by condition.

## **RECOMMENDATION(S)**

Subject to the completion of all documentation required to create a planning obligation under Section 106 of the Town and Country Planning Act 1990 (as amended) to secure:

- (i) A contribution of £6,150 towards Travel Plan monitoring
- (ii) The Council's legal costs in preparing the agreement

Planning permission is **GRANTED** subject to conditions.



In the event that a satisfactorily completed obligation is not received by 31 December 2018 or such longer period as may be agreed, the Head of Places and Planning be authorised to refuse permission for the following reason:

The proposal fails to make adequate provision for the monitoring of sustainable travel measures and local parking demand and therefore could give rise to a situation prejudicial to highway safety or which would fail to promote sustainable travel, contrary to policies Mo5 and Mo7 of the Reigate and Banstead Borough Local Plan 2005 and Policy CS17 of the Reigate and Banstead Core Strategy 2014.

## Consultations:

County Highway Authority: No objection subject to conditions and a financial contribution to Travel Plan monitoring

Natural England: No comments

Tree Officer: No objection subject to conditions

Environment Agency: No objection subject to conditions in respect of groundwater/protection of Controlled Waters

Surrey Wildlife Trust: Recommends that the applicant should be required to undertake all recommended actions in Ecological Impact Assessment Report (EclA) and associated Method Statement. Recommends opportunities to restore or enhance biodiversity are secured as advised in Section 10 of the EclA Report. Advises that the school undertakes an ecological management regime for the woodland and pond elsewhere on the site.

Surrey CC Sustainable Drainage Consenting Team: Satisfied that the drainage scheme meets policy, practice guidance and relevant Technical Standards. No objection subject to conditions.

Gatwick Airport: No objection subject to securing submission of a Bird Hazard Management Plan.

Contaminated Land Officer: No objection subject to conditions

UK Power Networks: No objections

## Representations:

Letters were sent to neighbouring properties on 07<sup>th</sup> August 2018; a site notice was posted 9<sup>th</sup> August 2018 and the application was advertised in local press on 16<sup>th</sup> August 2018. Further letters were sent to neighbouring properties on 10<sup>th</sup> October regarding additional information submitted by the applicant.

27 responses have been received raising the following main issues as set out below.

<b>Issue</b>	<b>Response</b>
Overshadowing/loss of daylight and sunlight	See paragraphs 6.16-6.18 and condition 9
Overlooking and loss of privacy	See paragraphs 6.19
Overbearing relationship	See paragraphs 6.16-6.18 and condition 9
Loss of outlook/visual amenity	See paragraphs 6.16-6.18
Noise and other disturbance (including light)	See paragraphs 6.20-6.23 and conditions 4, 11, 16, 17 and 21
Poor design	See paragraphs 6.9-6.13

Overdevelopment	See paragraphs 6.9-6.13
Out of character with the surrounding area	See paragraphs 6.9-6.13
Inconvenience during construction	See paragraph 6.22 and condition 4
Inadequate parking	See paragraphs 6.27-6.29 and condition 13, 22 and 23
Crime fears	See paragraphs 6.64
Health fears	See paragraphs 6.64
Loss of/harm to trees	See paragraphs 6.39-6.42 and condition 3
Harm to wildlife habitat	See paragraph 6.43 and conditions 10 and 14
No need for the development	See paragraph 6.7
Alternative location/proposal preferred	Application on its own merits, applicant has provided an appraisal of the options considered and discounted within their Design & Access Statement
Loss of private view	This is not a material planning consideration
Property devaluation	This is not a material planning consideration

Concerns were raised regarding insufficient information/inaccuracy of the submitted plans. These issues have been reviewed by Officers who are satisfied that the information submitted is adequate to allow a full and proper assessment and consideration of the application. A site visit has been undertaken and the report below reflects the Officers assessment taking account of the information and plans submitted and the Officers own observations on site.

## **1.0 Site and Character Appraisal**

- 1.1 The site consists of the existing The Warwick secondary school which is set within extensive grounds including playing fields and sports facilities. The site is within the built up area of Redhill, to the west of Redhill Town Centre.
- 1.2 The existing main school buildings are a mixture of single storey and two storey structures, largely concentrated in the south-west corner of the site. The buildings are of varied size, configuration and appearance, reflecting the ad hoc nature in which the school has evolved and expanded over the years. In the north-west corner of the site, are existing outdoor sports facilities, including tennis courts, a hard surface play area and an artificial multi-sports pitch. The eastern half of the site is broadly given over to grass playing pitches, with an area of woodland in the southern-east corner of the wider school which is presently used as an ecological area by the school, although this is outside of the red line of the current application. There is a significant change in levels across the site, particularly along the western boundary where the land rises significantly towards the neighbouring flats on St Anne's Rise.

- 1.3 The Redhill Brook runs through the southern part of the wider school site; however, it is outside of the red line of the current application. Part of the school site and application site is within Flood Zones 2 and 3 as a result, albeit no development is proposed outside of Flood Zone 1 in this application.
- 1.4 The school is in a predominantly residential environment, albeit it is bounded on its eastern side by the railway line. To the north, the site adjoins Wordsworth Mead, a mixed housing estate, with a number of properties on the estate backing or side on to the application site. To the west is a public footpath, beyond which is St Anne's Rise which consists of a number of three and four storey apartment blocks; these properties are situated at a higher land level. There are a number of trees along the western boundary, a significant proportion of which are subject to preservation orders. To the south, beyond the wooded area within the school site, are further residential properties, including 45 and 47 Cavendish Road which are Grade II listed.
- 1.5 As a whole, the application site extends to approximately 4.4ha.

## **2.0 Added Value**

- 2.1 Improvements secured at the pre-application stage: Pre-application advice relating to the provision of new replacement teaching space and sports hall was sought in 2017. No in principle objection was raised, although advice was given as to the need to consider the design, appearance and impact on neighbour amenity.
- 2.2 Improvements secured during the course of the application: The following were secured during the course of the application:
- Supplemental information regarding parking provision and strategy and noise impacts (including "break out" from the Sports Hall was sought and supplied)
  - Additional tree planting along the northern boundary
- 2.3 Further improvements to be secured through planning conditions or legal agreement: Various conditions are recommended including regarding materials (specifically to switch the proposed cladding colour on the buildings), implementation of landscaping and parking, construction management, tree protection and ecological mitigation.

## **3.0 Relevant Planning and Enforcement History**

- 3.1 There is various planning history associated with the expansion and operation of The Warwick School, with most applications determined by SCC as the Local Planning Authority.

## **4.0 Proposal and Design Approach**

- 4.1 The proposed development seeks full planning permission to demolish the existing main teaching block and sports facilities and erect a new teaching block together with a new sports hall. The works would also include changes to the layout of the

site, including vehicle access, parking areas and play areas. The works would be carried out in a phased manner to enable continued operation of the School.

- 4.2 The scheme comprises two blocks, linked together via a bridge/raised walkway at first floor level.
- 4.3 The teaching block, which would be the larger of the two, would be located in the western part of the site. The building would be in a broadly similar position to the existing sports hall, gym and two storey changing block. It would have a more compact, rationalised rectangular footprint than the existing teaching block. The teaching block would be a flat roofed, part two storey building along the western edge of the site but, due to the change in levels, would rise to three storeys within the site. It would accommodate the main school hall, along with a variety of teaching spaces and ancillary rooms.
- 4.4 The new Sports Hall would be sited on a north-south orientation, between the existing raised tennis courts and artificial pitch. It would be broadly comparable in scale to a three storey building, with a small single storey element. It would comprise a full height sports hall with changing and ancillary facilities at ground floor together with some teaching accommodation and support rooms (e.g. staff room) in partial first floor at the southern end of the building.
- 4.5 Both buildings would have a contemporary design and appearance, employing brick to the ground floor and vertical cladding to upper floors.
- 4.6 Within the wider site, the existing teaching block and sports hall would be demolished. The area currently occupied by the teaching block would, for the most part, be reconfigured to provide new outdoor space (combination of hard and soft landscaped) for pupils and a small, 9 space, visitor car park. The existing main car park to the front (south) of the site would be reconfigured and extended to provide 64 spaces (total 73).
- 4.7 The works are proposed to be carried out in a phased manner as follows:
  - Phase 1 – site set up, construction of new sports hall
  - Phase 2 – demolition of sports hall and partial demolition of teaching accommodation, construction of new teaching block
  - Phase 3 – demolition of remaining part of teaching block, completion of hard and soft landscaping and car parking works
- 4.8 A design and access statement should illustrate the process that has led to the development proposal, and justify the proposal in a structured way, by demonstrating the steps taken to appraise the context of the proposed development. It expects applicants to follow a four-stage design process comprising:  
Assessment;  
Involvement;  
Evaluation; and  
Design.
- 4.9 Evidence of the applicant's design approach is set out below:

<p>Assessment</p>	<p>The school site, of approximately 4.4 hectares, is located in a heavily residential area with Redhill town centre to the west, and a former landfill site to the East. The site also resides adjacent to a railway line, which serves Redhill Station. The School is made up of a cluster of five teaching blocks, of largely mixed specialism constructed between the 1960s-2000s, largely in the south-western corner of the site. Residential properties of four storeys are to the West of the site in St Anne’s Rise, with two storey properties in Wordsworth Mead. The school buildings occupy the southwestern corner of the site, with adjacent playing fields to the eastern and northern areas. The topography surrounding the site includes a high ground to the northwest, sloping down from this area to the southeast towards a wet habitat area that the school have labelled as an ecological area. The site is located in the catchment of the Redhill Brook, an EA designated Main River and tributary of the River Mole. The site is bordered by scattered trees, ornamental planting, scrub and amenity grassland. To the south of the site there is an area of broadleaved woodland containing a large pond. The main access point site via Noke Drive, for both pedestrians and vehicles is located in the south. There is an additional locked pedestrian access point from the northwest of the site via St Anne’s Drive. The school and local authority have confirmed the total PAN, which was increased in September 2017, to be 1050 pupils. The existing school’s maximum capacity however, is 1125. The proposed project will be based on the current PAN of 1050 pupils, with no expected increase to pupil numbers.</p> <p>Three existing school blocks (EFAB, EFAC, EFAD) will remain, creating a cluster of blocks centralised around soft and hard landscaping. All existing outdoor sporting provision (including MUGA, tennis courts and playing field) will be retained.</p>
<p>Involvement</p>	<p>The Planning Statement and Statement of Community Involvement identifies that pre-application advice was undertaken and that a public consultation event held in the school in November. Feedback is summarised as being largely positive with the main issues being traffic, parking and issues of privacy/impact on residential amenity.</p>
<p>Evaluation</p>	<p>The Design Statement explains and discusses a number of options, building typologies and different layout configurations which were considered through the design process. The Design Statement and Planning Statement explain that the accommodation layout and size for the proposed buildings on site has been greatly influenced by site and environmental constraints (including protecting existing playing areas and flooding risk), together with the need for the existing school to</p>

	be operational throughout the construction phases. The building locations need to enable the project to be safely constructed adjacent to an occupied school.
Design	The proposed location [of the buildings] was identified as the most feasible and least disruptive during construction and ultimately provided the most suitable long term for educational delivery for the school. The scheme has been design to reflect and utilise the sites existing levels and topography and minimise the requirement for 'cut & fill'. Building FFL have been set to reflect existing ground levels. A key priority of the design strategy for the new teaching blocks is to clearly organise space that facilitates the provision of the curriculum in accordance with the Authority's requirements and school specific brief and to provide for the needs of the pupils, staff and other users. The intention with the facade approach is to provide an inspirational building of which the school and local community can feel proud of and that significantly improves the local visual amenity. The choice of materials has been generated through consideration of their suitability for the local climate, conditions and anticipated use, including the need for robustness.

4.10 Further details of the development are as follows:

Site area	4.4ha
Existing use	Secondary School (Use Class D1)
Proposed use	Secondary School (Use Class D1)
Proposed floorspace	5,246m <sup>2</sup>
Existing parking	64 spaces (marked), space for 34 unmarked
Proposed parking	73 spaces (marked), space for 30 unmarked (see Landscape Planning Statement dated 16 July 2018)
Parking standard	BLP 2005 - individual assessment – BLP states that <i>“only operational requirements should be provided for...Pupil parking and drop off/pick up</i>
Pupil numbers	Current PAN of 1,050; no proposal to increase

**5.0 Policy Context**

5.1 Designation

Urban Open Land

Flood Zone 2/3 (part of site – south-east corner) [Note: no new buildings or development is proposed in FZ2/3 through this application]

## 5.2 Reigate and Banstead Core Strategy

CS1 (Presumption in favour of sustainable development)  
CS4 (Valued townscapes and historic environment)  
CS5 (Valued people/economic development),  
CS8 (Area 2a (Redhill))  
CS10 (Sustainable development),  
CS11 (Sustainable construction),  
CS12 (Infrastructure delivery)  
CS17 (Travel options and accessibility)

## 5.3 Reigate & Banstead Borough Local Plan 2005

Landscape & Nature Conservation	Pc2C, Pc2G, Pc4, Pc6
Community Facilities	Cf1, Cf2
Recreation	Re11
Movement	Mo4, Mo5, Mo6, Mo7, Mo13
Utilities	Ut4

## 5.4 Other Material Considerations

National Planning Policy Framework	
National Planning Practice Guidance	
Supplementary Planning Guidance	Local Distinctiveness Design Guide Developer Contributions SPD Surrey Design
Other	Human Rights Act 1998 Community Infrastructure Levy Regulations 2010 (as amended) Conservation of Habitats and Species Regulations 2010

## 6.0 **Assessment**

6.1 The application site comprises an existing secondary school within the urban area to the west of Redhill town centre. The site, like many schools in the borough, is designated as Urban Open Land and the southern part of the site is within the Flood Zone 2/3 according to EA Flood Maps. The proposals seek permission to demolish a number of the existing school buildings and the erection of a new main teaching block and Sports Hall with associated internal reconfiguration and re-landscaping of the site.

6.2 The main issues to consider are therefore:

- development on Urban Open Land
- design and impact on the character of the area
- effects on the amenity of neighbouring properties
- access, parking and highway implications
- other matters



### Development on Urban Open Land

- 6.4 As above, the Warwick School site – as is common with many school sites in the borough – is designated as Urban Open Land.
- 6.5 Policy Pc6 – which generally seeks to control development on, and resist the loss of, Urban Open Land – is therefore applicable. Policy Pc6 does however allow for proposals for ancillary buildings or for the extension or replacement of existing buildings subject to consideration of the relevant design policies, the contribution made by the UOL to the character of the area and to the functioning of any essential social, community or educational use. The proposals in this case are considered to fall within the scope of this since they are a replacement of existing educational facilities.
- 6.6 The site is, for the most part, already largely built up either with the footprint of the school buildings or associated hardstanding and ancillary structures. The siting of the proposed buildings would preserve the vast majority of the current open parts of the site (particularly the playing fields and woodland area to the east). Furthermore, the two proposed buildings combined would occupy a lesser footprint on the site than those which they would replace, with additional areas given over to open outdoor and soft landscaping within the site. Taken in the round, it is therefore considered that the proposals by virtue of the rationalised footprint and siting of the buildings would support the openness of the site and its designation as Urban Open Land rather than detriment it. In coming to this view, it is acknowledged that elements of the site which are proposed for development are visible from public vantage points (e.g. the area proposed for the Sports Hall can be glimpsed from Wordsworth Mead); however, these parts of the site are urbanised in character (given over to hardstanding and surrounded by hard engineering or tall fences) and as a result are not considered to contribute positively to the character of these areas.
- 6.7 The exception in Policy Pc6 also advises that consideration should be given to the functioning of any social, community or educational use. In this case it is agreed that, as described in the Planning Statement, the proposals would support a significant improvement and investment in educational facilities on the site, providing a learning environment suited to modern teaching and curriculum. The fact that the school has secured ESFA funding to replace existing buildings suggests the current accommodation is qualitatively sub-standard. In this respect, the proposals would support the strategic objectives of the Core Strategy, notably SO13 which sets out the Council's aim to "*secure in appropriate locations, adequate land, community services and infrastructure to support business and community needs*" and would also find favour in Policy CS12 of the Core Strategy. In accordance with national policy (NPPF para 94), the need to – and benefits from – altering and improving the school to meet education requirements is afforded "*great weight*".
- 6.8 Overall, it is therefore concluded that the proposals would be consistent with the provisions and aims of Urban Open Land policy and would not therefore conflict with Policy Pc6 of the 2005 Borough Local Plan.

Design and impact on the character of the area

- 6.9 The proposal comprises two blocks linked by a first floor glazed walkway/bridge.
- 6.10 The teaching block would be a part two storey, part three storey building, reflecting the change in levels within the site. From external vantage points on St Anne's Rise, it would appear as a two storey building, not dissimilar in height to the existing buildings and would be in keeping with the scale of buildings in the locality, particularly mindful of 3 to 4 storey flatted blocks which it would be glimpsed in the backdrop to.
- 6.11 The Sports Hall building would be, in effect, a three storey building (10m from ground to parapet) with a smaller single storey element on its northern end. Whilst it would be of larger scale and massing than the residential properties on the adjoining Wordsworth Mead, it would be set back sufficiently from the public realm and orientated such that it would appear as an unduly dominant feature within the character of the area or street scene. Its height would be in keeping with other buildings within the wider school estate (and not dissimilar to the Sports Hall it would replace) and thus would not be incongruous in this respect. Additional landscaping proposed along the northern boundary would help to screen and soften views of it from public vantage points.
- 6.12 The proposals would be laid out such that the main teaching buildings of the school (either new or retained) would be laid out in a loose cluster around a more generous central area of outdoor communal space. The applicant also argues that the buildings have been laid out to avoid "dead" spaces which might encourage anti-social behaviour within the school grounds, an approach which is supported. The central outdoor areas would be re-landscaped as part of the proposals with a mixture of hard and soft landscaping, with features such as stepped gabion walls used to both manage the variation in levels across the site but also add interest to the public realm of the school. As discussed above, overall it is considered that the layout of the school following the works would have a greater sense of openness than is presently the case.
- 6.13 Both the teaching block and Sports Hall buildings would have a flat roofed form, not dissimilar to many of the existing school buildings which they would replace. Whilst the buildings would have a relatively contemporary appearance, owing to both their boxy form, fenestration and materials selection, this is not in itself objectionable, particularly mindful of the Framework which advises against preventing or discouraging appropriate innovation. The buildings themselves are considered to be well-designed relative to their use and function, with ordered, coherent elevations and features such as the large glazed two storey entrance serving the dining hall/library on the main teaching block, deep contrasting colour window reveals and the glazed link elevating the design above the ordinary. The materials palette, whilst differing from the brick and render which is typical of the surrounding residential development, is a key part of this design ethos and there is sufficient variety in the surrounding area (including for example Park 25) such that it would not appear unduly alien, particularly given views of the buildings would be limited to a small

number of public vantage points. Overall, it is considered that they would add positively to the environment, identity and sense of place of the school.

- 6.14 Taking all of the above into account, it is considered that the proposed buildings and layout represent good design which would both improve the school environment and fit comfortably with the character of the wider area. Consequently, the proposals are considered to comply with policy Cf2 and Re2 of the Local Plan 2005, Policy CS4 and CS11 of the Core Strategy and the provisions of the NPPF.

Effects on the amenity of neighbouring properties

- 6.15 The impact of the proposals on neighbouring properties has been carefully assessed, with particular regard to neighbours on St Anne's Rise and Wordsworth Mead who are most closely affected by the proposals.
- 6.16 The proposed teaching block would be sited close to the western boundary of the site with St Anne's Rise which comprises a number of blocks of 3-4 storey flats. The block would be two storeys along its western flank and would be comparable in height to the existing two storey building which it would in part replace. Given this, and taking account of both the separation distance to the adjoining blocks of flats and fact that the land levels along the western boundary are approximately 3m above the proposed floor level of the building, it is considered that the buildings would not be unduly overbearing or dominant on these neighbours, nor cause unacceptable loss of light.
- 6.17 The Sports Hall would be situated on the northern part of the site, between the existing elevated tennis courts and the artificial playing pitch and, given its function, would be a relatively tall building – just over 10m to the flat roof, with a lower single storey element (c.3m high) on its northern end. Whilst it is acknowledged that the introduction of the Sports Hall in this location would represent a change for neighbouring properties, particularly to those to the north of the school on Wordsworth Mead, it is – on balance – not considered that it would cause harmful detriment to their amenities so as to be contrary to policy and warrant refusal. The two storey element of the proposed Sports Hall building would be approximately 24m from the rear/side boundaries of the residential properties to the north on Wordsworth Mead, with the smaller single storey element approximately 20m from the boundaries. At this distance, even acknowledging the scale of the building, it is not considered that it would be unduly dominant, overbearing or seriously detrimental to outlook, even when taken in combination with existing structures on this northern part of the site. During the course of the application, additional tree planting has also been secured along this northern boundary which would supplement the existing planting and help to dapple views of the building and soften the interface to the neighbouring properties (albeit the conclusion that it would not be overbearing or harm outlook is not reliant on this planting). The same conclusion is reached in respect of the perpendicular terrace at no.34-40.
- 6.18 In terms of overshadowing, it is acknowledged that the proposed building would be positioned due south of the neighbours on Wordsworth Mead. The applicant has supplied an indicative section which illustrates on the plan the shading effects at mid-day on the summer and winter solstices (i.e. 21<sup>st</sup> June and 21<sup>st</sup> December).

This demonstrates that, whilst the building would give rise to some shading of the rear garden of properties on Wordsworth Mead, this effect would be mostly in the winter months with little, if any, impact in the summer months (as demonstrated by the summer solstice shading point). Furthermore, the plans also demonstrate that – in relation to those properties which back onto the site, the proposed building would not infringe the 25 degree rule taken from the rear windows of these properties. It is appreciated that some concerns have been raised in representations regarding the accuracy of these plans (including for example the fact that they omit rear conservatories and the like from neighbouring properties). However, having carefully reviewed the full suite of plans and the Council's own mapping, I am satisfied that the plans are sound and representative of the relationships and effect which would arise as a result of the proposals. On the specific point of the omission of rear conservatories on the applicant's plans, I have nonetheless considered the effect that the proposals would have on these additions and conclude that the 25 degree rule would still be passed and, whilst there would be some limited shading effect in winter months, this would not be unduly harmful to amenities, particularly given such additions are highly glazed and not reliant on a single window for light source. Consideration has also been given to the impact on the terrace of properties at no.34-40 Wordsworth Mead (which run perpendicular to the northern boundary with the school); however, given the juxtaposition and separation between these neighbours and the Sports Hall building, it is considered that there would not be an unacceptable overshadowing impact on these neighbours: whilst the Sports Hall may infringe the 45 degree rule in the horizontal plane when taken from the rear windows on the nearest of the properties in the terrace (no.45) this would be at some considerable distance and, furthermore, it would pass in the vertical plane such that any loss of light to rear windows would not be so harmful to warrant refusal.

- 6.19 No windows are proposed in the northern end of the Sports Hall and the windows in the side elevations would be towards the southern end of the building. Given the distances involved to neighbouring properties, any views would be very long range and sufficiently oblique such that they would not give rise to a harmful loss of privacy. The same conclusion applies to the glazed link between the teaching block and Sports Hall given the separation distances. The teaching block would have a number of windows in its western elevation which would look towards the adjoining flats on St Anne's Rise. However, given the differences in levels, intervening footpath, boundary treatments and tree cover, it is not considered that these windows would give rise to unacceptable loss of privacy for these neighbours. In coming to this view, it is also acknowledged that the distance between the teaching block windows and properties on St Anne's Rise would be approximately 30m at the closest point.
- 6.20 Consideration has also been given to noise and disturbance impacts which is another concern identified in representations from neighbouring properties. With regard to the main teaching block building, it is concluded that the likely nature and profile of noise arising would not be materially different from the existing school buildings on this part of the site. Whilst it is appreciated that this building would include a hall which could be used for congregations of pupils or events, this would be at lower ground floor level, with other conventional classroom spaces above. Any noise breakout from the hall is therefore likely to be limited and not sufficient to give

rise to a harmful effect on the flats on St Anne's Rise, particularly given the separation distances involved.

- 6.21 An Acoustic Assessment and Noise Assessment were submitted and the latter was updated during the course of the application to include any assessment of potential noise break out from the use of the Sports Hall building. This updated Assessment identifies that, based on the construction specification proposed; noise break out from the Sports Hall would be 26.1 dBA (Lfmax) at the rear gardens of properties on Wordsworth Mead, significantly below recommended British Standard levels of 50-55 dBA. The applicant explains that the Sports Hall has to be constructed to high acoustic performance standards by the Department for Education due to its proposed ancillary use as an exam hall. Furthermore, the Noise Assessment confirms that doors would not be required to be opened to provide ventilation and a condition ensuring that the building is constructed and operate in accordance with this commitment is recommended. Taking this into account, it is not considered that the use of the Sports Hall would give rise to a harmful increase in noise and disturbance for neighbouring properties on Wordsworth Drive. In coming to this view, account has also been taken of the fact that – in its current form – the location of the proposed Sports Hall is presently an area of open hardstanding which is used for outdoor play and recreation which would in itself give rise to a level of unabated noise. Concerns have been raised regarding potential evening use of the Sports Hall; it is acknowledged that – in its current form – the area on which the Sports Hall is located is unlikely to be used in evenings or later at night and thus the proposal could change the timing of noise, not only in terms of use but also in general activity as visitors leave. The adjoining sports courts are subject to a condition limiting hours of use and it is considered reasonable in this case to limit use of the Sports Hall to no later than 10pm Mondays to Saturdays, and no later than 6pm on Sundays. This would strike a balance between allowing for the appropriate use of the Sports Hall (including dual use anticipated by Policy Re11) whilst also preserving neighbour amenities.
- 6.22 A number of representations received identify concerns regarding inconvenience during the construction process. Whilst it is appreciated that some disturbance might arise during the construction process, this would by its nature be a temporary impact. Other environmental and statutory nuisance legislation exists to protect neighbours and the public should any particular issues arise. A condition requiring a Construction Transport Management Plan will assist in ensuring impacts of movements; parking and transport activity associated with the development are considered.
- 6.23 In respect of light pollution, the application was supported by an External Lighting Report which identifies the broad principles which would be used to govern external lighting installations around the site. This includes a plan which identifies that, for those external areas close to residential neighbours, external lighting would be of relatively low illuminance (5 lux) and would thus be unlikely to cause material harm or disturbance to neighbour amenities. A condition requiring a detailed lighting specification and scheme – in broad compliance with the principles in the External Lighting Report – to be submitted for approval is however considered prudent to ensure that this is satisfied. There would be limited fenestration in the Sports Hall towards the northern end of the building (i.e. close to Wordsworth Mead

neighbours) and as such light spill from inside the building is not considered to be an issue.

- 6.24 Overall, it is concluded that whilst the proposals would result in a change in relationship to neighbouring properties, particularly those on Wordsworth Mead, on balance the development would not give rise to serious adverse impacts on neighbour amenity and therefore complies with policies Cf2 and Cf3 the Borough Local Plan 2005.

Accessibility, parking and highway implications

- 6.25 The application incorporates alterations and reconfiguration of the car parking and internal circulation within the site; however, the main vehicular and pedestrian access points would be unchanged.
- 6.26 The school currently has 873 pupils on roll and is therefore operating below its published capacity of 1,050 pupils (PAN of 210 specified by Surrey County Council – note there is no planning restriction on pupil numbers) which it anticipates to reach by 2021. The school could therefore operate at a higher capacity than it presently does even in absence of this current application (indeed the current application does not increase the overall floorspace of the school and the national School Capacity Survey 2016/2017 indicates that The Warwick has physical capacity for 1,125 pupils in the existing buildings). Nonetheless, the application was supported by a Transport Assessment which considers the implications of the school operating at 1,050.
- 6.27 In terms of car parking, at present the site has a total of 65 formal designated car parking spaces which consists of a main car park at the entrance to the site from Noke Drive for 41 spaces, and a number of other marked out parking areas fragmented across the site providing a further 24 spaces. The current application would increase provision to 73 spaces and would consolidate the majority of the parking into a single larger main car park at the entrance to the site for 65 spaces, with a further 8 disability compliant spaces provided closer to the new main teaching block. The Transport Assessment notes that it anticipates that the number of staff would potentially increase by 10 if the school were operating at its full published PAN; hence, the increased parking provision is welcomed. The County Highway Authority has reviewed the submission and confirms that they have no objection to the proposed level of parking, particularly noting that there are extensive parking restrictions in the surrounding area which would prevent displacement parking on nearby roads or in unsafe locations.
- 6.28 In addition to formal designated car parking, it is acknowledged and observed from the site visit that the school, in its current layout and configuration, has areas where parking can be made available on an “ad hoc” basis to cope with increased demand (e.g. during school events). The applicant has confirmed that they consider that such capacity would remain within the proposals through a combination of parking on hardstanding/playground areas, reinforced amenity grass areas, spaces “freed up” by staff not required for the event and additional provision on the playing fields (c.100 spaces) if required. Having reviewed this, I agree that sufficient space would remain within the reconfigured layout to manage temporary “surge” demand in

parking associated with a large event such that it would not prejudice highway safety or operation.

- 6.29 The school is considered to be in a highly accessible location, close to bus and rail services in Redhill Town Centre. Hence, in principle, it is well situated to encourage modal shift towards more sustainable travel modes, both for staff and pupils. The application includes provision to significantly increase on-site cycle parking (from a current capacity of 65 bicycles to 115) in order to meet the latest standards. The provision of such an enhancement in cycle storage infrastructure is welcomed and meets with the overall thrust of policy CS17 of the Core Strategy. Furthermore, the application was supported by a draft School Travel Plan which begins to set out the measures to promote sustainable travel to school by both staff and pupils; a condition requiring a final Travel Plan following the County Council's favoured Modeshift STARS model is recommended prior to occupation.
- 6.30 Taking all of the above into account, include the expert advice of the CHA following their detailed review of the application, it is considered that the scheme complies with policies Mo4, Mo5, Mo6 and Mo7 of the Local Plan and Policy CS17 of the Core Strategy.

#### Flooding and drainage

- 6.31 The application site, which covers part of the Warwick School grounds, is predominantly in Flood Zone 1; however, part of the southern part of the site is within Flood Zone 2 according to EA Flood Maps.
- 6.32 All of the new buildings (teaching block and Sports Hall) proposed in the application would be within Flood Zone 1 and the applicant's Design Statement explains that, amongst the options considered in developing the final scheme, buildings on the southern part of the site or areas at risk of flooding were discounted. This approach is supported by, and consistent with, the requirements of national policy which advises that applications should demonstrate that "*within the site, the most vulnerable development is located in areas of lowest flood risk, unless there are overriding reasons to prefer a different location*". Given the discussion above, there are not considered to be overriding reasons in this case. Improvement of the schools facilities could not realistically be achieved by providing development anywhere other than within the boundary of the existing School.
- 6.33 The Environment Agency was consulted on the application. No objection was raised but the EA recommended conditions to ensure no adverse groundwater impacts.
- 6.34 The application was supported by a Drainage Impact Assessment which considers the potential drainage solutions for the site and proposes a system of infiltration using a new soakaway. The County Council – as the Lead Local Flood Authority – has reviewed this information and considers it to be sufficient to support the scheme subject to conditions to secure the detail at a later date.
- 6.35 Based on the above and subject to conditions, the proposals comply with policy Ut4 of the Local Plan and CS10 of the Core Strategy.

### Community Infrastructure Levy (CIL) and requested contributions

- 6.36 The proposal, being for a new school, falls outside of the uses which attract a charge based on the Council's adopted Charging Schedule and as such the development would not be liable to pay CIL.
- 6.37 Community Infrastructure Levy (CIL) Regulations were introduced in April 2010 which states that it is unlawful to take a planning obligation into account unless its requirements are (i) relevant to planning; (ii) necessary to make the proposed development acceptable in planning terms; and (iii) directly related to the proposed development. As such only contributions, works or other obligations that are directly required as a consequence of development can be requested and such requests must be fully justified with evidence including costed spending plans to demonstrate what the money requested would be spent on.
- 6.38 In this case, no such contributions or requirements have been requested.

### Trees and landscaping

- 6.39 The wider school site has relatively significant tree cover, particularly along the boundary with the railway line and the area of woodland around the Brook in the southern reaches of the site. With respect to the current application, the most sensitive potential arboricultural implications relate to the protected trees along the western boundary of the site with the adjoining flats at St Anne's Rise.
- 6.40 The application was supported by a Tree Survey and Arboricultural Impact Assessment which shows the implications of the development for trees and tree cover and the site and the tree protection measures to be put in place. This has been reviewed by the Council's Tree Officer who concludes that "*the tree losses are considered to be relatively minor in respect of the development*" and that "*none of the trees lost would result in any significant or long lasting adverse affect on the existing landscape*". The Tree Officer notes that the protection measures and method statements in the current submission generally meet the Council's requirements; however, as the routing of services has not been confirmed at this stage, it is prudent to require a finalise TPP and AMS to be submitted prior to commencement to avoid any currently unanticipated impacts on retained trees.
- 6.41 The response from the Tree Officer also notes that there would be opportunities to include replacement tree planting to mitigate any losses. The application was supported by a Landscape Planning Statement which provides sufficient detail of the proposed hard and soft landscaping strategy for the site, including specifying materials, furniture, plant species, sizes and density which would be used. This is considered adequate and a condition requiring the landscaping associated with each phase to be completed prior to occupation of that phase is considered reasonable.



- 6.42 Based on the above, it is considered that – subject to conditions – the effect of the development on existing tree cover and local landscape character would be acceptable. It therefore complies with policies Pc4 and Cf2 on this issue.

Other matters

- 6.43 The application was supported by numerous ecological reports including a Preliminary Ecological Appraisal, Ecological Impact Assessment and dedicated Bat and Great Crested Newt Surveys. Although the site itself is not subject to any specific nature conservation designations, it is located close to the Holmethorpe Sandpits Complex Site of Nature Conservation Importance; the Preliminary Ecological Appraisal submitted with the application concludes that, given the site is screened from the SNCI by the tree lined embankments and given the existing noise levels along the railway; works on the site are unlikely to disturb qualifying wintering bird species of the SNCI. A supplemental Great Crested Newt (GCN) survey was also undertaken given the original PEA identified a potential (albeit low) risk of GCNs on site. The survey identified a likely absence of Great Crested Newts within the ponds/waterbodies on site and therefore that the proposal is unlikely to give rise to any impacts. Bat Surveys were also undertaken (as recommended by the original PEA); these identified a common pipistrelle bat roost within a section of one of the buildings to be demolished but concludes that the roost is of low conservation value at the local level and recommends mitigation measures (bat boxes be implemented within the site). The Ecological Impact Assessment includes detailed and extensive recommendations in terms of working practices and general mitigation measures; a condition requiring compliance with these is recommended. Based on the information submitted, it is considered that the ecological impacts of the proposal have been adequately appraised and, subject to conditions, can be appropriately mitigated such that it would not have an unacceptable adverse impact on overall biodiversity value of the site or on protected species. As such, the proposal is considered to comply with Pc2C and Pc2G of the Local Plan 2005, CS2 of the Core Strategy and relevant legislation. Surrey Wildlife Trust was consulted on the application and raised no objection subject to compliance with the measures in the Ecological Impact Assessment.
- 6.44 The application was supported by an Energy Statement which sets out the building has been designed to follow a “Be Lean” approach and would implement a range of passive and active energy efficiency measures to reduce energy demand and achieve efficiency. In accordance with Policy CS11 of the Core Strategy, the development will be required to meet BREEAM ‘Very Good’ standard and a condition to this effect is recommended.
- 6.45 Concerns have been raised in relation to crime and health. In respect of crime, some representations argue that the proposals may encourage bullying and anti-social behaviour; in this regard, the applicant’s Design & Access Statement explains that these issues have taken into account when designing the layout by reducing ‘dead areas’ or unsupervised areas where bullying can take place, providing generous circulation space and generally improved facilities such as WCs. Whilst the concerns of residents in this respect are acknowledged, on balance, the rationalised footprint of the building, increased areas of open, uninterrupted communal space and generally improved facilities are considered to be a step

forward compared to the existing situation (as the applicant's submission suggests) in terms of minimising opportunity for poor behaviour. Policies and monitoring/management of pupil behaviour – including anti-social behaviour – would be a matter for the school, assisted as appropriate by local policing. With regard to health, whilst the specific concerns made in relation to the impacts on particular neighbours who are experiencing ill health, as above, it is not considered that the proposals would give rise to unacceptable amenity impacts.

## CONDITIONS

1. The development hereby permitted shall be carried out in accordance with the following approved plans:

<b>Plan Type</b>	<b>Reference</b>	<b>Version</b>	<b>Date Received</b>
Site Layout Plan	27700_T	0	30.07.2018
Site Layout Plan	27700_T(1)	0	30.07.2018
Site Layout Plan	27700_T(2)	0	30.07.2018
Site Layout Plan	27700_T(3)	0	30.07.2018
Site Layout Plan	27700_T(4)	0	30.07.2018
Site Layout Plan	12564D_UG 1		30.07.2018
Site Layout Plan	12564D_UG 2		30.07.2018
Site Layout Plan	12564D_UG 3		30.07.2018
Site Layout Plan	12564D_UG 4		30.07.2018
Site Layout Plan	12564D_UG 5		30.07.2018
Site Layout Plan	125268-LLB-ZZ-ZZ-DR-L-4014	P02	30.07.2018
Site Layout Plan	125268-LLB-ZZ-ZZ-DR-L-4013	P02	30.07.2018
Site Layout Plan	125268-LLB-ZZ-ZZ-DR-L-4012	P03	30.07.2018
Site Layout Plan	125268-LLB-ZZ-ZZ-DR-L-4011	P03	30.07.2018
Location Plan	UNNUMBERED		30.07.2018
Section Plan	125268-LLB-XX-XX-DR-L-3002	P04	30.07.2018
Section Plan	125268-LLB-XX-XX-DR-L-3003	P03	30.07.2018
Block Plan	125268-LLB-ZZ-XX-DR-L-4017	P01	30.07.2018
Floor Plan	WRK-SS-ZZ-LF-DR-B-102	S1	30.07.2018
Section Plan	125268-LLB-XX-XX-DR-L-3001	P04	30.07.2018
Block Plan	WRK-SS-ZZ-LF-DR-B-201	S1	30.07.2018
Floor Plan	125286-LEP-WS-01-M2-A-0305	C02	02.08.2018
Floor Plan	125286-LEP-WS-02-M2-A-0302	C02	02.08.2018
Floor Plan	125286-LEP-WS-01-M2-A-0301	C02	02.08.2018
Floor Plan	125286-LEP-WS-00-M2-A-0304	C02	02.08.2018
Elevation Plan	125286-LEP-WS-XX-M2-A-0200	C2	02.08.2018
Floor Plan	125286-LEP-WS-00-M2-A-0300	C02	02.08.2018
Other Plan	08423-LEP-WS-ZZ-M2-A-0253	C02	02.08.2018
Other Plan	08423-LEP-WS-ZZ-M2-A-0250	C02	02.08.2018
Other Plan	08423-LEP-WS-ZZ-M2-A-0252	C02	02.08.2018
Other Plan	08423-LEP-WS-ZZ-M2-A-0251	C02	02.08.2018
Floor Plan	WRK-SS-ZZ-ZZ-DR-B-101	S1	30.07.2018
Other Plan	WRK-SS-ZZ-ZZ-DR-B-103	S1	30.07.2018
Site Layout Plan	125268-LLB-ZZ-XX-DR-L-4001	P06	30.07.2018
Elevation Plan	125286-LEP-WS-ZZ-M2-A-0320	C04	02.08.2018

Roof Plan	125286-LEP-WS-02-M2-A-0306	C04	02.08.2018
Roof Plan	125286-LEP-WS-03-M2-A-0303	C04	02.08.2018
Elevation Plan	125286-LEP-WS-ZZ-M2-A-0322	C04	02.08.2018
Elevation Plan	125286-LEP-WS-ZZ-M2-A-0321	C04	02.08.2018
Other Plan	125268-LLB-ZZ-ZZ-DR-L-4018	P01	27.09.2018
Site Layout Plan	125268-LLB-ZZ-XX-DR-L-4002	P05	27.09.2018
Section Plan	125268-LLB-XX-XX-DR-L-3004	P05	27.09.2018

Reason:

To define the permission and ensure the development is carried out in accord with the approved plans and in accordance with National Planning Practice Guidance.

Note: Should alterations or amendments be required to the approved plans, it will be necessary to apply either under Section 96A of the Town and Country Planning Act 1990 for non-material alterations or Section 73 of the Act for minor material alterations. An application must be made using the standard application forms and you should consult with us, to establish the correct type of application to be made.

2. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason:

To comply with Section 91(1) of the Town and Country Planning Act 1990 as amended by Section 51 (1) of the Planning and Compulsory Purchase Act 2004.

3. No development on a given phase, including demolition or any groundworks preparation, shall commence until a detailed, scaled Tree Protection Plan (TPP) and the related finalised Arboricultural Method Statement (AMS) for that phase is submitted to and approved in writing by the Local Planning Authority (LPA). These shall include details of the specification and location of exclusion fencing, ground protection and any construction activity that may take place within the Root Protection Areas of trees (RPA) shown to scale on the TPP, including the installation of service routings. The AMS shall also include a pre commencement meeting, supervisory regime for their implementation & monitoring with an agreed reporting process to the LPA. All works shall be carried out in strict accordance with these details when approved.

Reason:

To ensure good arboricultural practice in the interests of the maintenance of the character and appearance of the area and to comply with British Standard 5837:2012 'Trees in Relation to Design, demolition and Construction – Recommendations' and policies Pc4 and Cf2 of the Reigate and Banstead Borough Local Plan.

4. No development on a given phase shall commence until a finalised Construction Transport Management Plan has been submitted to and approved in writing by the Local Planning Authority. The final plan shall include details of:
  - (a) parking and turning for vehicles of site personnel, operatives and visitors
  - (b) loading and unloading of plant and materials
  - (c) storage of plant and materials
  - (d) provision of boundary hoarding behind any visibility zones
  - (e) construction vehicle routing to and from the site
  - (f) on-site turning for construction vehicles

(g) measures to prevent the deposit of materials on the highway  
(h) before and after construction condition surveys of the highway and a commitment to fund the repair of any damage caused  
has been submitted to and approved in writing by the Local Planning Authority. Only the approved details shall be implemented during the construction of the development.

Reason:

In order that the development should not prejudice highway safety nor cause inconvenience to other highway users to satisfy policies Mo5 and Mo7 of the Reigate and Banstead Borough Local Plan 2005 and the objectives of the NPPF.

5. No development, except demolition, shall commence on a given phase until a detailed remediation method statement for that phase of works has been submitted to and approved in writing by the Local Planning Authority Such a statement should be in accordance with The Warwick School Scope of Ground Investigation (dated 8th May 2018 Ref TETDm-12908-030518-Gibreif-F1), Geo-Environmental Desk Study (June 2015 by Mott Macdonald), Factual Report on Phase 2 Geo-Environmental Site Investigation (dated September 2017 ref 3453913917\_01 by Arcadis) and Interpretative Site Assessment Report (dated September 2017 ref 3453913925 by Arcadis) and should detail the extent and method(s) by which the site is to be remediated, the information to be included in a validation report and any additional requirements which the Local Planning Authority may specify.

The Local Planning Authority shall then be given a minimum of two weeks written notice of the commencement of the approved remediation works.

Reason:

To ensure that the proposed development and any site investigations and remediation will not cause harm to human health or pollution of controlled waters with regard to policy CS10 of the Reigate and Banstead Core Strategy 2014 and the NPPF.

6. No above ground construction or superstructure works shall take place until the detailed design of the surface water drainage system for the site has been submitted to and approved in writing by the Local Planning Authority. This shall include:
- (a) A design that satisfies the SuDS hierarchy and that is compliant with the national non-statutory technical standards for SuDS, NPPF and Ministerial Statement on SuDS
  - (b) Results of infiltration testing carried out in accordance with BRE Digest 365
  - (c) Evidence that the proposed solution will effectively manage the 1 in 30 & 1 in 100 (+CC% allowance) for climate change storm events, during all stages of the development (pre, post and during). If infiltration is deemed unfeasible then an alternative discharge strategy will need to be provided, discharging to a watercourse using a discharge rate of Greenfield Qbar
  - (d) Evidence that any proposed infiltration will not give rise to unacceptable risk to Controlled Waters
  - (e) Detailed drawings showing the finalised drainage layout with location of drainage elements, pipe diameters, levels, and long or cross sections of each drainage element including any flow restrictions and maintenance/risk reducing features (e.g. silt traps, inspection chambers, etc.)

(f) Details of how the system will be protected during construction and how runoff (including any pollutants) from the development site will be managed before the system is operational

(g) A plan showing exceedance flows (i.e. during rainfall greater than design events or during blockage) and how property on and off site will be protected.

Only the approved details shall be implemented during the construction of the development.

Reason:

To ensure that the SuDS are adequately planned, delivered and maintained and that the development is served by an adequate and approved means of drainage to comply with Policy Ut4 of the Reigate and Banstead Borough Local Plan 2005, Policy CS10 of the Core Strategy 2014 and the requirements of non-statutory technical standards.

7. No above ground construction or superstructure works on the buildings hereby approved shall take place until evidence that the development is registered with a BREEAM certification body and a pre-assessment report (or design stage certificate) indicating that the development can achieve a 'Very Good' rating has been submitted to and approved in writing by the Local Planning Authority.

Reason:

To ensure that the development is constructed to appropriate sustainability standards with regard to Policy CS11 of the Reigate and Banstead Core Strategy 2014.

8. The development shall be carried out in strict accordance with the ground levels and finished floor levels specified on the approved drawings and within the Landscape Planning Statement (by Kier Construction dated 16 July 2018). There shall be no variation to these approved levels, or to any site levels outside of the areas specified in the Landscape Planning Statement, without the prior consent in writing of the Local Planning Authority.

Reason:

To ensure the Local Planning Authority are satisfied with the details of the proposal and its relationship with adjoining development and to safeguard the visual amenities of the locality with regard to Reigate and Banstead Borough Local Plan 2005 policy Cf2.

9. The development shall be carried out in strict accordance with the recommendations, avoidance and mitigation measures identified in the Ecological Impact Assessment (by Lloyd Bore Ltd reference 125268-LLB-RP-EC-0003-S3-P02 dated 02/10/2018) in respect of construction working methods, ecological enhancement and provision of replacement or alternative habitat.

Reason:

In order to preserve and enhance the wildlife and habitat interest on the site and ensure species present on the site are afforded appropriate protection during construction works with regard to Reigate and Banstead Borough Local Plan 2005 policy Pc2G.

10. The teaching block and Sports Hall hereby approved shall be constructed in accordance with the recommendations, principles and sound insulation specification

set out in the Acoustic Strategy Report (ref: PC-18-0130-RP2RevA) by Pace Consult Ltd.

Reason:

In order to ensure the buildings are appropriately constructed to provide an appropriate internal environment and to minimise the break out of noise with regard to policy Cf2 of the Reigate and Banstead Borough Local Plan 2005.

11. The development hereby approved shall be carried out in strict accordance with the external facing materials and details specified on the approved plans except that the Ice Blue cladding shall be installed on the Sports Hall and the Wedgewood Blue shall be installed on the teaching block. There shall be no variation in the above without the prior consent in writing of the Local Planning Authority.

Reason:

To ensure that a satisfactory external appearance is achieved of the development with regard to Reigate and Banstead Borough Local Plan 2005 policy Cf2.

12. The teaching block hereby approved shall not be first occupied unless and until a final School Travel Plan through MODESHIFT STARS has been submitted to and approved in writing by the Local Planning Authority. Such a statement should be in accordance with the sustainable development aims and objectives of the National Planning Policy Framework, Surrey County Council's "Travel Plans Good Practice Guide" and in general accordance with the Robert West School Travel Plan document numbered 4662/005/002.

The approved Travel Plan shall be implemented upon occupation of the teaching block and thereafter the Travel Plan shall be maintained and developed through STARS to the satisfaction of the Local Planning Authority.

Reason:

To ensure that the development would promote sustainable transport choices with regard to Policy CS17 of the Reigate and Banstead Core Strategy 2014 and in recognition of Section 9 "Promoting Sustainable Transport" in the National Planning Policy Framework

13. Prior to the occupation of a given phase, or within the first planting season following its occupation, all hard and soft landscaping associated with that phase shall be completed in accordance with the details in the Landscape Planning Statement.

In respect of the Sports Hall building, this shall also include the planting of 3 No. Acer Campestre 'Streetwise' trees along the northern boundary as specified on approved drawing 125268-LLB-ZZ-XX-DR-L-4002 P05. These trees shall have an initial planting height of not less than 3.0m.

All new tree planting shall be positioned in accordance with guidelines and advice contained in the current British Standard 5837. Trees in relation to design, demolition and construction-Recommendations.

Any trees, shrubs, plants or turf planted in accordance with this condition which are removed, die or become damaged or become diseased within five years of planting shall be replaced within the next planting season by trees, shrubs of the same size and species.

Reason:

To ensure good arboricultural and landscape practice in the interests of the maintenance of the character and appearance of the area and to comply with policies Pc4 and Cf2 of the Reigate and Banstead Borough Local Plan 2005.

14. Prior to the occupation of a given phase, a verification report carried out by a qualified drainage engineer and demonstrating that the sustainable urban drainage system has been constructed as per the agreed scheme shall be submitted to and approved by the Local Planning Authority.

Reason:

To ensure that the SuDS are adequately planned, delivered and maintained and that the development is served by an adequate and approved means of drainage to comply with Policy Ut4 of the Reigate and Banstead Borough Local Plan 2005, Policy CS10 of the Core Strategy 2014 and the requirements of non-statutory technical standards.

15. Prior to the occupation of each phase, details of any plant or machinery, including fume extraction, ventilation and air conditioning, which may be required on that phase by reason of granting this permission, shall be submitted to the Local Planning Authority.

Any extraction equipment installed to the teaching block shall be designed to accord with the broad principles and parameters set out in the Ventilation Extraction Statement by Van Zyl & de Villiers Consulting Engineers (dated 20/07/2018).

Any plant, machinery or other extraction and ventilation equipment installed on the buildings shall be maintained thereafter in accordance with the approved details and any manufacturer's recommendations.

Reason:

To ensure that a satisfactory external appearance is achieved of the development and to safeguard the amenities of neighbouring occupiers with regard to Reigate and Banstead Borough Local Plan 2005 policy Cf2.

16. Prior to the occupation of each phase, a scheme for any external lighting associated with that phase, to be installed shall be submitted to the Local Planning Authority. Such a scheme shall accord with the broad principles set out in the External Lighting Report by Van Zyl & de Villiers Consulting Engineers (dated 20/07/2018).

Reason:

To ensure that a satisfactory external appearance is achieved of the development and to safeguard the amenities of neighbouring occupiers with regard to Reigate and Banstead Borough Local Plan 2005 policy Cf2.

17. Prior to the occupation of each phase, a remediation validation report for the phase shall be submitted to and approved in writing by the Local Planning Authority. The report shall detail evidence of the remediation, the effectiveness of the remediation carried out and the results of post remediation works, in accordance with the approved remediation method statement and any addenda thereto, so as to enable future interested parties, including regulators, to have a single record of the remediation undertaken at the site. Should specific ground gas mitigation measures be required to be incorporated into a development the testing and verification of

such systems should be in accordance with CIRIA C735 guidance document entitled 'Good practice on the testing and verification of protection systems for buildings against hazardous ground gases'.

Reason:

To demonstrate the effectiveness of remediation works and demonstrate that the proposed development will not cause harm to human health or pollution of controlled waters with regard to Reigate and Banstead Borough Council Local Plan 2005 Policy and the NPPF.

18. Contamination not previously identified by the site investigation, but subsequently found to be present at the site, shall be reported to the Local Planning Authority as soon as is practicable. If deemed necessary by the Local Planning Authority, development shall cease on site until an addendum to the remediation method statement detailing how the unsuspected contamination is to be dealt with, has been submitted in writing to the Local Planning Authority. The remediation method statement is subject to the written approval of the Local Planning Authority and any additional requirements that it may specify.

Reason:

To ensure that the proposed development will not cause harm to human health or pollution of controlled waters with regard to Policy CS10 of the Reigate and Banstead Core Strategy 2014 and the NPPF.

19. Piling or any other foundation designs using penetrative methods shall not be permitted other than with the express consent of the Local Planning Authority which may be given for those parts of the site where it has been demonstrated that there is no resultant unacceptable risk to groundwater. The development shall be carried out in accordance with any approved details.

Reason:

To ensure that the proposed development will not cause harm to human health or pollution of controlled waters with regard to Policy CS10 of the Reigate and Banstead Core Strategy 2014 and the NPPF.

20. The Sports Hall hereby approved shall only be used between the hours of 7am and 10pm Mondays to Fridays, 9am and 10pm on Saturdays and 9am and 6pm on Sundays.

In accordance with the Acoustic Strategy Report (ref PC-18-0130-RP2RevA), all external doors to the Sports Hall shall remain closed during use of the Sports Hall, except in the event of an emergency.

Reason:

To manage the intensity and timing of use in order to safeguard the amenities of neighbours with regard to policy Cf2 of the Reigate and Banstead Borough Local Plan 2005.

21. Within six months of the occupation of the teaching block hereby approved, facilities for the secure, accessible and covered cycle parking for a minimum of 115 bicycles shall be provided on-site in accordance with a scheme to be submitted to and approved in writing by the Local Planning Authority.



Thereafter, the cycling parking shall be provided, retained and maintained in perpetuity in accordance with the approved details to the satisfaction of the Local Planning Authority.

Reason:

To ensure that the development would promote sustainable transport choices with regard to Policy CS17 of the Reigate and Banstead Core Strategy 2014 and in recognition of Section 9 "Promoting Sustainable Transport" in the National Planning Policy Framework.

22. Within six months of the occupation of the teaching block hereby approved, the revised car parking and turning area shall be laid out within the site in accordance with the approved plans numbered (125268 LLB ZZ L 4018 Rev P01 and 4002 Rev P05) to provide for 73 vehicle parking spaces and for vehicles to turn so that they may enter and leave the site in forward gear.

Thereafter the parking/turning areas shall be retained and maintained for their designated purpose.

Reason:

In order that the development should not prejudice highway safety nor cause inconvenience to other highway users to satisfy policies Mo5, Mo6 and Mo7 of the Reigate and Banstead Borough Local Plan 2005 and the objectives of the NPPF.

23. Within six months of the occupation of the teaching block hereby approved, a final certificate demonstrating that BREEAM 'Very Good' rating is achieved for this development shall be submitted to and approved by the Local Planning Authority.

Reason:

To ensure that the development is constructed to appropriate sustainability standards with regard to Policy CS11 of the Reigate and Banstead Core Strategy 2014.

## INFORMATIVES

1. Your attention is drawn to the safety benefits of installing sprinkler systems as an integral part of new development. Further information is available at [www.firesprinklers.info](http://www.firesprinklers.info).
2. The applicant is encouraged to provide renewable technology within the development as part of meeting the BREEAM Very Good standard in order to reduce greenhouse gas emissions.
3. The school is reminded that the travel plan should be submitted through MODESHIFT STARS through the following link <https://modeshiftstars.org>.
4. You are advised that the Council will expect the following measures to be taken during any building operations to control noise, pollution and parking:
  - (a) Work that is audible beyond the site boundary should only be carried out between 08:00hrs to 18:00hrs Monday to Friday, 08:00hrs to 13:00hrs Saturday and not at all on Sundays or any Public and/or Bank Holidays;

- (b) The quietest available items of plant and machinery should be used on site. Where permanently sited equipment such as generators are necessary, they should be enclosed to reduce noise levels;
- (c) Deliveries should only be received within the hours detailed in (a) above;
- (d) Adequate steps should be taken to prevent dust-causing nuisance beyond the site boundary. Such uses include the use of hoses to damp down stockpiles of materials, which are likely to generate airborne dust, to damp down during stone/slab cutting; and the use of bowsers and wheel washes;
- (e) There should be no burning on site;
- (f) Only minimal security lighting should be used outside the hours stated above; and
- (g) Building materials and machinery should not be stored on the highway and contractors' vehicles should be parked with care so as not to cause an obstruction or block visibility on the highway.

Further details of these noise and pollution measures can be obtained from the Council's Environmental Health Services Unit. In order to meet these requirements and to promote good neighbourliness, the Council recommends that this site is registered with the Considerate Constructors Scheme - [www.ccscheme.org.uk/index.php/site-registration](http://www.ccscheme.org.uk/index.php/site-registration).

- 5. Notwithstanding any permission granted under the Planning Acts, no signs, devices or other apparatus may be erected within the limits of the highway without the express approval of the Highway Authority. It is not the policy of the Highway Authority to approve the erection of signs or other devices of a non-statutory nature within the limits of the highway.
- 6. The permission hereby granted shall not be construed as authority to carry out any works on the highway or any works that may affect a drainage channel/culvert or water course. The applicant is advised that a permit and, potentially, a Section 278 agreement must be obtained from the Highway Authority before any works are carried out on any footway, footpath, carriageway, verge or other land forming part of the highway. All works on the highway will require a permit and an application will need to be submitted to the County Council's Street Works Team up to 3 months in advance of the intended start date, depending on the scale of the works proposed and the classification of the road. Please see <http://www.surreycc.gov.uk/roads-and-transport/road-permits-and-licences/the-traffic-management-permit-scheme>. The applicant is also advised that Consent may be required under Section 23 of the Land Drainage Act 1991. Please see [www.surreycc.gov.uk/people-and-community/emergency-planning-and-community-safety/flooding-advice](http://www.surreycc.gov.uk/people-and-community/emergency-planning-and-community-safety/flooding-advice).
- 7. The permission hereby granted shall not be construed as authority to carry out any works on the highway. The applicant is advised that prior approval must be obtained from the Highway Authority before any works are carried out on any footway, footpath, carriageway, or verge to form or modify a vehicle crossover or to install dropped kerbs. Please see: [www.surreycc.gov.uk/roads-and-transport/road-permits-and-licences/vehicle-crossovers-or-dropped-kerbs](http://www.surreycc.gov.uk/roads-and-transport/road-permits-and-licences/vehicle-crossovers-or-dropped-kerbs).
- 8. The developer is reminded that it is an offence to allow materials to be carried from the site and deposited on or damage the highway from uncleaned wheels or badly loaded vehicles. The Highway Authority will seek, wherever possible, to recover any

expenses incurred in clearing, cleaning or repairing highway surfaces and prosecutes persistent offenders. (Highways Act 1980 Sections 131, 148, 149).

9. The developer is advised that as part of the detailed design of the highway works required by the above conditions, the County Highway Authority may require necessary accommodation works to street lights, road signs, road markings, highway drainage, surface covers, street trees, highway verges, highway surfaces, surface edge restraints and any other street furniture/equipment.
10. The use of a suitably qualified arboricultural consultant is essential to provide acceptable supervision and monitoring in respect of the arboricultural issues in respect of the above condition. All works shall comply with the recommendations and guidelines contained within British Standard 5837.
11. The use of landscape/arboricultural consultant is considered essential to provide acceptable submissions in respect of the above relevant conditions. Replacement planting of trees and shrubs shall be in keeping with the character and appearance of the locality and shall have a strong native or indigenous influence, suitable and appropriate cultivars of native species will be acceptable. There is an opportunity to incorporate structural landscape trees into the scheme to provide for future amenity, biodiversity, wildlife habitat and long term continued structural tree cover in this locality. It is expected that the replacement structural landscape trees will be of semi Advanced Nursery Stock sizes with initial planting heights of not less than 4.5m with girth measurements at 1m above ground level in excess of 16/18cm.
12. The use of a landscape/arboricultural consultant is considered essential to provide acceptable submissions in respect of the above relevant condition. The planting of trees and shrubs shall be in keeping with the character and appearance of the locality.

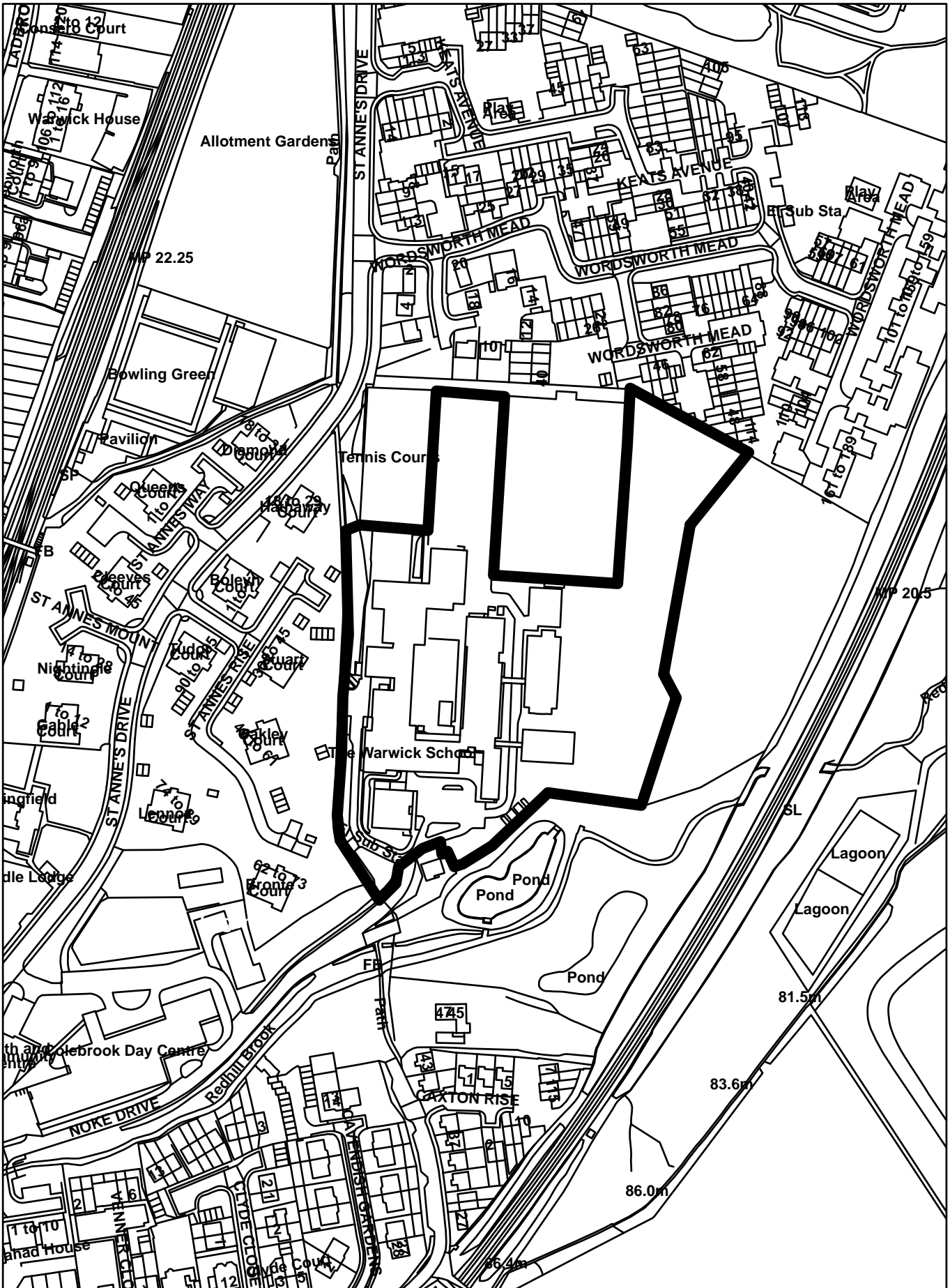
## **REASON FOR PERMISSION**

The development hereby permitted has been assessed against development plan policies CS1, CS3, CS4, CS5, CS8, CS10, CS11, CS12, CS17, Pc2C, Pc2G, Pc4, Pc8, Co1, Cf1, Cf2, Re11, Mo4, Mo5, Mo6, Mo7, Mo13 and Ut4 and material considerations, including third party representations. It has been concluded that the development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.

## **Proactive and Positive Statements**

The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.

# 18/01637/F - The Warwick School, Noke Drive, Redhill





Key:

- 1 Existing Playing Fields / Sports Pitches. No works proposed to this area as part of development.
- 2 Existing All Weather Pitches, No works proposed to this facility as part of development.
- 3 Existing MUGA / Games Court. No works proposed to this facility as part of development.
- 4 Existing School Building retained.
- 5 Existing School Pond and Woodland Ecology Area. No works proposed in this area as part of the development.
- 6 Existing Caretakers House and Garden. Area not included within scope of development.
- 7 Proposed New Sports Hall Block.
- 8 Proposed New Teaching Block.
- 9 Existing Site Entrance Gates (No works proposed).
- 10 Existing Car Park. Area extended following demolition of existing buildings to provide 64 car parking spaces.
- 11 New Visitor and DDA car park providing 8 spaces and required vehicle turning.
- 12 New Main Bin Store Enclosure with 1.8m high timber palisade fence (30 sq.m), and drainage and water provision.
- 13 Secondary Bin Store with 1.8m high timber palisade fence (17 sq.m), and drainage and water provision.
- 14 Existing External Canopy retained. Surfacing to be replaced beneath.
- 15 Brick Retaining Wall. Circa 3m high.
- 16 Gabion Steps / Wall. 3 tiers 300mm high by 500mm deep.
- 17 Areas of Reinstated Amenity Grassland. Following demolition areas to be covered by min 350mm subsoil, 150mm topsoil and turf.
- 18 New Pedestrian Access Gate. 1.8m high manual weld mesh gate.
- 19 New Vehicle Access Gate. 1.8m high manual weld mesh gate.
- 20 New 1.8m high secure mesh fence.
- 21 New Pedestrian Crossing.
- 22 New External Steps. To provide pedestrian access from changing rooms to MUGA.
- 23 Existing MUGA Pedestrian Gate moved to top of new steps.
- 24 Gabion Retaining Wall. Circa 1m high to allow for a pedestrian access path along building providing level access from Reception to MUGA.
- 25 Existing Pupil Access Gate retained. No works proposed.
- 26 Grass Reinforcement Mat.
- 27 Existing Sub Station. Potential requirement for minor upgrading of facilities subject to UKPN consultation.
- 28 3 No. Proposed Acer campestra 'Streetwise' planted adjacent to northern boundary to provide screening of Sports Hall from properties to north of site.



Legend:

- Site Boundary
- Development Area  
*The proposal does not include any proposed works outside of this highlighted area other than standard remedial or making good associated with construction activities.*
- Ownership Boundary
- Existing Building Retained.
- Proposed Building
- Concrete Flag Paving
- Vehicular Asphalt
- Pedestrian Asphalt
- Retaining Walls
- Gabion Steps / Wall
- Existing External Canopy
- Bin Store (Timber Palisade Enclosure)
- Existing Cycle Shelters
- Amenity Grass
- Shrub Planting
- Existing Tree Retained
- Proposed Tree

Revision Date	Revision Description	Issued By
10/09/18	P02 - Additional trees added to northern boundary	AC
03/09/18	P04 - Trees repositioned, and updated to CP status	SH
22/08/18	P03 - Secure fence line completed. gabion steps removed	SH
12/07/18	P02 - Scale changed to 1:500 for planning	AC

Client: Lloyd Bore Ltd - Landscape Architect  
 Project: The Warwick School  
 Noke Drive, Redhill, RH1 4AD  
 Drawing: Annotated Site Plan

Drawing Number	Revision
125268-LB-ZZ-XX-DR-L-4002	P05

Scale	First Issue	Rev	Drawn	Check
1:500	04/05/18	AD	AC	SH

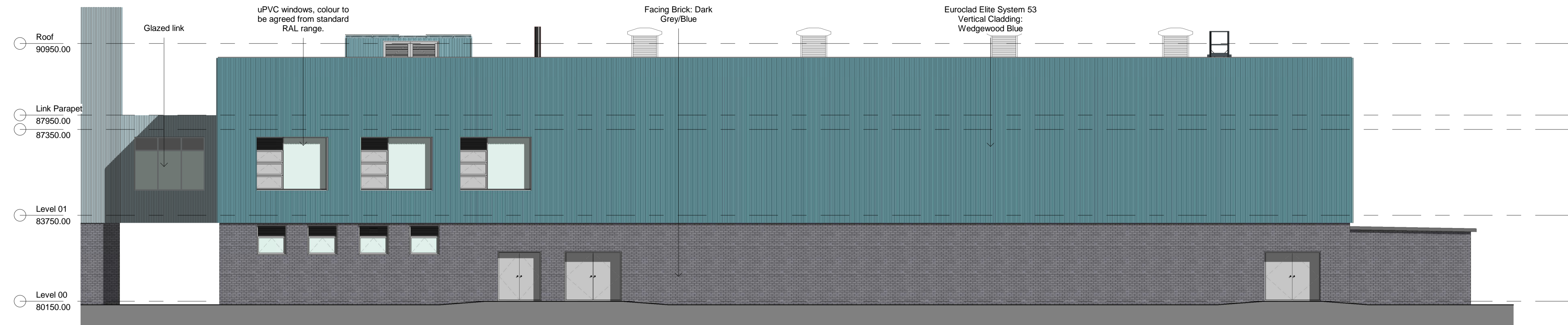
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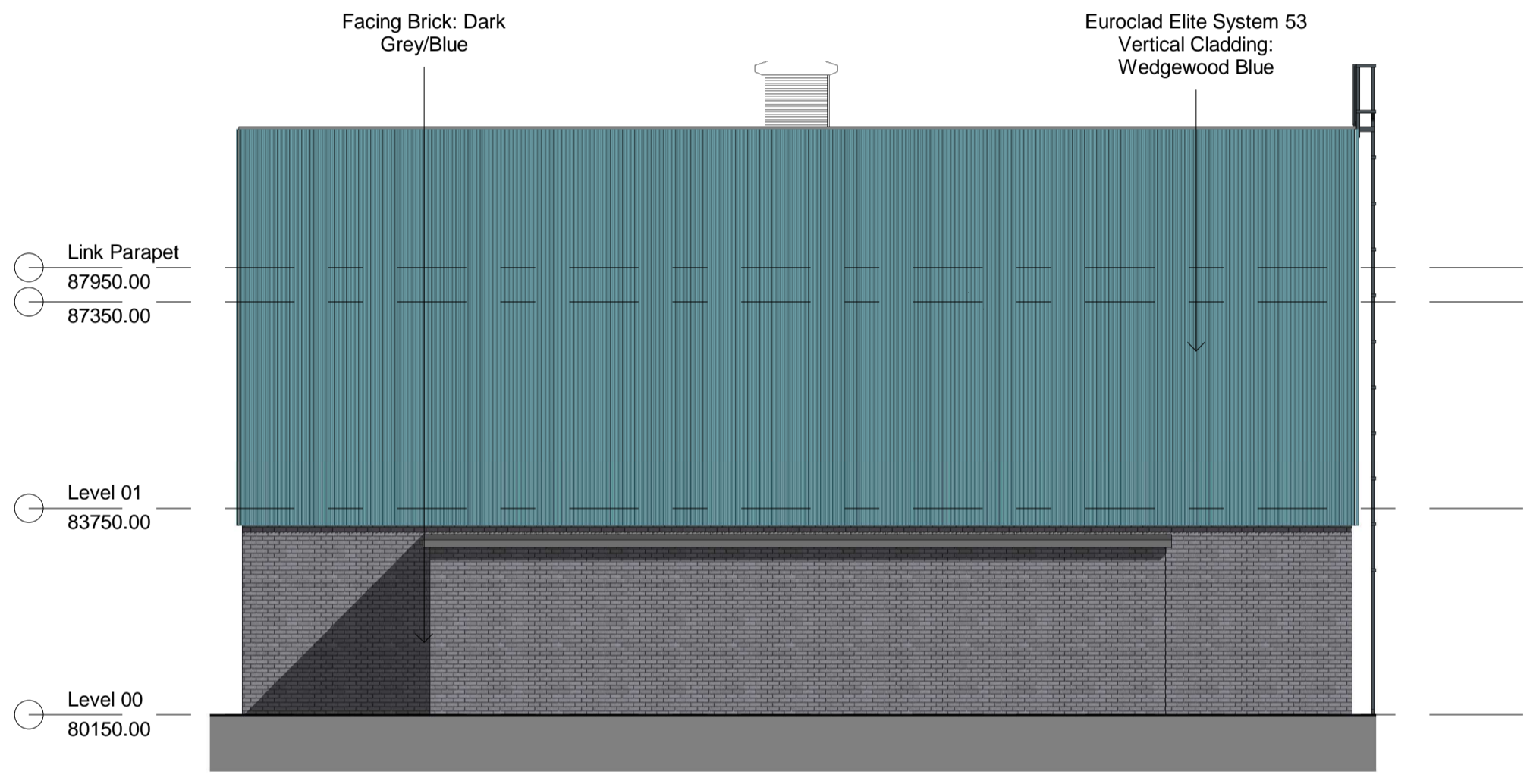
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No.	Date	DRN	CHK	Description
P01	01.06.18	MHY	MH	DRAFT I/TT ISSUE
P02	06.06.18	MHY	MH	I/TT ISSUE
C02	20.07.18	HH	MH	PLANNING
C03	28.07.18	HH	MH	REVISED PLANNING ISSUE
C04	01.08.18	HH	MH	REVISED PLANNING ISSUE



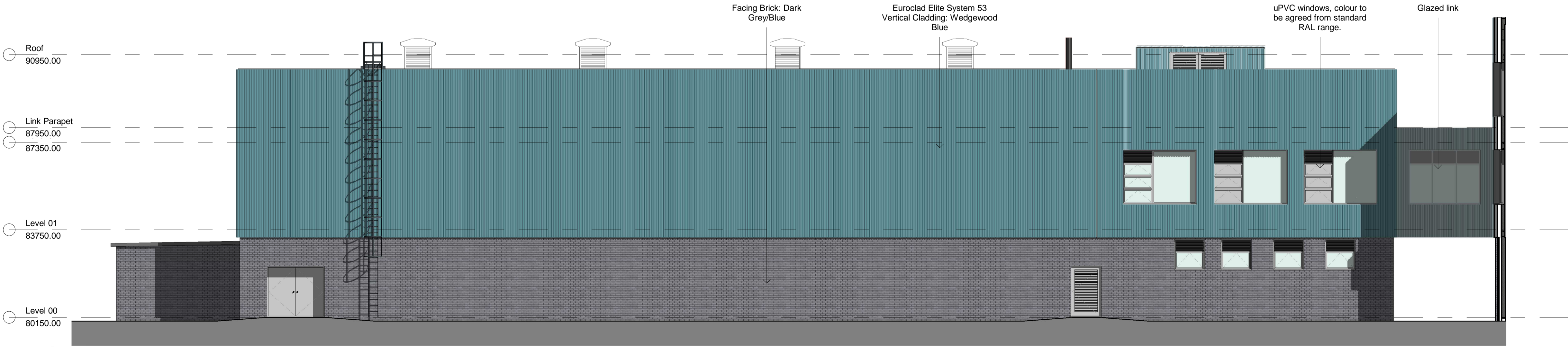
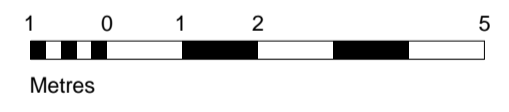
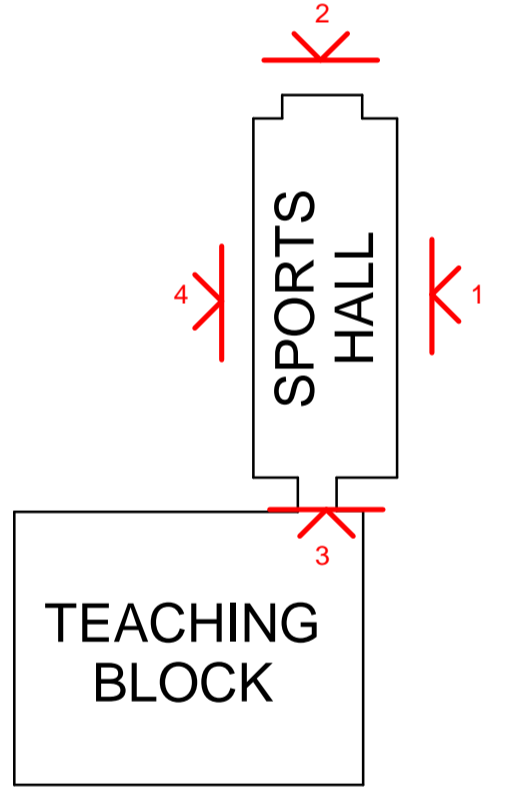
**1** East Sports Hall  
1 : 100



**2** North Sports Hall  
1 : 100



**3** South Sports Hall  
1 : 100



**4** West Sports Hall  
1 : 100

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Project Name  
**The Warwick School**

Client  
**Kier Construction - Guildford Office**

Date of Issue  
**01/08/2018**

Scale  
**As A1**

Issue Status  
**PLANNING**

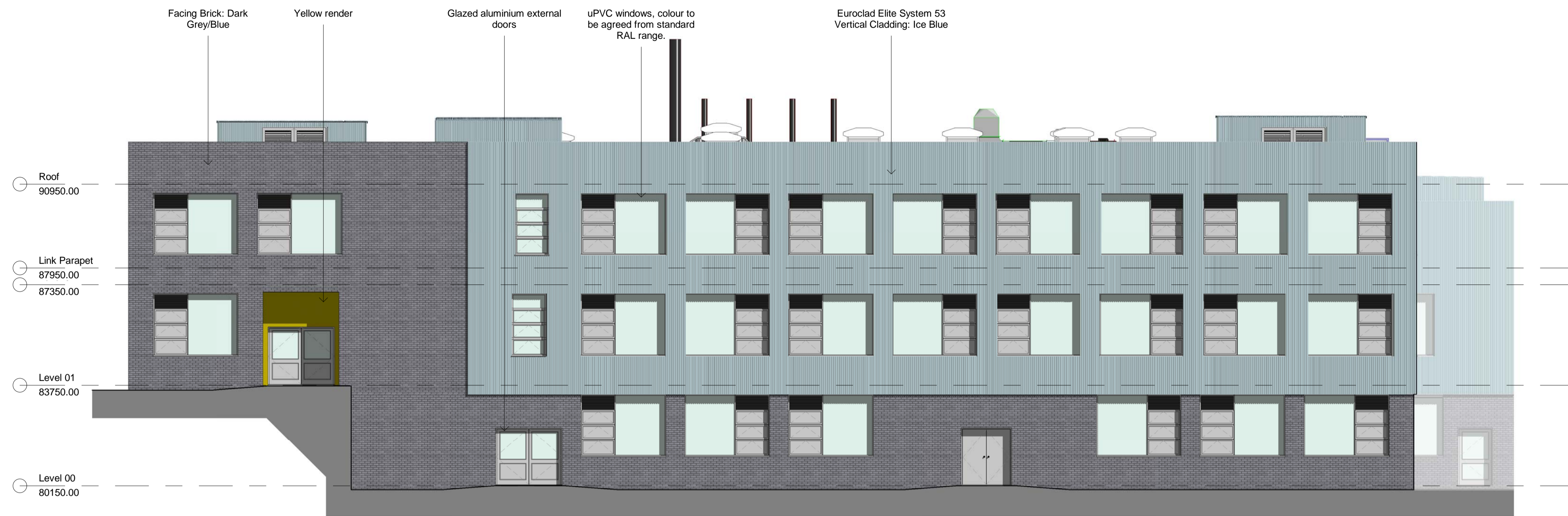
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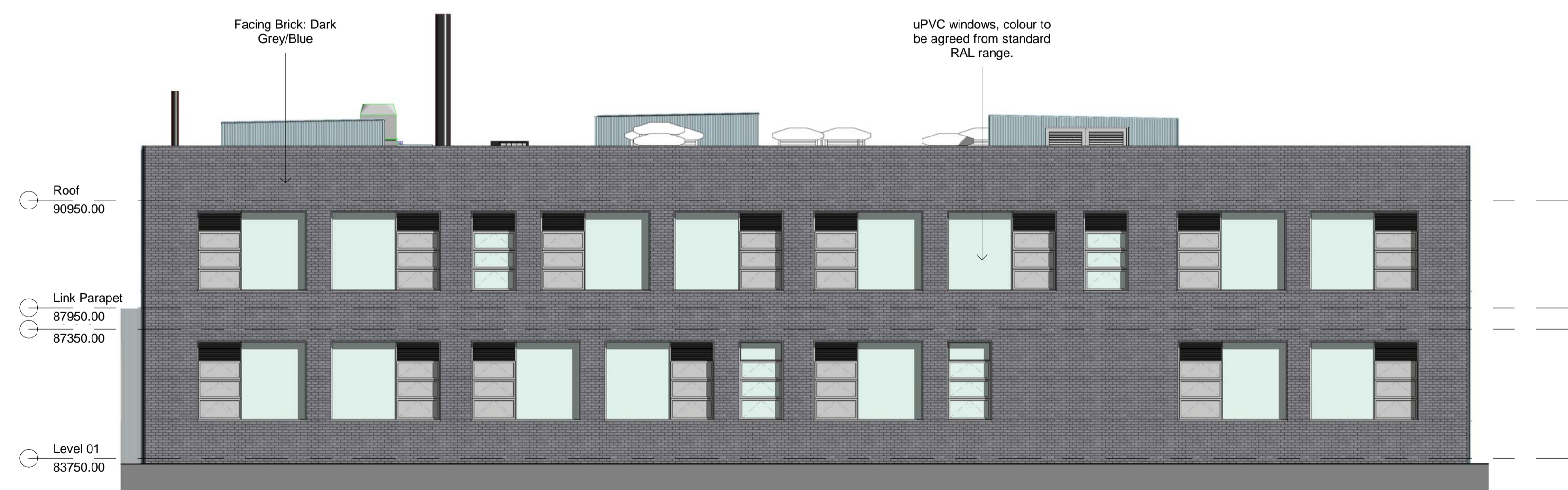
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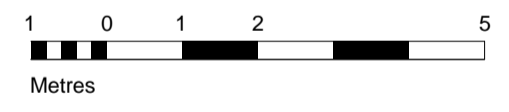
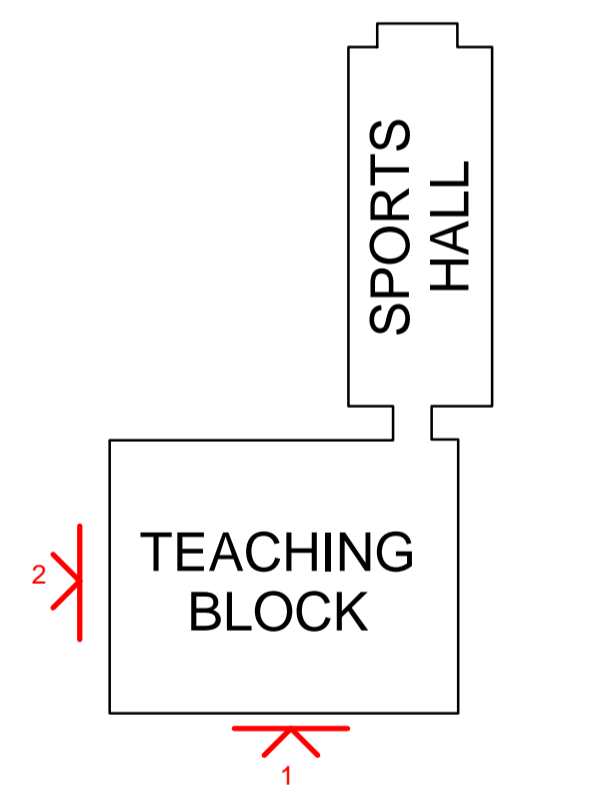
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P02	06.06.18	MHY	MH	ITT ISSUE
C02	20.07.18	HH	MH	PLANNING
C03	28.07.18	HH	MH	REVISED PLANNING ISSUE
C04	01.08.18	HH	MH	REVISED PLANNING ISSUE



**1** South Teaching Block  
1 : 100



**2** West Teaching Block  
1 : 100



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Project Name  
**The Warwick School**

Client  
**Kier Construction - Guildford Office**

Date of Issue 01/08/2018 Dim + Ctd  
MHY MH

Drawing Title  
**Teaching Block Elevations Sheet 2**

Issue Status **PLANNING** Scale **As A1**

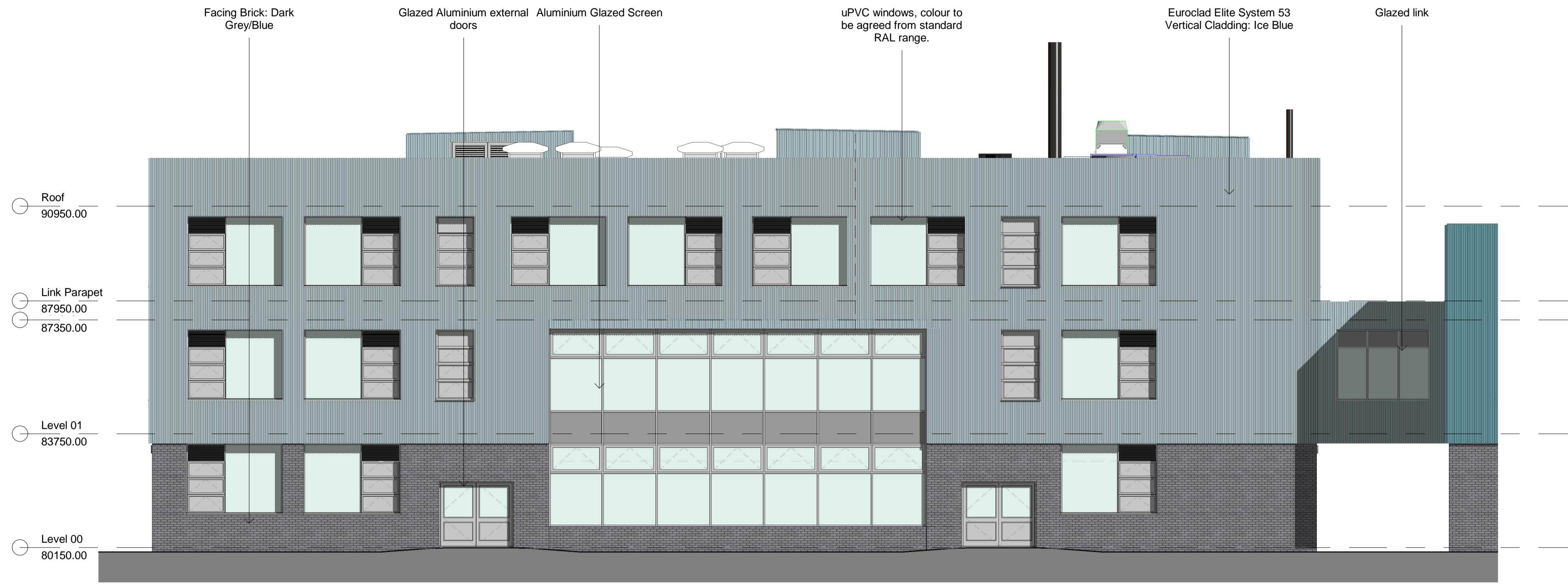
Drawing number indicated@ rev.

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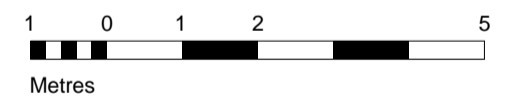
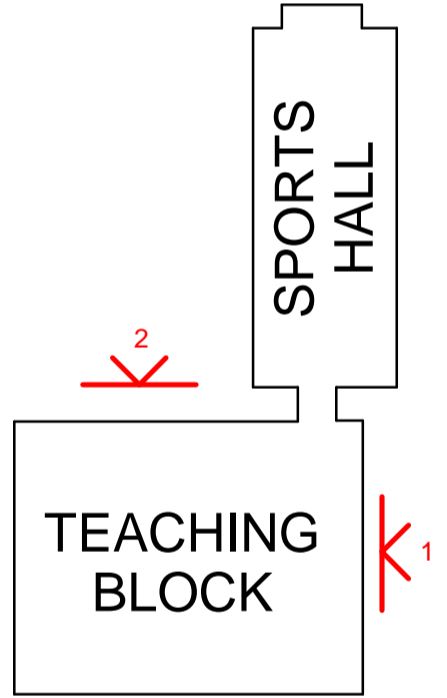
No.	Date	DRN	CHK	Description
P01	01.06.18	MHY	MH	DRAFT I/TT ISSUE
P02	06.06.18	MHY	MH	ITT ISSUE
C02	20.07.18	HH	MH	PLANNING
C03	28.07.18	HH	MH	REVISED PLANNING ISSUE
C04	01.08.18	HH	MH	REVISED PLANNING ISSUE



**1** East Teaching Block  
1 : 100



**2** North Teaching Block  
1 : 100



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email: architects@lee-evans.co.uk website: www.lee-evans.co.uk fax: 01227 819102

Project Name  
**The Warwick School**

Client  
**Kier Construction - Guildford Office**

Date of Issue \_\_\_\_\_ Dm + Ctd  
**01/08/2018** **MHY MH**

Drawing Title  
**Teaching Block Elevations Sheet 1**

Issue Status **PLANNING** Scale **As A1**

Drawing number **indicated@** rev. \_\_\_\_\_

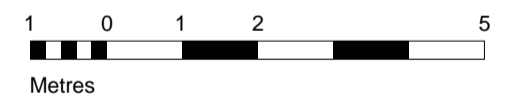
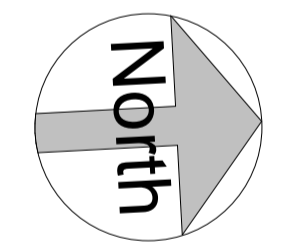
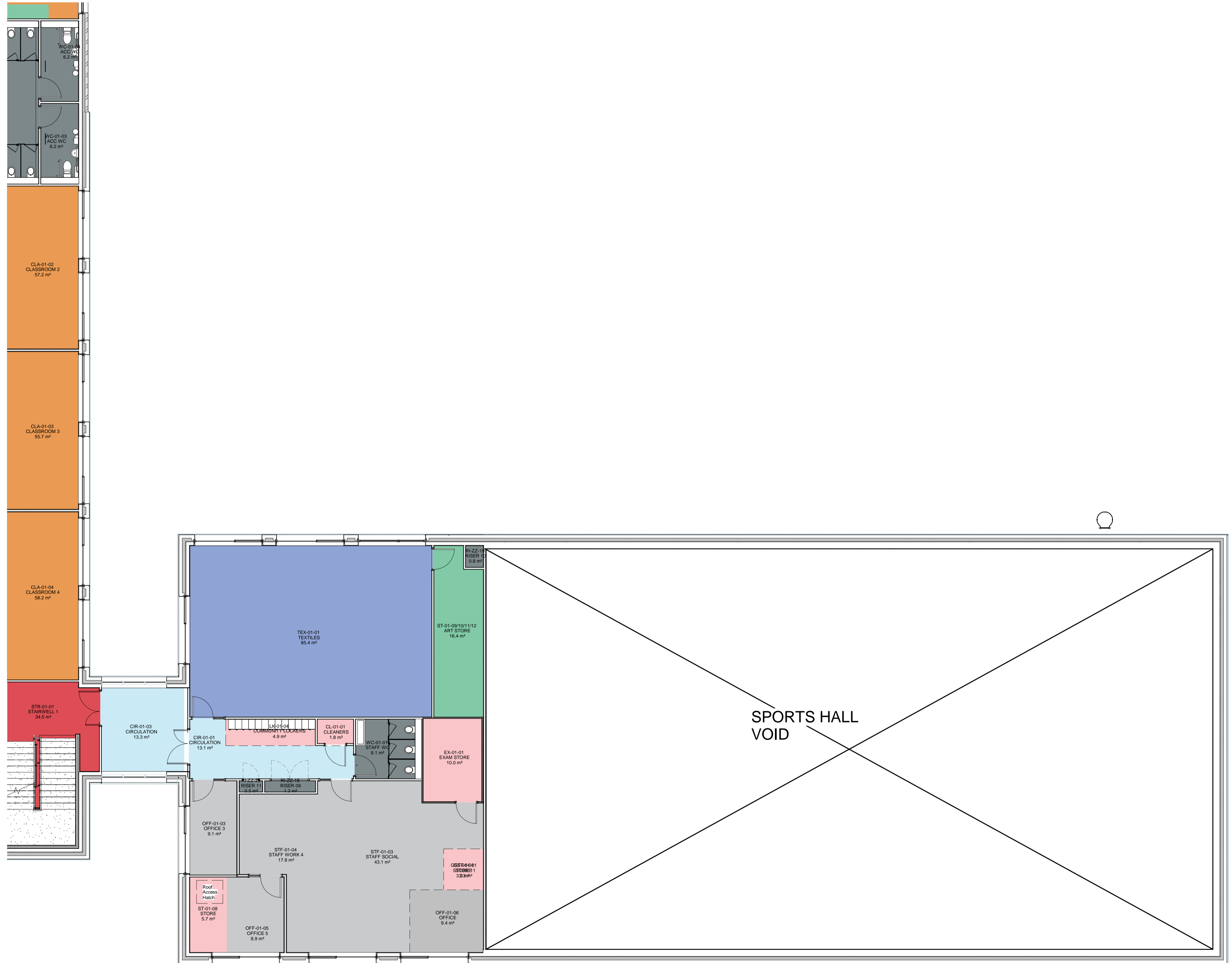
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No.	Date	DRN	CHK	Description
P01	01.06.18	MHY	MH	DRAFT ITT ISSUE
P02	06.06.18	MHY	MH	ITT ISSUE
A	28.06.18	HH	MH	DRAFT CP ISSUE
C02	20.07.18	HH	MH	PLANNING

FOR TEACHING BLOCK PLEASE REFER TO DRAWING 125286-LEP-WS-01-M2-A-0301



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Project Name  
**The Warwick School**

Client  
**Kier Construction - Guilford Office**

Date of Issue \_\_\_\_\_ Dim + Ctd  
**20/07/2018** **MHY MH**

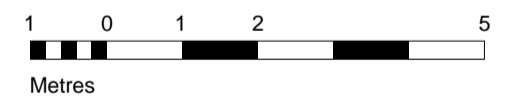
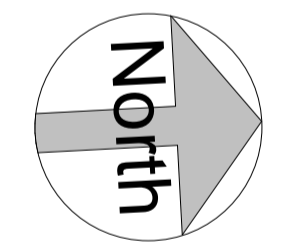
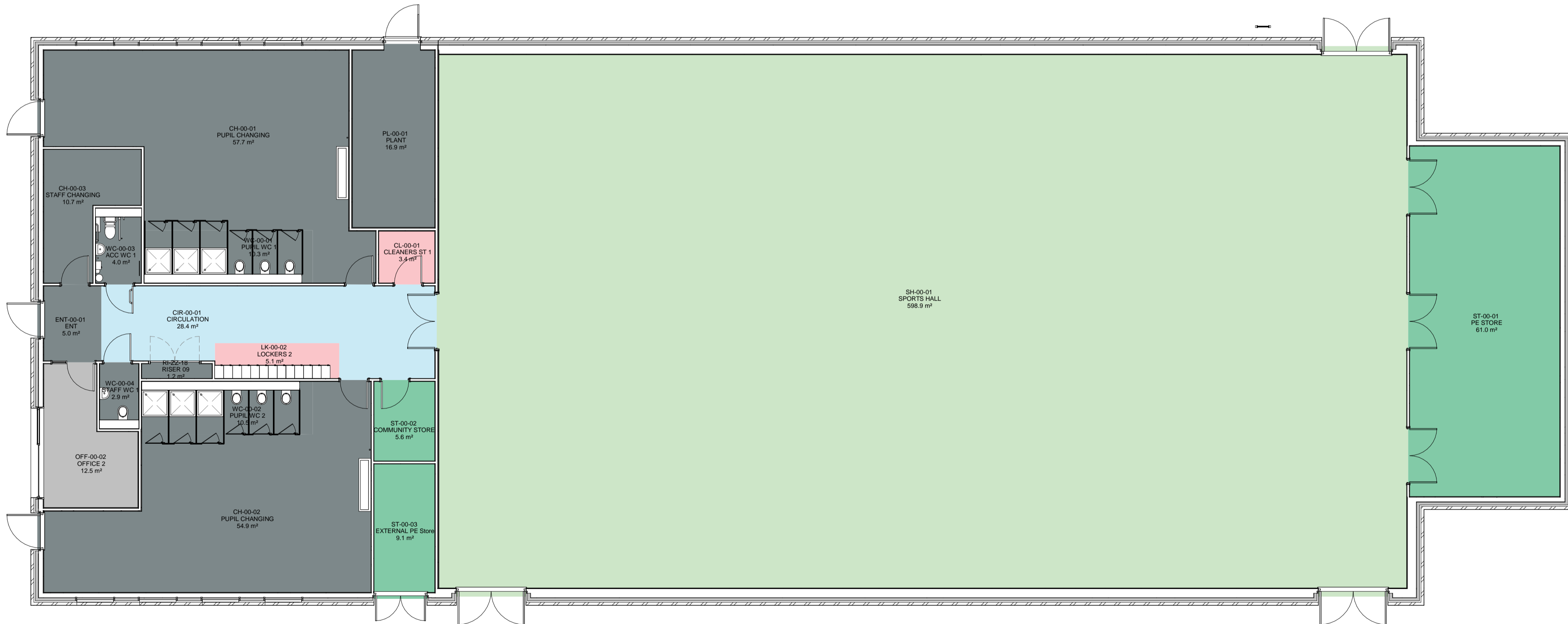
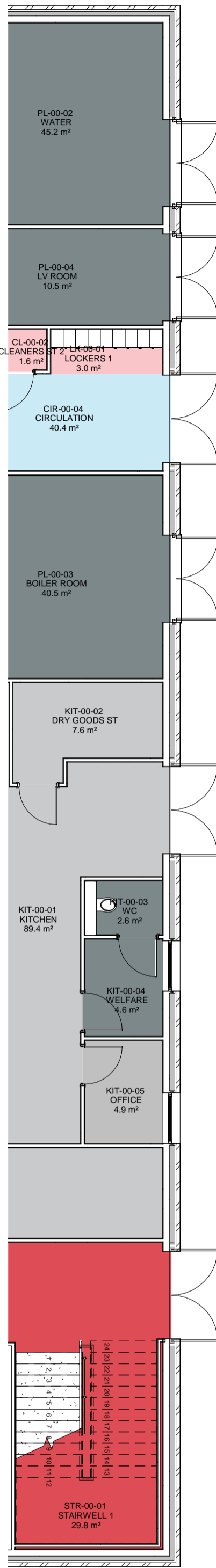
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**PLANNING** **1 : 100@ A1**  
 Drawing number \_\_\_\_\_ rev. \_\_\_\_\_

**125286-LEP-WS-01-M2-A-0305 C02**

**1** Level 01 Sports Hall  
 1 : 100

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P02	06.06.18	MHY	MH	ITT ISSUE
A	28.06.18	HH	MH	DRAFT CP ISSUE
C02	20.07.18	HH	MH	PLANNING

FOR TEACHING BLOCK PLEASE REFER TO DRAWING 125286-LEP-WS-00-M2-A-0300



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Date of Issue 20/07/2018 Dim + Ctd  
MHY MH

Drawing Title  
**Ground Floor Sports Hall**

Issue Status **PLANNING** Scale  
Drawing number 1 : 100@ A1 rev.

125286-LEP-WS-00-M2-A-0304 C02

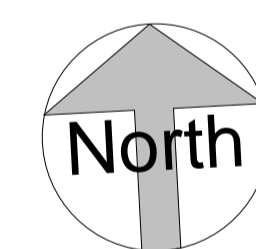
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A	28.06.18	HH	MH	DRAFT CP ISSUE
C02	20.07.18	HH	MH	PLANNING

FOR SPORTS HALL PLEASE REFER TO DRAWING 125286-LEP-WS-01-M2-A-0305



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**The Warwick School**

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Date of Issue 20/07/2018 Dm + Cld  
MHY MH

Drawing Title  
**First Floor Teaching Block**

Issue Status  
**PLANNING** Scale 1 : 100@ A1  
Drawing number rev.

125286-LEP-WS-01-M2-A-0301 C02

**1** Level 01 Teaching Block  
1 : 100



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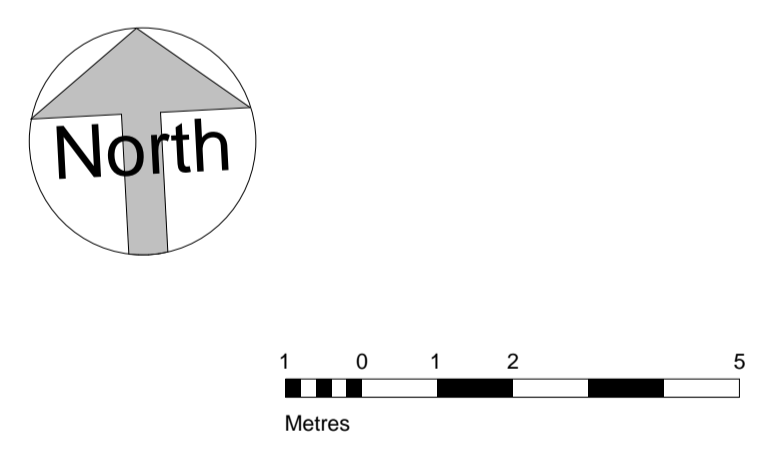
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P02	06.06.18	MHY	MH	ITT ISSUE
A	28.06.18	HH	MH	DRAFT CP ISSUE
C02	20.07.18	HH	MH	PLANNING



4/6

**1** Level 02 Teaching Block  
1 : 100



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Project Name  
**The Warwick School**

Client  
**Kier Construction - Guilford Office**

Date of Issue  
**20/07/2018**

Drawing Title  
**Second Floor Teaching Block**

Issue Status  
**PLANNING**

Drawing number  
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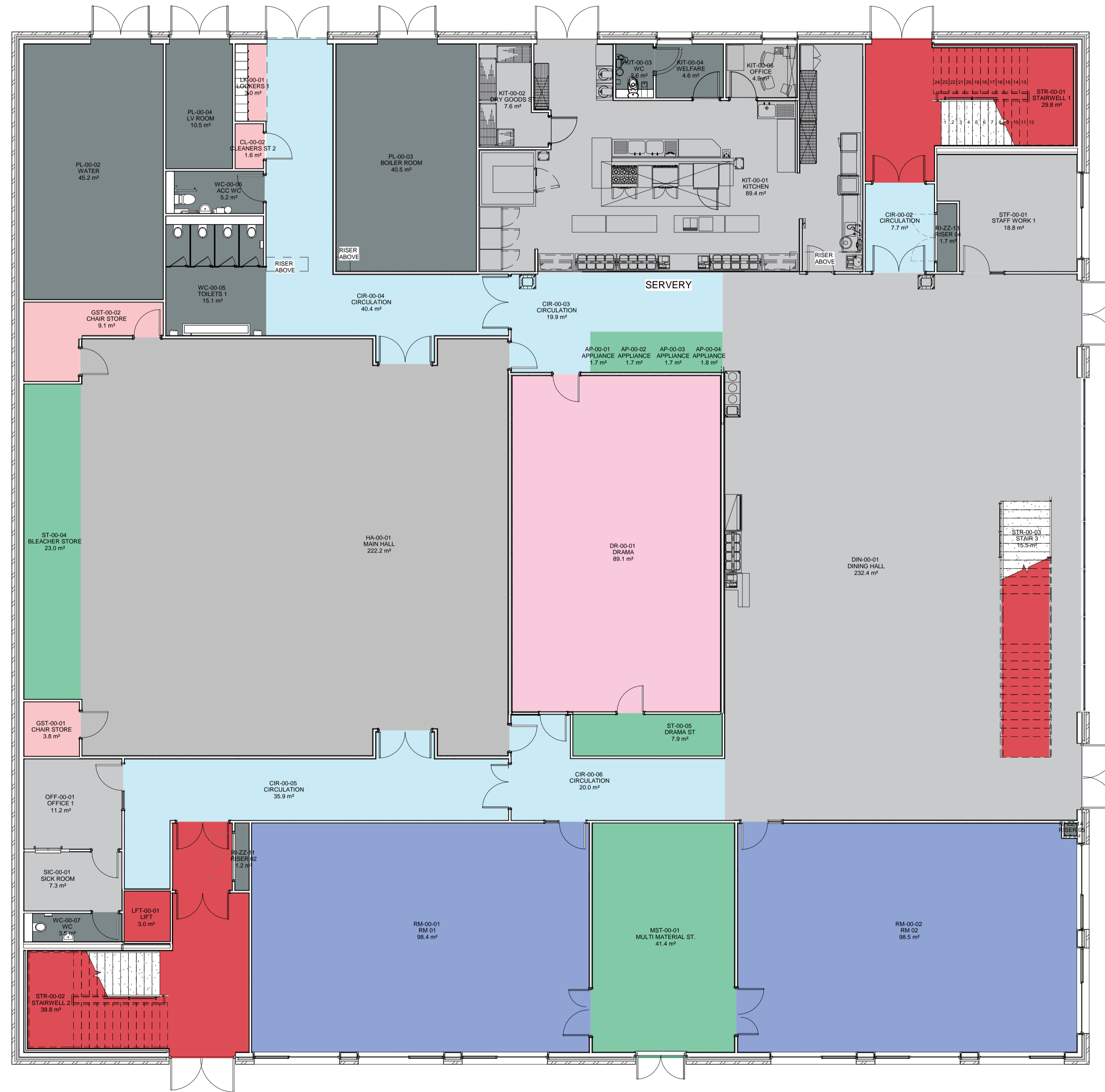
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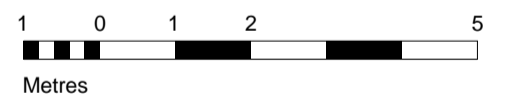
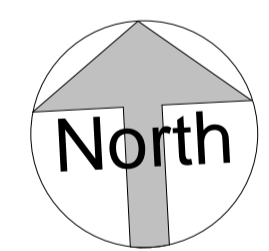
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P02	06.06.18	MHY	MH	ITT ISSUE
A	28.06.18	HH	MH	DRAFT CP ISSUE
C02	20.07.18	HH	MH	PLANNING



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Date of Issue 20/07/2018 Dim + Ctd  
MHY MH

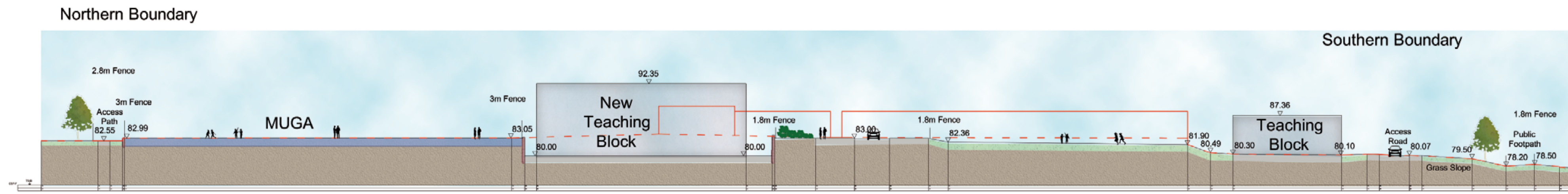
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Issue Status **PLANNING** Scale  
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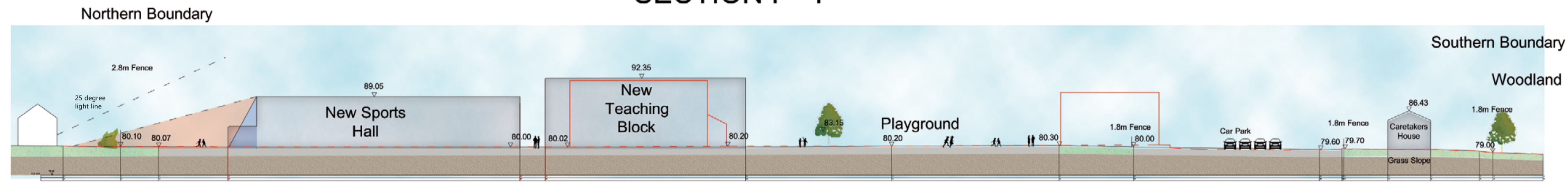
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1 : 100

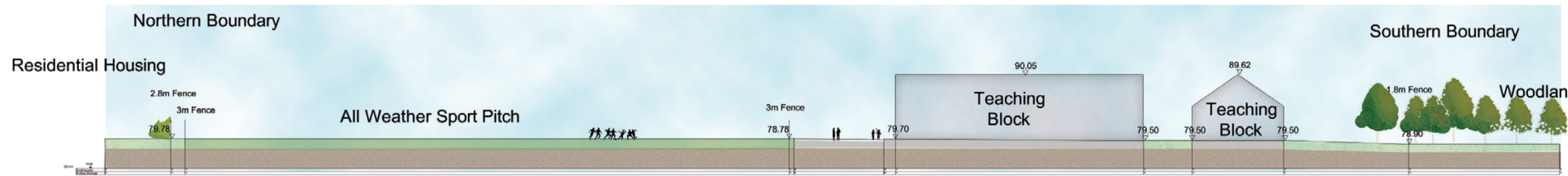




SECTION F - F

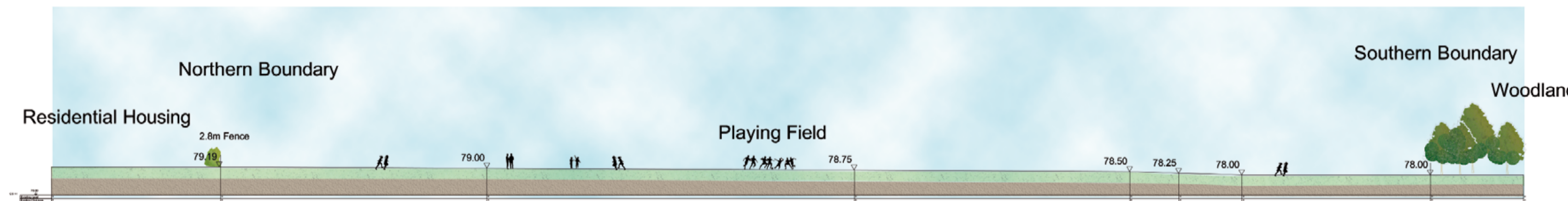


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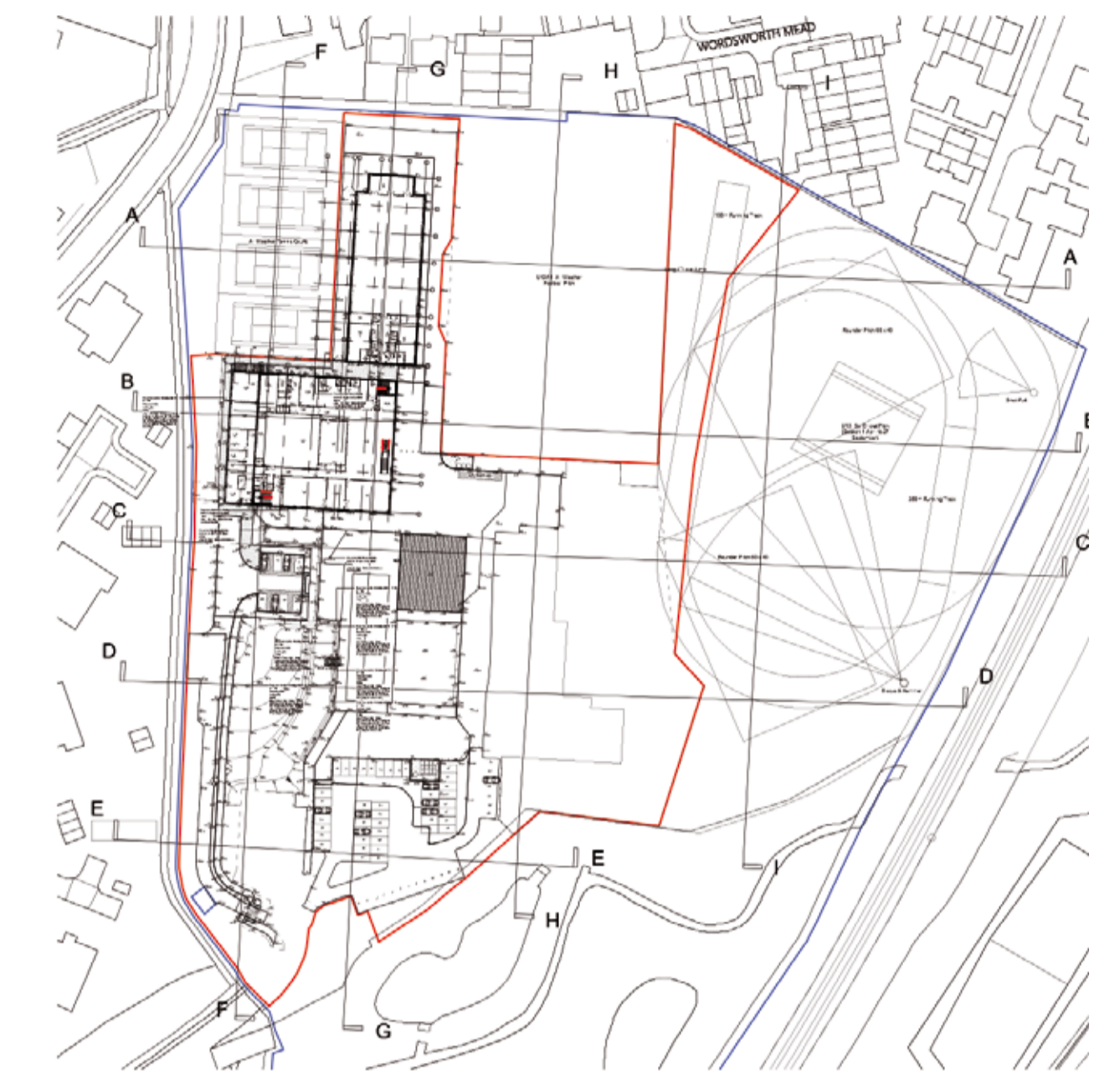
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SECTION H - H



Note: No change to this section.

SECTION I - I



KEY PLAN

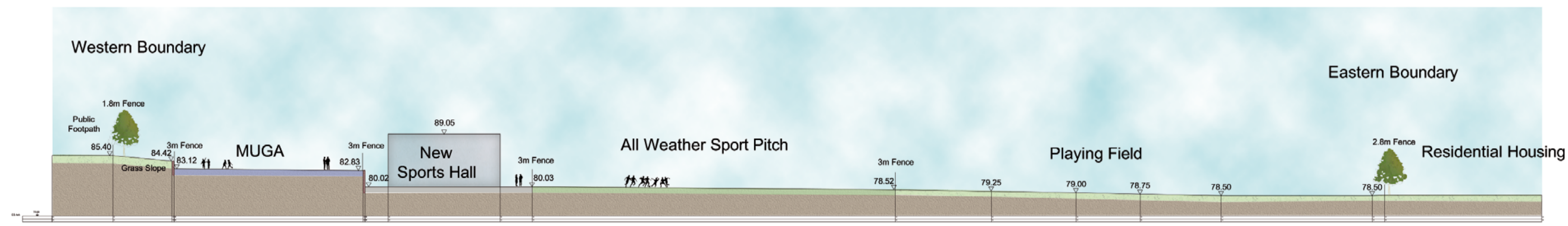
Legend

- - - Existing ground profile
- Existing Building to be demolished
- 
- 

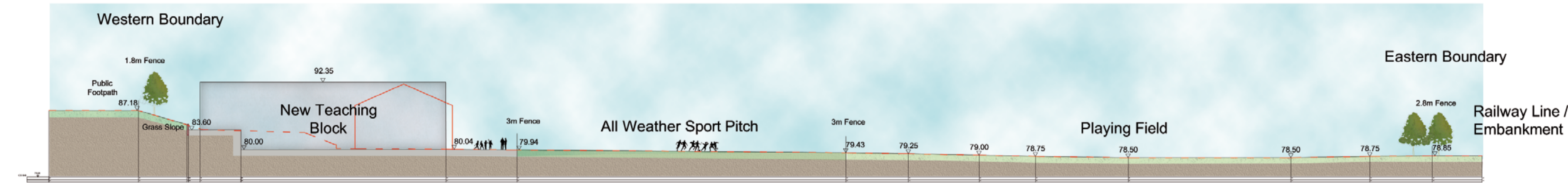
Rev Date	Revision Description	Issue
19/09/18	P05 - 25 degree light line added to section GG	AC
23/08/18	P04 - Section GG extended to include residential property	AC
12/07/18	P03 - Colour Added	AC
4/06/18	P02 - 1:250 scale bar added	AC
19/05/18	Scale changed to 1:250	AC
19/05/18	Project name corrected	AC

Discipline: Lloyd Bore Ltd - Landscape Architect  
 Project: Lloyd Bore Ltd - Landscape Architect  
 The Warwick School  
 Noke Drive, Redhill, RH1 4AD  
 Noke Drive, Redhill, RH1 4A  
 Drawing:  
 Indicative Site Sections  
 Sheet 2 of 2  
 Proposed Site Sections  
 Proposed Site Sections  
 Drawing Number: 125268-LLB-XX-DR-L-3004  
 Revision: P05  
 Scale(s): 1:500@A1, 1:250@A0  
 First Issued: 21/05/18  
 Size: A1  
 Drawn: AC  
 Check: AC  
 Status: PLANNING

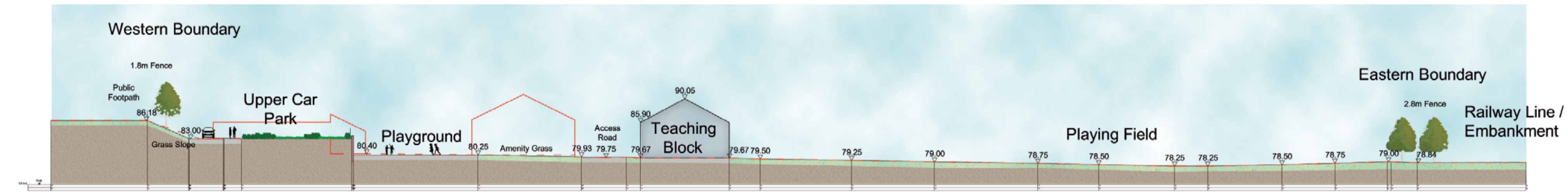




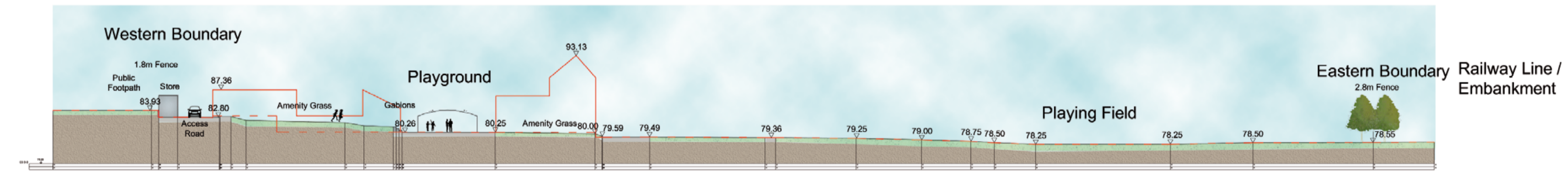
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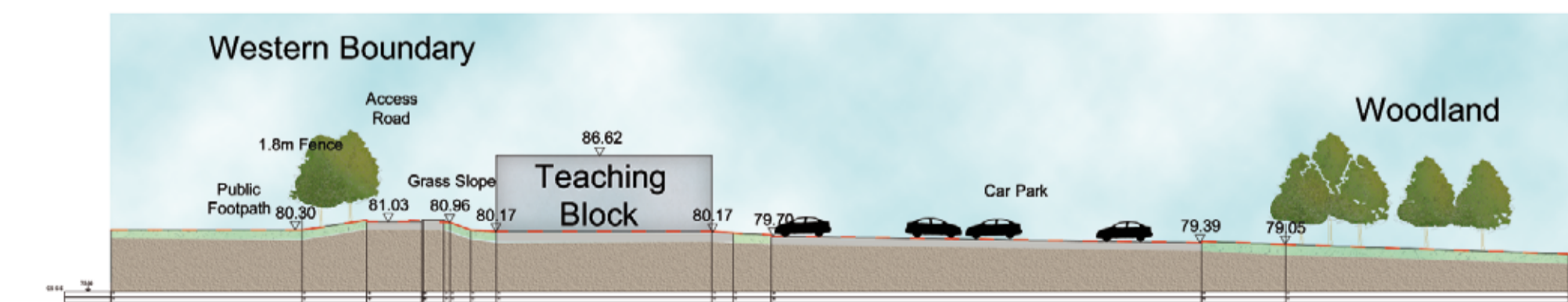
SECTION B - B



SECTION C - C



SECTION D - D



SECTION E - E

Note: No change to this section.



KEY PLAN

Legend

- Existing ground profile
- Existing Building to be demolished

Revision	Date	Revision Description	Issued By
12/07/18		P03 - Colour Added	AC
4/06/18		P02 - 1:250 scale bar added	AC
1/06/18		P01 - Initial version	AC

Discipline: Lloyd Bore Ltd - Landscape Architect

Project: The Warwick School  
Noke Drive, Redhill, RH1 4AD

Drawing: Indicative Site Sections  
Sheet 1 of 2  
Proposed Site Sections

Drawing Number	Revision
125268-LLB-XX-XX-DR-L-3003	P03

Scale(s)	First Issued	Size	Drawn	Check
1:500@A1	21/05/18	A1	AC	AC
1:250@A0				

Status: PLANNING




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# Agenda Item 6

Planning Committee  
31 October 2018

Agenda Item: 6  
18/01180/F

 <b>Reigate &amp; Banstead</b> BOROUGH COUNCIL Banstead   Horley   Redhill   Reigate	<b>TO:</b>	PLANNING COMMITTEE
	<b>DATE:</b>	31 <sup>st</sup> October 2018
	<b>REPORT OF:</b>	HEAD OF PLACES & PLANNING
	<b>AUTHOR:</b>	Billy Clements
	<b>TELEPHONE:</b>	01737 276087
	<b>EMAIL:</b>	billy.clements@reigate-banstead.gov.uk
<b>AGENDA ITEM:</b>	6	<b>WARD:</b> Salfords and Sidlow

<b>APPLICATION NUMBER:</b>	18/01180/F	<b>VALID:</b>	4 <sup>th</sup> June 2018
<b>APPLICANT:</b>	Goya Developments & Hillwood	<b>AGENT:</b>	PRC
<b>LOCATION:</b>	<b>FORMER PHILIPS RESEARCH LABORATORIES SOUTH SITE, CROSSOAK LANE, SALFORDS</b>		
<b>DESCRIPTION:</b>	The redevelopment of the site to include four employment buildings incorporating 5 units for open b1(b), b1(c), b2 and b8 use comprising 15,623sqm GEA with associated parking and landscape planting.		
<b>All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail.</b>			

## SUMMARY

This is a full application for the redevelopment of the site to provide 5 commercial units with associated parking, service yards and landscaping. The units are proposed to be for open B1(b), B1(c), B2 and B8 use.

The site is within a designated Employment Area, being part of the Salfords Industrial Estate. In this regard, the nature, mix and type of development proposed is consistent with policies and strategy in the Local Plan which specifically seek to direct new industrial, storage and distribution uses to such sites. The proposals would support the aim of the Core Strategy which seeks to make best use of existing employment land within industrial areas and the redevelopment would bring back into use a long vacant brownfield site, consistent with the thrust of the NPPF, bringing economic benefits and job creation in the process.

The layout of the site and design of the buildings are considered to be typical of, and appropriate for, a commercial/industrial estate. Whilst the proposed units would be large in scale and relatively tall (13-15m), this is not unusual for industrial/commercial units and the height would not appear unduly out of keeping in the context of large buildings in the surroundings such as the adjacent Titan Travel offices. The layout of the site, including the fact that the buildings would be generously set back from the road enabling the retention and enhancement of existing boundary landscaping, is considered to help ensure that the buildings would not appear unduly dominant and would maintain the presently verdant

character of the adjoining stretches of Bonehurst Road and Cross Oak Lane. The appearance of the units would reflect modern industrial/warehouse accommodation; however, but would be enhanced somewhat by the addition of areas of complementary timber cladding which add interest to the buildings. Overall, the site is considered to be appropriately designed and is not felt to harm the character of the area.

Whilst the buildings would be relatively large, the separation distances to the nearest residential properties are such that the proposals are not considered to be detrimental to residential amenities with respect to overbearing, outlook or daylighting. The application was accompanied by a Noise Impact Assessment which demonstrates that, subject to the inclusion of an acoustic fence along the southern boundary, the likely use, operation and activity associated with the units would not give rise to an unacceptable noise impact or disturbance to neighbouring properties, either during the day or at night. No other adverse neighbour impacts have been identified.

In terms of access and highways, access to the site would be via an existing access to Cross Oak Lane which would be modified as part of the development. This is considered to be acceptable by the County Highway Authority in terms of visibility and geometry. Specific and detailed consideration has been given to the impact of the movements from the proposed development on the Cross Oak Lane/A23 junction and, whilst it is acknowledged that there would be some impact in terms of additional queuing in the AM and PM peaks, the County Highway Authority concludes that this would not be so severe as to warrant refusal. A condition limiting the amount of floorspace on the development to be used for B8 use is proposed, this would prevent the use of the whole site as a distribution centre which may give rise to different highway impacts. The proposals are considered to provide adequate parking provision for the nature and type of development proposed and the levels of parking are supported by evidence from sites drawn from the national TRICS database. With regard to concerns regarding HGV movements and capacity, the scheme would provide a total of 17 designated HGV loading bays; however, it is concluded that there would be adequate additional space and flexibility within the service yards of a number of the units to allow for HGVs to wait in the event that all loading bays were full. A condition is however recommended requiring the developer to implement double yellow lines on both sides of Cross Oak Lane up to the railway bridge in order to prevent vehicles associated with the site (or any other vehicles) parking on the road which may prejudice highway safety. A Travel Plan and Delivery & Servicing Plans are also recommended to be sought through condition to manage potential impacts on the transport and highway network.

The proposals are considered to satisfy the Sequential Test in respect of flood risk and are considered to be otherwise acceptable in flooding terms. No objection is raised by either the Environment Agency or Surrey CC as the Lead Local Flood Authority. A sustainable drainage system is proposed to be secured by condition.

## **RECOMMENDATION(S)**

Subject to the completion of all documentation required to create a planning obligation under Section 106 of the Town and Country Planning Act 1990 (as amended) to secure:

- (i) £3,000 towards a review and implementation of parking restrictions for up to five years past full occupation of the site;
- (ii) £6,150 towards Travel Plan monitoring and auditing
- (iii) £4,000 towards reviewing the rail bridge height restriction east of the site on Cross Oak Lane
- (iv) The Council's legal costs in preparing the agreement;

Planning permission is **GRANTED** subject to conditions.

In the event that a satisfactorily completed obligation is not received by 31 December 2018 or such longer period as may be agreed, the Head of Planning be authorised to refuse permission for the following reason:

The proposal fails to make adequate provision for the monitoring of sustainable travel measures and local highway impacts and therefore could give rise to a situation prejudicial to highway safety or which would fail to promote sustainable travel, contrary to policies Mo5 and Mo7 of the Reigate and Banstead Borough Local Plan 2005 and Policy CS17 of the Reigate and Banstead Core Strategy 2014.

## Consultations:

County Highway Authority: No objection subject to conditions. Comments as follows:

*“The model has been audited by SCC modellers. The model assumes that the development would be used for the proposed mix of development and has been revised according to instructions from our modelling team in such a way that the inputs and assumptions made have all been passed. The model shows that the development would add more traffic such that the junction would be over its theoretical capacity. However no new junctions would be affected. In the AM peak queues on the north and south sides of the A23 Bonehurst Road junction with Cross Oak Lane would increase from respectively 145 metres and 165 metres in 2023 without the development to 163 metres and 175 metres with the development. In the PM peak queues on the north and south sides of the A23 Bonehurst Road junction with Cross Oak Lane would increase from respectively 174metres and 156 metres in 2023 without the development to 201 metres and 178 metres with the development.*

*The queues on Cross Oak Lane will increase but they will not be so long as to reach the railway bridge east of the access. The only junction that would be affected would be the new junction on to the A23 from the Horley North West Sector. However that junction would be affected anyway because queues already form where the junction is proposed to be located. No additional junctions north of the new junction would be affected by the longer queues in either the AM or PM peaks.*

*I have carried out a sensitivity test assuming that the proposed development would comprise 9000m2 of B8 Distribution use. I have assumed the rest of the site would be used as per the proposed B1c and B2 uses. This sensitivity test shows that such a use would not increase queue lengths to such an extent that new highway junctions would be affected.*

*The developer has carried out a parking accumulation survey using site from TRICS to establish whether adequate parking for vehicles including HGV vehicles is being proposed. The data from the developer shows that adequate parking is being proposed for the mix of land use proposed. However to safe guard all highway users against future demand for parking on the highway I have recommended that the developer provide double yellow lines on both sides of Cross Oak Lane between the rail bridge east of the eastern most access and the Cross Oak Lane junction with the A23. In addition I have asked for a contribution of £3000 towards reviewing the parking restrictions on Cross Oak Lane east of the rail bridge should this become necessary after occupation of the development for a period of up to 5 years post full occupation of the proposed development.*

*If the site were used by a B8 distribution company, there is likely to be more demand for parking. However the developer is proposing a quantum of parking spaces that would more than cover the likely demand for car parking therefore leaving unfilled spaces. A proportion of those spaces within the site could be used for parking of HGVs while the driver waits for a loading/unloading bay. This would not displace cars onto the highway because the parking accumulation shows that there would be adequate space within the development to accommodate demand for parking of HGVs and non HGVs.*

*The developer is proposing to alter the existing western most access and to close the eastern most access. Both of these are subject to conditions to be carried out in accordance with plans to be submitted. With respect to the modified access, there is adequate visibility proposed. Turning overlays show that the access would have adequate geometry to accommodate HGVs albeit with some crossing into the opposing traffic lane. In order to prevent parking on Cross Oak Lane I have recommended a revised plan showing details of double yellow lines. These details can be provided as part of highway works under Section 278 of the Highways Act 1980.*

*I have recommended a condition for details of a Construction Transport Management Plan to be submitted. The developer has submitted a travel plan, but this needs to be revised.”*

Contaminated Land Officer: Identifies potential for ground contamination to be present on and/or in close proximity to the site and therefore recommends conditions.

Environment Agency: No objection subject to conditions

Surrey Lead Local Flood Authority: No objection subject to conditions

Surrey County Council Minerals & Waste Policy Team: No comments

Natural England: No comments

Salfords and Sidlow Parish Council: Objects on the basis of four main concerns: a) density of units, no room to accommodate likely number of heavy and light goods vehicle movements on the proposed site; b) traffic that would be generated, including cumulative effects with other developments and capacity of junctions onto the A23 to cope with additional traffic; c) inadequate parking and d) height of the building could be overbearing. Also raises additional concerns regarding noise and disturbance, including night time activities.

Horley Town Council: Objects on the basis that the current transport infrastructure will not support the proposal at both Crossoak Lane and at the junction of Crossoak Lane with the A23, especially when taking into account that the Westvale Park development access road joins into this junction. Supports development of the site but suggests that offices/residential would be more appropriate.

Gatwick Airport: Recommends condition requiring a bird hazard management plan

NATS: No objection with respect to National Air Traffic Services safeguarding criteria

Network Rail: Recommends informative notes regarding practices which the developer must follow, during construction and after completion of works on site, to ensure there would be no prejudice to the operation and safety of the railway.

## **Representations:**

Letters were sent to neighbouring properties on 13<sup>th</sup> June 2018; a site notice was posted 3<sup>rd</sup> July 2018 and the application was advertised in local press on 28<sup>th</sup> June 2018.

One response was received which neither supported or objected to the proposals but expressed pleasure that the proposed site layout includes for retention/relocation of the Polar Theme statue

## **1.0 Site and Character Appraisal**

- 1.1 The site comprises the site of the former Philips Laboratory, situated on the eastern side of the A23 and on the southern end of the urban area of Salfords. The site is cleared, with the majority of the previous buildings having been demolished to slab level back in 2009/10 and the final remaining building (Building J) being demolished earlier this year under a separate demolition prior approval. The site is part of the designated Salfords Industrial Area within the 2005 Borough Local Plan.
- 1.2 The southern boundary of the site marks the transition between the urban area of Salfords and the Metropolitan Green Belt. To the south of the site, is an area of open amenity land/recreational space and a small number of residential properties, all of which are in the Metropolitan Green Belt. The site adjoins the Brighton mainline railway line to the east which is elevated up an embankment. To the north is an office campus of Titan Travel, which is also part of the Salfords Industrial area. Land on the opposite side of the A23 is in the Green Belt also.
- 1.3 The site accesses onto Cross Oak Lane, close to the signalised junction with the A23. To the east, is a restricted height railway bridge. There are trees on most boundaries of the site, notably the boundaries with the A23 and Cross Oak Lane and this is a feature which prevails along much of this stretch of the A23. The northern part of the site is within Flood Zones 2 and 3 according to EA Flood Maps.
- 1.4 As a whole, the application site extends to approximately 3.12ha.

## **2.0 Added Value**

- 2.1 Improvements secured at the pre-application stage: Pre-application advice was sought prior to submission of the application. Advice was given in respect of the sensitivity of the highways issues and the need for this to be robustly evidenced and justified in any application. The proximity of the buildings and car parking to the road frontage was identified as a concern and more generous separation and landscaping was encouraged. No in principle objection was raised given the site is a designated employment area.
- 2.2 Improvements secured during the course of the application: Further amendments to the layout to increase the set back of Unit 1 from the corner of A23/Crossoak Lane and to set back areas of parking from Crossoak Lane with associated minor reduction in amount of floorspace (c.208sqm).
- 2.3 Further improvements to be secured through planning conditions or legal agreement: Various conditions are recommended regarding highways and access works, including the implementation of restrictions along Crossoak Lane. Conditions are also recommended to secure implementation of the landscaping, restoration and relocation of the Polar Theme statue and compliance with noise mitigation

recommendations. A legal agreement is proposed to secure contributions towards travel plan monitoring, parking review and bridge signage.

### **3.0 Relevant Planning and Enforcement History**

3.1 There is extensive planning history associated with the historic use and development as an employment site. This includes the following which are most recent/relevant:

- 93/02280/F – Erection of new research building and additional car parking – Approved
- 00/04690/F - Research building, storage, workshops, laboratories & offices – Approved with conditions
- 09/00822/CU - Change of use of building to use class B8 storage or distribution – Approved with conditions

### **4.0 Proposal and Design Approach**

4.1 The proposed development seeks planning permission for the redevelopment of the site to provide four buildings consisting of 5 units for open B1(b), B1(c), B2 or B8 use.

4.2 The units would be laid out around a central access road, with three on the western side adjacent to the frontage with the A23 and a further two alongside the embankment with the railway line. The units would range in size from 1,923sqm to 4,702sqm, with the units along the A23 broadly 13.6m in height and the units adjacent to the railway line approximately 15.1m. Each unit would have a large open plan warehouse type space with ancillary mezzanine office accommodation. The building would employ a mix of profiled metal cladding and timber cladding to the main elevations, with profiled metal roof.

4.3 The development would use the existing western access to the site which would lead to the main access road. Units 4 and 5 would have their own dedicated secure yards serving the HGV loading bays and providing some vehicle parking. These units would also have separate parking areas. Units 1, 2 and 3 would have smaller loading bay areas, served directly off the main access road. Each unit would have additional surface parking. New planting is proposed along the northern, eastern and southern boundaries of the site.

4.4 A design and access statement should illustrate the process that has led to the development proposal, and justify the proposal in a structured way, by demonstrating the steps taken to appraise the context of the proposed development. It expects applicants to follow a four-stage design process comprising:  
Assessment;  
Involvement;  
Evaluation; and  
Design.

4.5 Evidence of the applicant's design approach is set out below:

<p>Assessment</p>	<p>The Design &amp; Access (D&amp;A) Statement describes that the existing site as being generally regular in shape and relatively level. The site covers the former laboratory site, which predominantly remains as vacant land; the previous buildings were demolished late 2009. The site is part of the Salfords Industrial Estate which is fairly linear and is split in two, separated by a residential area. The application site is bounded by Bonehurst Road to the west and is tree line with extensive hedgerow. The northern boundary along Cross Oak Lane is similarly tree lined and there is a ditch along the boundary which is culverted at the two access points. The Metropolitan Green Belt surrounds the application site, including the former PRL sports ground to the south of the site. Existing buildings surrounding the proposed development range between circa 7.5-13m in height to the ridge.</p> <p>The trees that surround the application site offer an important buffer to surrounding Metropolitan Green Belt to the east, south and west. The proposal sets the building back from these to protect them.</p>
<p>Involvement</p>	<p>The Planning Statement identifies that pre-application advice was sought from the Council in 2018 and design of the scheme amended in response. No evidence of public consultation is provided in the submission.</p>
<p>Evaluation</p>	<p>The Statement sets out the evolution of the design of the scheme, as a result of the pre-application discussions. This includes reducing addressing the set back and massing onto the A23 and the landscaping of the site. No evidence of other development options considered is identified within the applicant's submission.</p>
<p>Design</p>	<p>The Design Statement explains that the chosen design seeks to make best use of the site to provide a commercially viable redevelopment but whilst also not being overly dense so as to limit the functionality of the buildings. The extent of yard, loading doors, office content and building height have been balanced to suit commercial requirements. The scheme has been designed so that the buildings front onto the service road. The positioning of first floor office accommodation seeks to allow for natural surveillance of the site as well as acting as a feature onto the road frontages. The scheme incorporates additional landscape buffer and boundary treatments to reduce impacts to the Green Belt and residential uses to the south. Whilst the size of the buildings is market orientated, the layout is design lead, siting the larger scale and taller units along the railway line.</p>

4.6 Further details of the development are as follows:



Site area	3.1ha
Existing use	Cleared site (previously Research & Development offices/laboratory)
Proposed use	Mixed industrial/distribution (open B1(b), B1(c), B2 and B8)
Number of units	5
Total floorspace	15,623sqm
Proposed parking spaces (exc. HGVs)	229
Parking standard	BLP 2005 – ranges from 521 maximum (if all B1/B2) to 156 (if all B8 storage)

## 5.0 Policy Context

### 5.1 Designation

Urban Area  
Employment Area  
Part Flood Zone 2 and Flood Zone 3

#### Reigate and Banstead Core Strategy

CS1 (Presumption in favour of sustainable development)  
CS4 (Valued townscapes and historic environment)  
CS5 (Valued people/economic development)  
CS8 (Area 3: Horley)  
CS10 (Sustainable development)  
CS11 (Sustainable construction)  
CS12 (Infrastructure delivery)  
CS17 (Travel options and accessibility)

### 5.2 Reigate & Banstead Borough Local Plan 2005

Employment	Em1, Em2, Em3, Em8
Movement	Mo3, Mo4, Mo5, Mo6, Mo7
Utilities	Ut4

### 5.3 Other Material Considerations

National Planning Policy Framework

National Planning Practice Guidance

Supplementary Planning Guidance	Developer Contributions SPD Local Distinctiveness Design Guide
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Other	Human Rights Act 1998 Community Infrastructure Levy Regulations 2010 (as
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amended)  
Conservation of Habitats and Species Regulations 2017  
Public Sector Equality Duty

## 6.0 Assessment

- 6.1 The application site comprises a cleared site formally used as a research and development campus. The site is within a designated Employment Area within the 2005 Borough Local Plan and is partially within Flood Zone 2 and 3 at its northern end.
- 6.2 The main issues to consider are therefore:
- principle of development
  - design and impact on the character of the area
  - effects on the amenity of neighbouring properties
  - access, parking and highway implications
  - flooding and drainage
  - other matters

### Principle of development

- 6.3 The application site is within a designated Employment Area, namely Salfords Industrial Estate which occupies a ribbon along the eastern side of the A23. The proposals seek to redevelop the site for to provide 5 commercial units, totalling 15,623sqm, with an open B1(b), B1(c), B2 or B8 use. Given the designation of the site and the proposed development, policies Em2 and Em8 of the Local Plan 2005 and policy CS5 of the Core Strategy are particularly relevant.
- 6.4 Policy CS5 seeks, in general terms, to promote sustainable economic prosperity in the Borough, setting out that the Council will plan *“for a range of types and sizes of employment premises to cater for the needs of established, growing and start-up businesses”* by *“focussing on retaining and making best use of existing employment land, particularly within town centres and industrial areas”*. These proposals, which would redevelop a large site within a designated industrial area which has lain vacant for a number of years, would be wholly consistent with the thrust of this policy. The proposals would make good use of a previously developed site which the NPPF advises should be given “substantial weight”. The Borough Local Plan specifically encourages and seeks to direct the sorts of industrial, storage and distribution uses proposed in this application to designated Employment Areas (which this site is). The proposals would support this overarching strategy.
- 6.5 Turning to the other Borough Local Plan policies, policy Em2 sets out the need to have consideration to a number of factors in considering proposals for industrial, storage and distribution uses. Limb (ii) of this policy requires that consideration be given to whether there are alternative land/premises available to deliver the proposed employment provision, in essence, seeking to manage supply to avoid undue pressure on housing and labour markets. It is questionable, whether this limb applies to this site (as the policies specifically exempts the *“redevelopment of outworn industrial storage and distribution uses”* from this test) and furthermore, it is questionable whether such a restrictive approach is consistent with the Framework

which provides that *“decisions should help create the conditions in which businesses can invest, expand and adapt”*.

- 6.6 At any rate, the Council’s latest Commercial Development Monitor demonstrates that there has been little, if any, growth in industrial floorspace in the borough over the past decade and, furthermore, indicates that current extant planning permissions would actually result in a net loss of industrial and distribution space if implemented (6,230sqm). According to the same monitor, the level of available (i.e. on the market to buy or lease) industrial and warehouse space identified through the Council’s monitoring has fallen by around half in the past 5 years (29,000sqm in 2013 to 14,191sqm in 2018). The Council’s Economic Prosperity team advises that their engagement with the market supports the view that there is a demand for the type and size of accommodation proposed in the borough, with limited opportunities available elsewhere. Furthermore, it should be noted that the Core Strategy (Policy CS8 Area 3) anticipates delivery of 24,000sqm of employment provision in the Horley/Salfords area; redevelopment of this site for the scale of floorspace proposed would support this and, given limited delivery to date, would not result in this figure being exceeded. As such, there is not considered to be an unacceptable risk of pressure in labour or housing markets. Taking all of the above into account, it is concluded that the proposals would pass limb(ii) of Policy Em2. The other aspects of Em2 (e.g. in respect of infrastructure impacts and housing/environmental policies) are discussed below.
- 6.7 With regard to policy Em8, the other specific provision this introduces relates to a general desire to resist regional distribution centres. Whilst this proposal provides a total of 15,623sqm (thus exceeding the 5,000sqm set out in the policy), this would be split across 5 units with no individual unit exceeding 5,000sqm (the largest would be 4,602sqm). The proposal does not therefore conflict with this requirement. This approach is consistent with other developments in the Salfords Industrial Area exceeding 5,000sqm in total but split amongst smaller units which have been approved.
- 6.8 Taking all of the above into account, the proposed uses on this designated Employment Area are considered, in principle, to be wholly consistent with development plan policies. Furthermore, the proposals would make good use of a vacant previously developed site of generally low environmental value which the Framework advises should be given substantial weight.

#### Design and impact on the character of the area

- 6.9 The proposals were subject pre-applications discussions with Officers regarding the proposed scale, massing and design of the building. The design has, as set out in the applicant’s Design & Access Statement, evolved significantly through this process and the application.
- 6.10 The layout is considered to be typical of, and suitable for, a modern industrial/commercial estate. The five units would be laid out around a broadly central access road running north-south through the site, each with its own yard/loading area and dedicated parking, either to the side or front of the building. The units would front onto the access road providing natural surveillance and

creating a frontage. Some tree planting and soft landscaping would be incorporated within the parking areas and along the access road, the extent of this is considered to be appropriate given the active commercial environment of the estate (where robustness is important) and mindful of the extensive landscape setting which would be retained around the boundaries of the site.

- 6.11 Whilst the proposed units would be large in scale and relatively tall (13-15m), this is not unusual for industrial/commercial units given their function. Furthermore, as the submitted street scene drawings demonstrate, the height of the buildings would not be dissimilar to the adjacent Titan Travel office building, and whilst there would be a marked step down in scale to properties to the south and west, there would be sufficient separation such that this would not appear stark or abrupt.
- 6.12 In addition, the layout of the site is considered to mitigate their impact on the character and street scenes of the wider area. The units would all be significantly set back from the road, retaining a gap of between 17.5 and 25m to Cross Oak Lane and 15 to 18m from Bonehurst Road (A23), helping to ensure that they would not appear unduly dominant in views along these roads. The existing boundary tree and hedgerow cover would also be retained along both frontage, and would be extensively supplemented along Bonehurst Road, to ensure that the verdant, "parkway" character of these thoroughfares would be maintained. The layout also position the larger footprint, taller units towards the back (east) of the site along the railway line where their scale would be less appreciable in the public realm and where it would be read against the backdrop of the steeply rising land of the railway embankment. Landscaping would also provide screening along the southern boundary in order to respect the transition to the adjoining countryside.
- 6.13 In terms of form and appearance, the units would be relatively typical of commercial/industrial premises with a simple, uncomplicated boxy form with shallow pitched roofs. Each of the units would have mezzanine office accommodation and the configuration/internal layout seeks to position these office elements so as to provide a feature onto more prominent frontages. In terms of materials, the buildings would be predominantly clad in profiled metal; however, the design also incorporates areas of timber cladding to good effect to break up and add visual interest to the elevations whilst also aiding legibility by demarcating entrances/office areas. Timber cladding is also proposed at high level on the elevations facing Bonehurst Road to provide a more natural backdrop behind the canopy of the trees along this frontage.
- 6.14 Overall, the layout of the site and design of the buildings is considered to be acceptable with respect to its end use and impact on the character of the surrounding area. The proposal would therefore comply with policy Em3 of the Local Plan 2005, CS4 and CS10 of the Core Strategy and the provisions of the Framework in respect of achieving well designed places.

#### Effects on the amenity of neighbouring properties

- 6.15 The nearest residential neighbours to the development site are a bungalow to the south (20 Bonehurst Road) and on the opposite side of the A23 (including Horley Place which has consent to convert to residential). Further residential properties

exist to the north, beyond the Titan Travel campus (Empire Villas); however, these are approximately 140m from the site.

- 6.16 In terms of 30 Bonehurst Road, the nearest of the proposed buildings would be over 35m from the side boundary of this neighbour. At this distance, whilst acknowledging the scale and height of the proposed buildings, it is not considered that it would be unduly overbearing nor give rise to unacceptable overshadowing (particularly the application site is due north of this neighbour). The buffer afforded by the existing and proposed tree cover between unit 3 and no.30 would also assist in screening the building so it would not appear obtrusive. No windows are proposed on the southern elevation of Unit 3; hence there is not considered to be any risk of overlooking or loss of privacy.
- 6.17 In terms of Horley Place and the other residential units on the opposite side of Bonehurst Road, these would similarly be well separate from the proposed units (c.45m). Given this distance, and the presence of the intervening major road, it is not considered that the buildings would appear dominant or unduly harmful to the outlook of the existing and potential residences on this side of Bonehurst Road. The proposals would pass the 25 degree rule in relation to easterly facing windows on these properties and thus are not considered to cause harmful loss of light.
- 6.18 The proposed development would introduce a considerable level of activity and permission is sought without any limitation on working hours. Whilst this is not objectionable in principle (and other similar developments have been granted free of restriction), consideration needs to be given to the impact of potential disturbance on neighbouring properties. The application was supported by a Noise Impact Assessment which considers both daytime and night-time noise arising from the proposals. The scope of the assessment, in terms of the types of activities which it factors into the analysis and the level of noise generated is considered to be appropriate and a reasonable representation of the potential operations on site (e.g. it includes activities within the buildings, plant and machinery on the buildings and operating outside and the noise from vehicular movements including idling vehicles).
- 6.19 The assessment concludes that, in respect of properties opposite the site on the western side of Bonehurst Road, the assessed level of noise (at 43dB LA<sub>eq1hr</sub>) they would experience as a result of the proposal would be below the daytime background noise level by 18dB and, for the night-time scenario would be 39dB which is equal to the background level. On this basis, the proposals would not create an unacceptable level of noise or disturbance for these neighbours. With regards to the single dwelling to the south at no.30 Bonehurst Road, the ambient noise levels are calculated as 52 dB during the day and 35 at night. Initial calculations showed that excesses of acceptable noise levels would occur for this neighbour; and, as a result, a 4m acoustic fence/noise barrier has been designed into the scheme along the majority of the southern boundary. When the scheme is remodelled with this in place the predicted noise levels of 45dB LA<sub>eq1hr</sub> during the day (which is below the background level of 52dB) and 35dB at night (i.e. no different to the background noise). Therefore, with the acoustic measures proposed, the proposals would not cause an unacceptable noise impact on no.30 Bonehurst

Road. A condition requiring implementation of the acoustic fence (and submission of details of its performance) is recommended.

- 6.20 The Noise Impact Assessment incorporates provision for mechanical plant within its calculations. However, as the units are to be speculatively built, the final plant requirements are not fully known at this point. To safeguard the amenity of neighbours, a condition requiring details of all plant and machinery required for each unit prior to its occupation is recommended. This will provide the Council scope to ensure that any plant would not have an unacceptable noise implication.
- 6.21 An External Lighting proposal was accompanied with the application, detailing all external lighting (e.g. to access roads/service yards). The strategy includes lighting diagrams which demonstrate that, at the immediate southern boundaries of the site, lux levels would be a maximum of 23 in isolated locations but generally in the low teens. These light levels are not considered to give rise to unacceptable light pollution or disturbance to no.30 Bonehurst Road, particularly given the significant intervening boundary planting along this southern boundary which would screen and dapple any light. The plans do not show any lighting along the western boundary.
- 6.22 Overall, whilst the proposal would result in some change for neighbouring properties, given the generous separation distances, intervening tree cover and proposed acoustic measures, it would not result in an unacceptable impact on amenity or living conditions of neighbouring occupiers. The proposal therefore complies with policy Em3 of the Borough Local Plan 2005 in this respect.

#### Flooding and drainage

- 6.23 According to Environment Agency flood maps, the northern part of the site closest to Cross Oak Lane is partially within Flood Zone 2 and partially within Flood Zone 3.
- 6.24 With regard to the Sequential Test, as above, the current proposals would contribute to meeting the 24,000sqm of additional employment floorspace anticipated for the Horley/Salfords area in the Core Strategy. Given the policies in the Local Plan which seek to direct industrial/warehouse development to designated Employment Areas and the limited availability of land elsewhere in such estates to accommodate a development of the nature proposed in this application, it is concluded that the proposals could not be achieved on land at lower risk of flooding. There are also specific visual and physical benefits associated with regenerating and redeveloping this prominent and long vacant site which would clearly not be achieved if development was carried out elsewhere. Consequently, the proposals satisfy the Sequential Test.
- 6.25 In terms of the flooding risk on the site, the new buildings are sited such that they would, as far as possible, avoid development within Flood Zone 2 and there would be no development within Flood Zone 3. Given this, it is not considered necessary for the development to satisfy the exception test.
- 6.26 The application was supported by a Flood Risk Assessment and initial drainage strategy. This has been reviewed by both the Environment Agency – who raises no

objection to the proposals from a flood risk perspective. In respect of drainage, the submission was reviewed by Surrey County Council as the Lead Local Flood Authority. The LLFA initially raised concerns regarding the proposed drainage strategy; however, following dialogue and agreement from the applicant to increase storage to reduce run-off rates, the LLFA has confirmed they have no objection subject to conditions. Compared to the historic situation on site (and historic significant extent of hardstanding), this would likely represent a marked improvement in the management of surface water from the site.

- 6.27 Based on the above and taking account of the expert advice of the relevant consultees, it is concluded that the application passes the Sequential Test and, furthermore, would respond to the flood risk on site appropriately in terms of drainage provisions. On this basis, the proposal complies with Policy CS10 of the Core Strategy, Ut4 of the Local Plan and the relevant national policy provisions.

Accessibility, parking and highway implications

- 6.28 The development would be access from Cross Oak Lane utilising a current access which would be brought back into use and appropriately modified/improved. The second of the two existing accesses (closer to the railway bridge) would be closed and the kerb/footway/verge reinstated.
- 6.29 The application was accompanied by a Transport Assessment which examines the travel patterns, parking demand and trip generation which would be associated with the proposed development. During the course of the application, the developer also provided – at the request of the County Council – specific modelling of the potential impacts of the proposals on nearby junctions, including the A23 with Cross Oak Lane and the proposed junction between the A23 and the new link road to Westvale Park. This modelling was undertaken and adapted in accordance with recommendation from the County Council’s modelling team and as such the inputs and assumptions made are all agreed by the County Council.
- 6.30 The results of this modelling show that in 2023, queues in the AM peak on the north side of the A23 junction with Cross Oak Lane would increase from 145m (without development) to 163m with the development. Queues on the south side of the junction would increase from 165m to 175m. In the PM peak, the increases would be 174m to 201m (north side) and 156m to 178m (south side). Overall, given the volumes of traffic on the A23, these increases in queue lengths are considered to be relatively modest and it is concluded that they do not represent the sort of “severe” impact on the operation of the junction and wider highway which the NPPF advises would warrant refusal. The County Highway Authority particularly notes that these additional queues would not affect any new junctions compared to the existing situation (other than the proposed new A23/Westvale Park junction which would be affected at any rate). The queues on Cross Oak Lane would similarly increase; however, they would not reach as far back as the railway bridge hence the impacts are not considered to be unacceptable or prejudicial to highway operation.
- 6.31 The TRICS comparator sites which underpinned the above modelling were industrial commercial estates with a mix of industrial and warehouse/distribution uses. The effect of a wholly B8 use of the site (i.e. as a distribution centre) has not

therefore been fully modelled. On this basis, a condition to limit the amount of floorspace on the development which could be used in B8 use is considered reasonable and necessary. The County Highway Authority has analysed the evidence and considers a limit of 9,098sqm is reasonable and would be likely to result in queue lengths ; this would effectively mean that no more than 3 of the proposed units could be operated in B8 use (and if the two largest units are in B8 use then no others).

- 6.32 As above, the site would be accessed using an existing site access to Cross Oak Lane which would be re-opened and modified. The County Highway Authority has reviewed the access point and considers that it would provide adequate visibility. The CHAs response also concludes that, whilst tracking overlays show that a HGV exiting the site in a westerly direction would have to cross partially into the opposing traffic lane, the access point would have adequate geometry to accommodate HGVs, particularly mindful of the fact that the relatively low numbers of HGV movements expected at peak times (as per the TRICS analysis carried out by the applicant and County Council) and given the proximity to a signalised junction which will provide periods of no flow eastbound along Cross Oak Lane providing space for HGVs to turn out of the site without impeding oncoming vehicles. The County Highway Authority response does however acknowledge that any on-street parking along Cross Oak Lane could affect this situation (as it would narrow the carriageway), hence, a condition is recommended to require the developer to implement double yellow line parking restrictions along both sides of Cross Oak Lane between the A23 and the rail bridge. Given the circumstances, this is clearly necessary and reasonable.
- 6.33 Concerns have been raised regarding the adequacy of the internal layout, particularly in terms of manoeuvrability of HGVs. In this regard, the applicant has provided tracking/swept path drawings for the two self-contained yards (Units 4 and 5) which demonstrate that HGVs can manoeuvre satisfactorily into each of the loading bay positions without conflict with any other vehicles (including parked vehicles) within the yards. Whilst it is acknowledged that the yards are tight, they are therefore adequate and simply represent a design which seeks to maximise the efficient use of the site.
- 6.34 The scheme incorporates a total of 229 vehicle parking spaces (excluding HGVs). The standards in the Borough Local Plan 2005 would advise anywhere between 156 and 521 spaces; however, it should be noted that the larger figure assumes that all of the units would be used for B1 (b) research and development which is considered unlikely to be a realistic scenario given the nature and size of the units. At any rate, the standards in the Borough Local Plan are maximum. Using evidence from the national TRICS database (and based on comparator sites specifically suggested by the County Council), the applicant's transport evidence demonstrates that likely peak parking accumulation is likely to be 149 vehicles; hence, the parking provision proposed would provide ample capacity with a considerable surplus of c.75 vehicles. Subsequent analysis by the County Council assuming the whole site was operated in B8 use suggests that peak parking accumulation (all vehicles) would reach 189 vehicles, still some distance below the 229 spaces proposed. On this basis, the general level of parking within the site is considered adequate and unlikely to give rise to displacement parking on Cross Oak Lane.



- 6.35 With regards to HGVs, the development incorporates a total of 17 loading bays across the 5 units. The applicant's initial analysis suggested a maximum of 72 total HGV trips to and from the site between 7am and 7pm, with no more than 2 loading bays ever occupied in any given half hour period. Concerns were expressed in local representations, including from Salfords and Sidlow Parish Council, about the robustness of this analysis.
- 6.36 As a result, further independent investigations were carried out by the County Highway Authority using TRICS data assuming sites of a similar size (15-20,000sqm) in use entirely as a distribution centre. These sites show a maximum HGV accumulation of 23 (based on c.270 HGV arrivals throughout the day). This would potentially exceed the number of loading bays on the proposed scheme by 6. However, as above, it is intended through condition to limit the amount of floorspace which could be used as B8 space on this site (to c.9,000sqm) such that these level of accumulations (which are based on 15-20,000sqm B8 distribution uses) are unlikely to arise.
- 6.37 Furthermore, it should be noted that this peak accumulation is anticipated to occur late at night (23:30-24:00), when general vehicle parking accumulation for such developments is shown to be lower (and at any rate, as discussed above at paragraph 6.34, there would be a surplus of normal vehicle parking spaces in such a scenario even at peak accumulation). In this regard, there are considered to be adequate areas within the site where any excess HGVs could be "stacked" whilst awaiting access to a loading bay, particularly – for example – in the yards of Units 1, 4 and 5 and - in a worst case - along the internal estate road. With this evidence in mind, there is considered to be adequate space within the site to accommodate and manage HGV movements such that there would be unlikely to be any overspill on surrounding roads (e.g. Cross Oak Lane).
- 6.38 However, to safeguard this position, the County Highway Authority has also recommended – as above – that a condition be imposed requiring the developer to implement double yellow lines on both sides of Cross Oak Lane between the A23 and railway bridge to further prevent HGVs parking this road whilst waiting to enter. A further condition is also recommended to require the submission and approval of a Delivery & Servicing Plan setting out how the site will be operated and managed (e.g. in terms of delivery sequencing/timing, management of parking areas, etc.) to prevent overspill HGV parking on surrounding roads. With these additional measures, it is concluded that the likely HGV activity associated with the site would not prejudice safety or operation of surrounding roads.
- 6.39 The application was supported by a draft Travel Plan designed to encourage sustainable travel to and from the site, particularly for staff and visitors. The County Council has recommended that a final revised version be secured by condition prior to occupation; this is considered necessary to ensure that the development would comply with Policy CS17.
- 6.40 Subject to the conditions proposed by the County Highway Authority, the proposal is considered to comply with policies Em3, Mo4, Mo5, Mo6 and Mo7 of the Borough Local Plan and policy CS17 of the Core Strategy.

### Community Infrastructure Levy (CIL) and requested contributions

- 6.41 The proposal, being for new industrial and distribution premises, falls outside of the uses which attract a charge based on the Council's adopted Charging Schedule and as such the development would not be liable to pay CIL.
- 6.42 The Community Infrastructure Levy (CIL) Regulations were introduced in April 2010 and state that it is unlawful to take a planning obligation into account unless its requirements are (i) relevant to planning; (ii) necessary to make the proposed development acceptable in planning terms; and (iii) directly related to the proposed development. As such only contributions that are directly required as a consequence of development can be requested and such requests must be fully justified with evidence including costed spending plans to demonstrate what the money requested would be spent on.
- 6.43 In this case, as above, the County Council has requested contributions towards a future parking review to the east of the rail bridge on Cross Oak Lane (£3,000), revised signage on the Cross Oak Lane rail bridge (£4,000) and travel plan monitoring (£6,150). These contributions are considered necessary to address and avoid potential unacceptable highways impacts and are proportionate in their scale and kind to the development proposed. A clear justification for each has been provided by the County Highway Authority and the contributions have been accepted and agreed by the applicant. These will be secured through a legal agreement. In addition, the County Highway Authority has requested that the applicant carry out double yellow lining along both sides of Cross Oak Lane between the A23 and the railway bridge; however, this can be secured through condition and subsequent s278 works.

### Other matters

- 6.44 The application was supported by appropriate arboricultural surveys and assessments which have been reviewed by the Council's Tree Officer. The Tree Officer concludes that the proposed development will not result in significant loss of mature trees and vegetation and notes that the new landscaping and tree planting proposed will *"not only mitigate the minor tree loss but will also provide significant enhancements and improvements to the existing landscape for the long term"*. In respect of the tree protection and method statements provided, the Tree Officer advises that a finalised Tree Protection Plan and Arboricultural Method Statement (including provision for a pre-commencement meeting and monitoring) should be required prior to commencement.
- 6.45 The application site is not subject to any specific nature conservation designations. A Preliminary Ecological Appraisal was supplied with the application which concludes that the habitats within the site are generally of limited ecological value (particularly owing to the fact that much of the site is hardstanding); however, the site does provide habitat which could support reptiles and breeding birds. Given the general limited ecological potential, no further surveys are recommended however the report makes recommendations regarding working practices and ecological

enhancement measures. These are agreed and a condition requiring compliance with the measures in the report is recommended.

- 6.46 The application was supported by an Energy Statement which discusses the measures to be used on site to achieve a reduction in energy use and associated emissions, focussing on a passive enhancement measures (i.e. building performance to reduce demands for heating/cooling) and active measures (such as LED lighting and switching) to minimise energy use. The Statement also acknowledges the need for the scheme to comply with BREEAM 'Very Good' standard as per Policy CS11. A condition requiring BREEAM compliance will be imposed.
- 6.47 The Conservation Officer has identified that the sculpture which was previously on site (but was removed for protection as part of a condition for the prior approval of the demolition) is by noted sculptor Keith Godwin and is known as the "Polar Theme". The Conservation Officer considers the sculpture to be an important and valuable asset which he considers worthy of listing. Whilst it is therefore a non-designated heritage asset at present, it is considered to be of greater than local significance and interest. A condition to secure, where feasible, repair and restoration of the sculpture to enable its relocation within the site in order to preserve this element of the heritage of the site is recommended.
- 6.48 The applicant has provided a Phase 1 and Phase 2 Contaminated Land Report which details the contamination risks on the site. On the basis of these, the Contaminated Land Officer raises no objection subject to a condition regarding the management/approach to any unexpected contamination encountered during the development.
- 6.49 Gatwick Airport have recommended a condition requiring submission and approval of a Bird Hazard Management Plan given the extent of flat/shallow pitched roofs on the buildings which could be attractive to nesting, roosting and loafing birds and therefore a risk to aerodrome safety.

## CONDITIONS

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.  
Reason:  
To comply with Section 91(1) of the Town and Country Planning Act 1990 as amended by Section 51 (1) of the Planning and Compulsory Purchase Act 2004.
2. The development hereby permitted shall be carried out in accordance with the following approved plans:

<b>Plan Type</b>	<b>Reference</b>	<b>Version</b>	<b>Date Received</b>
Location Plan	001		31.05.2018
Roof Plan	PL 016		31.05.2018
Roof Plan	PL 015		31.05.2018
Roof Plan	PL 014		31.05.2018
Roof Plan	PL 013		31.05.2018

Elevation Plan	PL 010		31.05.2018
Section Plan	PL 017	A	19.10.2018
Floor Plan	PL 006	A	04.10.2018
Other Plan	TR01	P1	04.10.2018
Floor Plan	PL 003	A	04.10.2018
Floor Plan	PL 005	A	04.10.2018
Floor Plan	PL 008	B	19.10.2018
Elevation Plan	PL 009	A	04.10.2018
Site Layout Plan	PL 002	C	19.10.2018
Arboricultural Plan	PL 12_001	C	04.10.2018
Arboricultural Plan	PL 12_003	C	04.10.2018
Arboricultural Plan	PL 12_002	B	04.10.2018
Elevation Plan	PL 011	A	04.10.2018
Floor Plan	PL 007	A	04.10.2018
Elevation Plan	PL 012	B	19.10.2018
Other Plan	18-033 / 300	T7	01.10.2018
Other Plan	TR05	P1	04.10.2018

Reason:

To define the permission and ensure the development is carried out in accord with the approved plans and in accordance with National Planning Practice Guidance.

Note: Should alterations or amendments be required to the approved plans, it will be necessary to apply either under Section 96A of the Town and Country Planning Act 1990 for non-material alterations or Section 73 of the Act for minor material alterations. An application must be made using the standard application forms and you should consult with us, to establish the correct type of application to be made.

3. No development shall commence until a Construction Transport Management Plan has been submitted to and approved in writing by the Local Planning Authority. The final plan shall include details of:
- (a) parking and turning for vehicles of site personnel, operatives and visitors
  - (b) loading and unloading of plant and materials
  - (c) storage of plant and materials
  - (d) programme of works (including measures for traffic management)
  - (e) provision of boundary hoarding behind any visibility zones
  - (f) construction vehicle routing to and from the site
  - (g) measures to prevent the deposit of materials on the highway
  - (h) before and after construction condition surveys of the highway and a commitment to fund the repair of any damage caused
- has been submitted to and approved in writing by the Local Planning Authority. Only the approved details shall be implemented during the construction of the development.

Reason:

In order that the development should not prejudice highway safety nor cause inconvenience to other highway users to satisfy policies Mo5 and Mo7 of the Reigate and Banstead Borough Local Plan 2005 and the objectives of the NPPF.

4. No development shall commence including demolition or any groundworks preparation until a detailed, scaled Tree Protection Plan (TPP) and the related

Arboricultural Method Statement (AMS) is submitted to and approved in writing by the Local Planning Authority (LPA). These shall include details of the specification and location of exclusion fencing, ground protection and any construction activity that may take place within the Root Protection Areas of trees (RPA) shown to scale on the TPP, including the installation of service routings. The AMS shall also include a pre commencement meeting with the LPA, supervisory regime for their implementation & monitoring with an agreed reporting process to the LPA. All works shall be carried out in strict accordance with these details when approved.

Reason:

To ensure good arboricultural practice in the interests of the maintenance of the character and appearance of the area and to comply with British Standard 5837:2012 'Trees in Relation to Design, demolition and Construction – Recommendations' and policies Pc4 and Ho9 of the Reigate and Banstead Borough Local Plan.

5. No development above ground floor slab level shall commence until the detailed design of the surface water drainage scheme has been submitted to and approved in writing by the Local Planning Authority. Such details should include:
- a) A design that satisfies the SuDS Hierarchy and is compliant with the national non-statutory Technical Standards for SuDS, NPPF and Ministerial Statement on SuDS
  - b) Evidence that the proposed solution will effectively manage the 1 in 30 and 1 in 100 (+40% allowance for climate change) storm events during all stages of the development (pre, post and during), associated discharge rates and storage volumes shall be provided using a Greenfield discharge rate of 30.6l/s (as per the SUDS pro-forma or otherwise agreed by the Local Planning Authority)
  - c) Detailed drainage design drawings and calculations to include: a finalised drainage layout detailing the location of drainage elements, pipe diameters, levels, and long and cross sections of each element including details of any flow restrictions and maintenance/risk reducing features (silt traps, inspection chambers, etc.)
  - d) Details of how the system will be protected during construction and how runoff (including any pollutants) from the development site will be managed before the system is operational
  - e) A plan showing exceedance flows (i.e. during rainfall greater than design events or during blockage) and how property on and off site will be protected.

Reason:

To ensure that the development is served by an adequate and approved means of drainage which would not increase flood risk on or off site and is suitably maintained throughout its lifetime to comply with Policy Ut4 of the Reigate and Banstead Borough Local Plan 2005, Policy CS10 of the Core Strategy 2014 and the requirements of non-statutory technical standards.

6. No development above ground floor slab level of any part of the development hereby approved shall take place until a Bird Hazard Management Plan detailing how the flat/shallow pitched roofs area will be managed to minimise their attractiveness to birds has been submitted to and approved in writing by the Local Planning Authority.

The Bird Hazard Management Plan shall be implemented upon completion of the roof and shall remain in force for the life of the building and shall not be revised or amended unless otherwise agreed with the Local Planning Authority.

Reason:

To ensure that the roof areas are adequately managed to minimise their attractiveness to birds which could endanger the safe movement of aircraft and the operation of Gatwick Airport with regard to the Town and Country Planning (Safeguarded Aerodromes, Technical Sites and Military Explosives Storage Areas) Direction 2002.

7. The units hereby approved shall be using the external facing materials, including fenestration, balconies and roof, specified on the approved drawings and no others without the prior consent in writing of the Local Planning Authority.

Reason:

To ensure that a satisfactory external appearance is achieved of the development with regard to Reigate and Banstead Borough Local Plan 2005 policy Em3

8. All hard and soft landscaping shall be completed in full accordance with the scheme as detailed on the approved Site Plan PL002 Rev C, Landscape General Arrangement and Details Plans (PL12\_001 Rev, PL12\_002 Rev B and PL12\_003 Rev C) prior to occupation or within the first planting season following completion of the development.

All new tree planting shall be positioned in accordance with guidelines and advice contained in the current British Standard 5837: Trees in relation to construction.

Any trees shrubs or plants planted in accordance with this condition which are removed, die or become damaged or become diseased within five years of planting shall be replaced within the next planting season by trees, and shrubs of the same size and species.

Reason:

To ensure good arboricultural and landscape practice in the interests of the maintenance of the character and appearance of the area and the historic gardens in order to comply with policies Pc4 and Em3 of the Reigate and Banstead Borough Local Plan 2005.

9. Notwithstanding the submitted drawings, no part of the development hereby approved shall be first occupied unless and until the following have been constructed or provided in strict accordance with a detailed scheme to be submitted to and approved in writing by the Local Planning Authority:

a) the existing western most vehicular access from the site to Cross Oak Lane has been constructed and provided within visibility zones of 2.4 metres by 43 metres in both directions; and

b) double yellow lines on both sides of Cross Oak Lane between the Cross Oak Lane junction with the A23 and the railway bridge to the east of the site.

Reason:

To ensure that the development would not prejudice highway safety nor cause inconvenience to other highway users to satisfy policies Mo5 and Mo7 of the Reigate and Banstead Borough Local Plan 2005 and the objectives of the NPPF.

10. The development hereby approved shall not be first occupied or brought into use unless and until a scheme for the repair and restoration of the Polar theme statue, and its relocation within the site, has been submitted to and approved in writing by the Local Planning Authority.

Such a scheme should be prepared by a suitably qualified conservator and should include an appraisal of the current condition of the sculpture and the feasibility of, and detailed specification for, any repairs as well as a method statement for returning the statute back to the site, including timeframes.

Reason:

In order to secure a realistic strategy for the preservation of this non-designated heritage asset with regard to policy CS4 of the Reigate and Banstead Core Strategy 2014 and the provisions of the NPPF.

11. The development hereby approved shall not be first occupied or brought into use unless and until the existing eastern most access from the site to Cross Oak Lane has been closed and the kerb, verge and/or footway reinstated in accordance with a scheme to be submitted to and approved in writing by the Local Planning Authority.

Reason:

To ensure that the development would not prejudice highway safety nor cause inconvenience to other highway users to satisfy policies Mo5 and Mo7 of the Reigate and Banstead Borough Local Plan 2005 and the objectives of the NPPF.

12. The development hereby approved shall not be first occupied or brought into use unless and until space has been laid out within the site in accordance with the approved plan numbered PL002 Rev C for vehicles to be parked and for the loading and unloading of HGV vehicles and for all vehicles to turn so that they may enter and leave the site in forward gear. Thereafter the parking/turning areas, including electric vehicle charging bays, shall be retained and maintained for their designated purposes.

Reason:

To ensure that the development would not prejudice highway safety nor cause inconvenience to other highway users to satisfy policies Mo5 and Mo7 of the Reigate and Banstead Borough Local Plan 2005 and the objectives of the NPPF.

13. Notwithstanding the submitted Travel Plan, the development hereby approved shall not be first occupied unless and until a revised Travel Plan has been submitted to and approved in writing by the Local Planning Authority. Such a statement should be in accordance with the sustainable development aims and objectives of the National Planning Policy Framework and Surrey County Council's "Travel Plans Good Practice Guide"

The approved Travel Plan shall be implemented upon first occupation and for each subsequent occupation of the development and shall thereafter be maintained and developed to the satisfaction of the Local Planning Authority.

Reason:

To ensure that the development would promote sustainable transport choices with regard to Policy CS17 of the Reigate and Banstead Core Strategy 2014 and in recognition of Section 9 "Promoting Sustainable Transport" in the NPPF.

14. The development shall be carried out in strict accordance with the External Lighting Proposals Report (Issue 2 29 May 2018) by Shepherd Brombley Partnership. No further external lighting other than that expressly identified within the above report shall be installed on site without the prior consent in writing of the Local Planning Authority.

Reason:

To ensure that appropriate external lighting is installed on site in order to safeguard the amenity of neighbouring properties and the character of the area, including the adjoining countryside, from excessive light spill with regard to policy Em3 of the Reigate and Banstead Borough Local Plan 2005.

15. Any contamination not previously identified by the site investigations summarised in the Land Quality Assessment Statement (by Bradbrook Consulting dated 14<sup>th</sup> May 2018), but subsequently found to be present at the site shall be reported to the Local Planning Authority as soon as is practicable.

If deemed necessary development shall cease on site until a remediation method statement, detailing how the unsuspected contamination is to be dealt with, has been submitted to and approved in writing to the Local Planning Authority. The remediation shall thereafter be completed in strict accordance with the approved statement, verification of which shall be provided to the Local Planning Authority.

Reason:

To ensure that the proposed development will not cause harm to human health or pollution of controlled waters with regard to Policy CS10 of the Reigate and Banstead Core Strategy 2014 and the NPPF.

16. The development hereby approved shall not be first occupied unless and until the 4m high acoustic fence along the southern boundary (as shown on approved plan PL002 Rev C) has been erected in accordance with a specification to be submitted to and approved in writing by the Local Planning Authority. Such a specification shall include details of the construction, appearance and acoustic performance of the fence to achieve the sound levels identified in the Noise Impact Assessment (ref PJB8387/18080/V1.1) by Spectrum Acoustic Consultants.

Reason:

To ensure that appropriate external lighting is installed on site in order to safeguard the amenity of neighbouring properties and the character of the area, including the adjoining countryside, from excessive light spill with regard to policy Em3 of the Reigate and Banstead Borough Local Plan 2005.

17. The development hereby approved shall not be first occupied unless and until a verification report demonstrating that the sustainable urban drainage system has been constructed as per the agreed scheme has been submitted to and approved in writing by the Local Planning Authority. The validation report should be carried out by a qualified drainage engineer.

Reason:

To ensure that the development is served by an adequate and approved means of drainage which would not increase flood risk on or off site and is suitably maintained throughout its lifetime to comply with Policy Ut4 of the Reigate and Banstead Borough Local Plan 2005, Policy CS10 of the Reigate and Banstead Core Strategy 2014 and the requirements of non-statutory technical standards.



18. The development shall be carried out and occupied in accordance with the recommendations for mitigation, habitat enhancement and working practices set out in the Preliminary Ecological Appraisal (dated May 2018) and BREEAM Ecology New Construction Assessment (dated May 2018) by Phlorum, including the management plan at Appendix E of the latter document. The recommended bat and bird boxes shall be installed on-site prior to first occupation of any unit on the site.

Reason:

In order to preserve and enhance the wildlife and habitat interest on the site and ensure species present on the site are afforded appropriate protection during construction works with regard to Reigate and Banstead Borough Local Plan 2005 policy Pc2G.

19. Prior to the occupation of any given unit, a Delivery & Servicing Management Plan specific to the occupation and operation of said unit shall be submitted to and approved in writing by the Local Planning Authority. Such a statement should include details of the anticipated number, frequency, type and timing of deliveries and how these will be managed to avoid overspill onto surrounding roads.

The approved Delivery & Servicing Management Plan shall be implemented upon first occupation and shall thereafter be maintained and developed to the satisfaction of the Local Planning Authority, including in the event of a change of occupier.

Reason:

To ensure that the development would promote sustainable transport choices with regard to Policy CS17 of the Reigate and Banstead Core Strategy 2014 and in recognition of Section 9 "Promoting Sustainable Transport" in the NPPF.

20. Prior to the occupation of any given unit, details of any plant or machinery, including fume extraction, ventilation and air conditioning, which may be required, shall be submitted to the Local Planning Authority.

Any plant, machinery or other extraction and ventilation equipment installed on the buildings shall be maintained thereafter in accordance with the approved details and any manufacturer's recommendations.

Reason:

To ensure that a satisfactory external appearance is achieved of the development and to safeguard the amenities of neighbouring occupiers with regard to Reigate and Banstead Borough Local Plan 2005 policy Em3.

21. Within three months of the occupation of each unit hereby approved, a final certificate demonstrating that BREEAM 'Very Good' rating is achieved as a minimum for the unit shall be submitted to and approved by the Local Planning Authority.

Reason:

To ensure that the development is constructed to appropriate sustainability standards with regard to Policy CS11 of the Reigate and Banstead Core Strategy 2014.

22. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015, (or any Order revoking and re-

enacting that Order with or without modification), the units hereby approved shall be occupied for purposes falling within Use Class B1(b), B1(c), B2 or B8 only and shall not be subdivided or used within any other use without the prior written consent of the Local Planning Authority.

Reason:

To control the use of the premises in the interests of maintaining an adequate supply of industrial, storage and distribution uses and with respect to the adequacy of parking provision and potential impacts on the surrounding highway network with regard to policies Em1, Em1A, Em8, Mo5 and Mo7 of the Reigate and Banstead Borough Local Plan 2005 and policy CS5 of the Reigate and Banstead Core Strategy 2014.

23. No more than 9,098 square metres (gross external area) of floorspace on the development hereby approved shall be used within Use Class B8 at any one time and there shall be no variation of this without the prior written consent of the Local Planning Authority.

Reason:

To control the use of the premises and the potential impacts on the surrounding highway network with regard to policies Mo5 and Mo7 of the Reigate and Banstead Borough Local Plan 2005 and policy CS5 of the Reigate and Banstead Core Strategy 2014.

## INFORMATIVES

1. Your attention is drawn to the safety benefits of installing sprinkler systems as an integral part of new development. Further information is available at [www.firesprinklers.info](http://www.firesprinklers.info).
2. The applicant is advised that prior to the occupation of the development, adequate provision should be made for waste storage and collection. You are advised to contact the Council's Recycling and Cleansing team to discuss the required number and specification of wheeled bins on [rc@reigate-banstead.gov.uk](mailto:rc@reigate-banstead.gov.uk) or on the Council's website at [http://www.reigate-banstead.gov.uk/info/20051/commercial\\_waste](http://www.reigate-banstead.gov.uk/info/20051/commercial_waste).
3. You are advised that the Council will expect the following measures to be taken during any building operations to control noise, pollution and parking:
  - (a) Work that is audible beyond the site boundary should only be carried out between 08:00hrs to 18:00hrs Monday to Friday, 08:00hrs to 13:00hrs Saturday and not at all on Sundays or any Public and/or Bank Holidays;
  - (b) The quietest available items of plant and machinery should be used on site. Where permanently sited equipment such as generators are necessary, they should be enclosed to reduce noise levels;
  - (c) Deliveries should only be received within the hours detailed in (a) above;
  - (d) Adequate steps should be taken to prevent dust-causing nuisance beyond the site boundary. Such uses include the use of hoses to damp down stockpiles of materials, which are likely to generate airborne dust, to damp down during stone/slab cutting; and the use of bowsers and wheel washes;
  - (e) There should be no burning on site;

- (f) Only minimal security lighting should be used outside the hours stated above; and
- (g) Building materials and machinery should not be stored on the highway and contractors' vehicles should be parked with care so as not to cause an obstruction or block visibility on the highway.

Further details of these noise and pollution measures can be obtained from the Council's Environmental Health Services Unit. In order to meet these requirements and to promote good neighbourliness, the Council recommends that this site is registered with the Considerate Constructors Scheme - [www.ccscheme.org.uk/index.php/site-registration](http://www.ccscheme.org.uk/index.php/site-registration).

4. The permission hereby granted shall not be construed as authority to carry out any works on the highway or any works that may affect a drainage channel/culvert or water course. The applicant is advised that a permit and, potentially, a Section 278 agreement must be obtained from the Highway Authority before any works are carried out on any footway, footpath, carriageway, verge or other land forming part of the highway. All works on the highway will require a permit and an application will need to be submitted to the County Council's Street Works Team up to 3 months in advance of the intended start date, depending on the scale of the works proposed and the classification of the road. Please see: [www.surreycc.gov.uk/roads-and-transport/road-permits-and-licences/the-traffic-management-permit-scheme](http://www.surreycc.gov.uk/roads-and-transport/road-permits-and-licences/the-traffic-management-permit-scheme). The applicant is also advised that consent may be required under Section 23 of the Land Drainage Act 1991. Please see: [www.surreycc.gov.uk/people-and-community/emergency-planning-and-community-safety/flooding-advice](http://www.surreycc.gov.uk/people-and-community/emergency-planning-and-community-safety/flooding-advice)
5. When a temporary access is approved or an access is to be closed as a condition of planning permission an agreement with, or licence issued by, the Highway Authority Local Highways Service will require that the redundant dropped kerb be raised and any verge or footway crossing be reinstated to conform with the existing adjoining surfaces at the developers expense.
6. Notwithstanding any permission granted under the Planning Acts, no signs, devices or other apparatus may be erected within the limits of the highway without the express approval of the Highway Authority. It is not the policy of the Highway Authority to approve the erection of signs or other devices of a non-statutory nature within the limits of the highway.
7. The permission hereby granted shall not be construed as authority to obstruct the public highway by the erection of scaffolding, hoarding or any other device or apparatus for which a licence must be sought from the Highway Authority Local Highways Service.
8. The developer is advised that as part of the detailed design of the highway works required by the above condition(s), the County Highway Authority may require necessary accommodation works to street lights, road signs, road markings, highway drainage, surface covers, street trees, highway verges, highway surfaces, surface edge restraints and any other street furniture/equipment.
9. The developer is reminded that it is an offence to allow materials to be carried from the site and deposited on or damage the highway from uncleaned wheels or badly

loaded vehicles. The Highway Authority will seek, wherever possible, to recover any expenses incurred in clearing, cleaning or repairing highway surfaces and prosecutes persistent offenders. (Highways Act 1980 Sections 131, 148, 149).

10. Section 59 of the Highways Act permits the Highway Authority to charge developers for damage caused by excessive weight and movements of vehicles to and from a site. The Highway Authority will pass on the cost of any excess repairs compared to normal maintenance costs to the applicant/organisation responsible for the damage.
11. With respect to the Travel Plan required by the above conditions, the following advice is provided to the developer in respect of the revisions and information required:
  - (a) Information on the likely quantum of employees working on the site.
  - (b) Clarification is needed on whether “operator” means “site management company
  - (c) As each unit becomes occupied, the occupier should then complete a pro-forma and submit it to the TPC (this wording could be added to para 6.2.7).
  - (d) Contact details of the Travel Plan Coordinator
  - (e) The action plan states the cycle parking, lockers and showers will be installed by the occupier, however, I would expect all of these facilities to be installed by the developer, otherwise there will be a need to retro-fit. Although there is an action plan, there is also a list of proposed measures in Table 6.1 as well as a description of other measures in Section 6. The content of all these all differ slightly, which is confusing. For example, 6.3.6 refers to an annual newsletter, 6.7.2 refers to the TPC seeking cycle shop discounts etc. These 2 measures haven’t been included in Table 6.1 or Table 8.1. It would be clearer to have one action plan which includes all measures; the detailed description of these measures can still be retained, but all measures should appear in the action plan.
  - (f) Different organisations are likely to occupy the units. And there is a risk that the units won’t be fully occupied for some time. Therefore, a survey should be undertaken within 3 months of occupation, regardless of occupancy in order to obtain base line data.
  - (g) No specific Single Occupancy Vehicle target is proposed. “Driving a car or van” is listed in Table 5.2, but this should be split into “driving alone” and “car sharing”.
  - (h) Interim targets have been identified for Yrs 3 + 5. However, Year1 targets are also required.
  - (i) Wording should also be included to state that the resulting monitoring reports will be submitted to SCC and to the LPA.
12. The developer would be expected to instruct an independent transportation data collection company to undertake the monitoring survey. This survey should conform to a TRICS Multi-Modal Survey format consistent with the UK Standard for Measuring Travel Plan Impacts as approved by the Highway Authority. To ensure that the survey represents typical travel patterns, the organisation taking ownership of the travel plan will need to agree to being surveyed only within a specified annual quarter period but with no further notice of the precise survey dates. The Developer would be expected to fund the survey validation and data entry costs.

13. The developer is advised that as part of the detailed design of the highway works required by the above condition(s), the County Highway Authority may require necessary accommodation works to street lights, road signs, road markings, highway drainage, surface covers, street trees, highway verges, highway surfaces, surface edge restraints and any other street furniture/equipment.
14. If the proposed works affect an Ordinary Watercourse, Surrey County Council as the Lead Local Flood Authority should be contacted to obtain prior written consent.
15. The developer is reminded that, as a riparian owner of the Cross Oak Lane ditch, it is their responsibility to ensure that the watercourse is kept in a good condition and free from blockages.
16. Any works within 8m of the ditch/river will require a Flood Risk Activity Permit. For further information, please see [www.gov.uk/guidance/flood-risk-activities-environmental-permits](http://www.gov.uk/guidance/flood-risk-activities-environmental-permits)
17. The developer is reminded of the need to comply with Network Rail requirements and standards for the safe operation of the railway and the protection of Network Rail's adjoining land, both during construction and after completion of works. In particular, the developer must ensure that, both during construction and after completion of the works, the proposal does not encroach onto Network Rail land, affect the safety, operation or integrity of the company's railway and its infrastructure, undermine its support zone, damage the company's infrastructure, place additional load on cuttings, adversely affect any railway land or structure, over-sail or encroach upon the air space of any Network Rail land or cause to obstruct or interfere with any works or proposed works or Network Rail development both now and in the future. Network Rail strongly recommends that the developer contacts Network Rail Asset Protection London South East at [assetprotectionsussex@networkrail.co.uk](mailto:assetprotectionsussex@networkrail.co.uk) prior to any works commencing on site.
18. The use of a suitably qualified arboricultural consultant is essential to provide acceptable supervision and monitoring in respect of the arboricultural issues in respect of the above condition. All works shall comply with the recommendations and guidelines contained within British Standard 5837.
19. The use of landscape/arboricultural consultant is considered essential to provide acceptable submissions in respect of the above relevant conditions. Replacement planting of trees and shrubs shall be in keeping with the character and appearance of the locality.
20. The applicant's attention is drawn to the specifics of the contaminated land conditional wording such as 'prior to commencement', 'prior to occupation' and 'provide a minimum of two weeks' notice'. The submission of information not in accordance with the specifics of the planning conditional wording can lead to delays in discharging conditions, potentially result in conditions being unable to be discharged or even enforcement action should the required level of evidence/information be unable to be supplied. All relevant information should be formally submitted to the Local Planning Authority and not direct to Environmental Health.

21. The applicant is advised that the Borough Council is the street naming and numbering authority and you will need to apply for addresses. This can be done by contacting the Address and Gazetteer Officer prior to construction commencing. You will need to complete the relevant application form and upload supporting documents such as site and floor layout plans in order that official street naming and numbering can be allocated as appropriate. If no application is received the Council has the authority to allocate an address. This also applies to replacement dwellings.

If you are building a scheme of more than 5 units please also supply a CAD file (back saved to 2010) of the development based on OS Grid References. Full details of how to apply for addresses can be found [http://www.reigate-banstead.gov.uk/info/20277/street\\_naming\\_and\\_numbering](http://www.reigate-banstead.gov.uk/info/20277/street_naming_and_numbering)

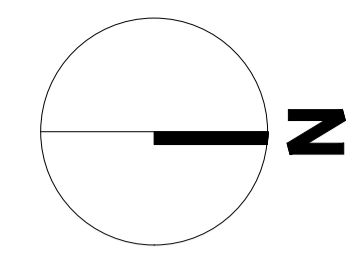
### **REASON FOR PERMISSION**

The development hereby permitted has been assessed against development plan policies CS1, CS4, CS5, CS8, CS10, CS11, CS12, CS17, Em1, Em1A, Em2, Em3, Em8, Mo3, Mo4, Mo5, Mo6, Mo7 and Ut4 and material considerations, including third party representations. It has been concluded that the development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.

The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.







Unit	GEA			CIH (m)
	Ground	First	Total	
1	26,136 ft² / 2,428 m²	4,671 ft² / 434 m²	30,807 ft² / 2,862 m²	10.5
2	22,310 ft² / 2,073 m²	3,475 ft² / 323 m²	25,785 ft² / 2,396 m²	10.5
3	17,910 ft² / 1,664 m²	2,785 ft² / 259 m²	20,695 ft² / 1,923 m²	10.5
4	42,723 ft² / 3,969m²	6,815 ft² / 633 m²	49,538 ft² / 4,602 m²	12.5
5	35,553 ft² / 3,301 m²	5,801 ft² / 539 m²	41,354 ft² / 3,840m²	12.5
T	144,632 ft² / 13,435 m²	23,547 ft² / 2,188 m²	168,179 ft² / 15,623m²	

SITE AREA - 7.71A / 3.12 HA  
 DEVELOPMENT DENSITY (GIA) - 48.8%

- KEY**
- Site application boundary
  - 2.4m high weld mesh fence
  - - - 4m acoustic fence
  - 
  - 
  - EV Electric vehicle charging point (x2 vehicles per post)
  - B Steel bollards (Green RAL 6018)
  - DB Directory Board
  - GAS Gas kiosk (Anthracite RAL 7016)
  - 
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Client: HE2 N1 SALFORDS 1 GP LIMITED

Project: CROSS OAK LANE, SALFORDS

Drawing Title: PROPOSED SITE PLAN

Scale @ A1: 1:500

Checked by: AMc

Date: Apr 18

Job No: 10947

Stage: PL 002

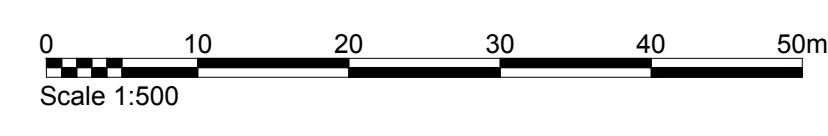
Drawing No: B

Issue Status: Construction  Preliminary  Information  Approval  Tender

Architecture: Planning, Master Planning, Urban Design, Interiors, Landscape

Offices: Working London, Milton Keynes, Warsaw

PRC Architecture & Planning



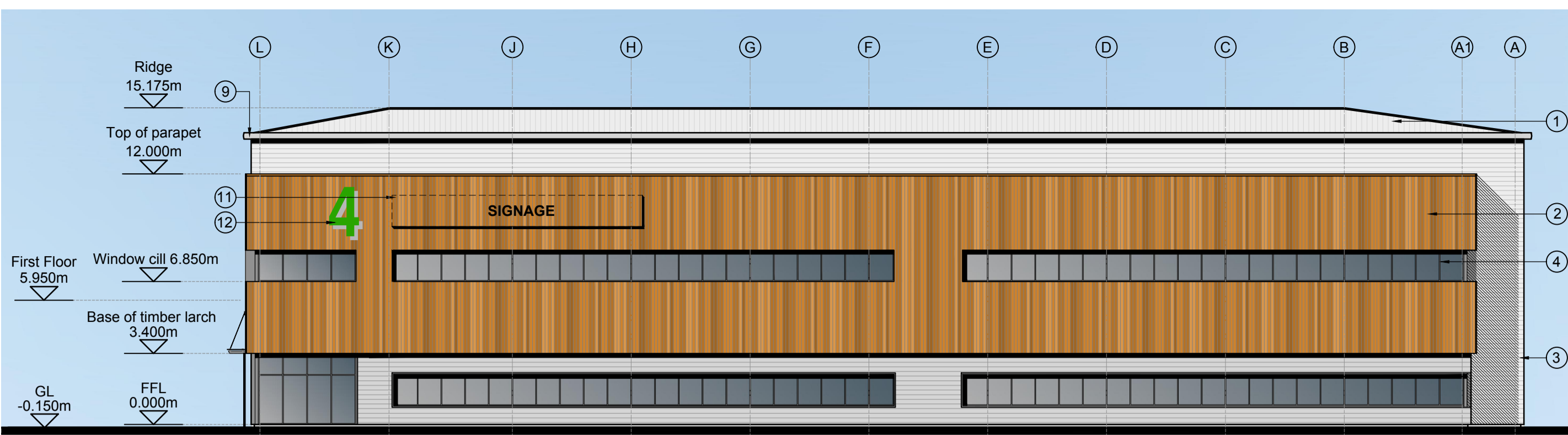


Figured dimensions only are to be used. All dimensions to be checked onsite. Differences between drawings and between drawings and specification or bills of quantities to be reported to the PRC Group.

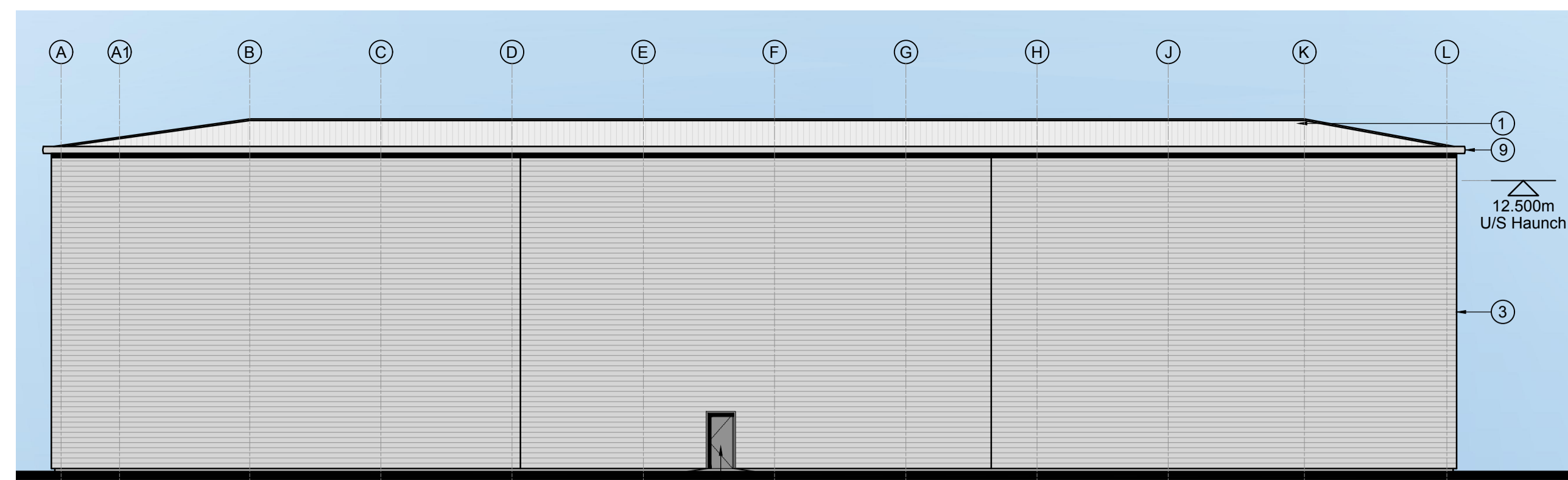
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Revisions: Drawn/Chkd: Date: HB SEP 18

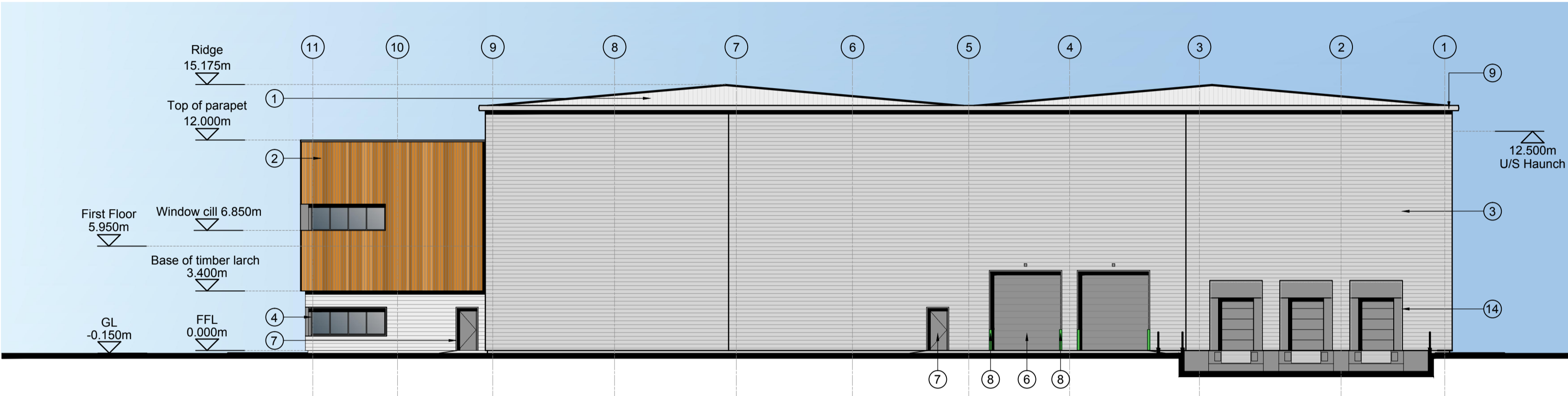
A. Changes made in accordance with planners comments. Building areas adjusted accordingly.



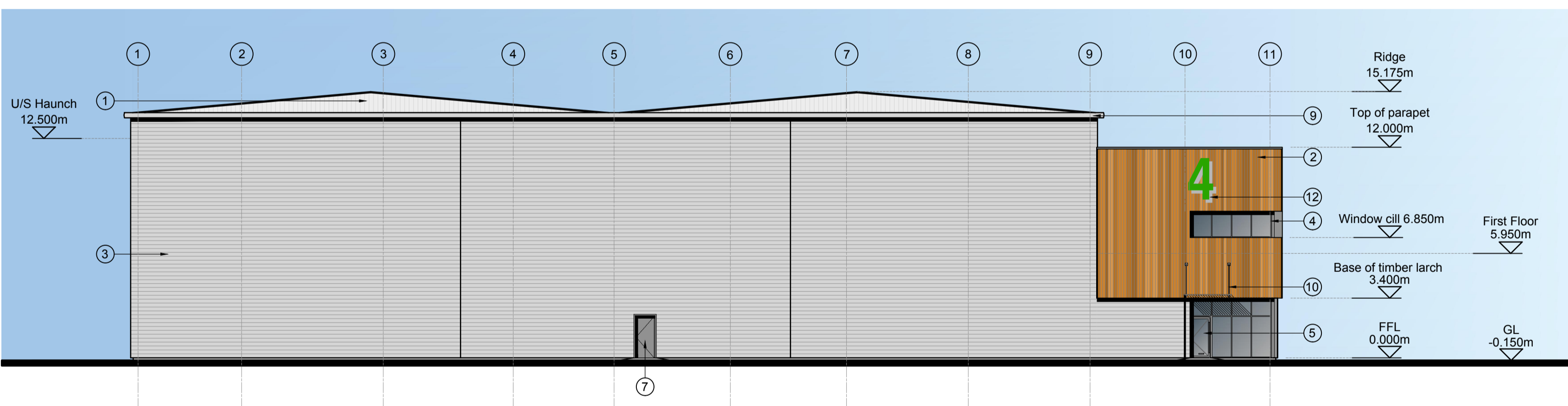
WEST ELEVATION



EAST ELEVATION

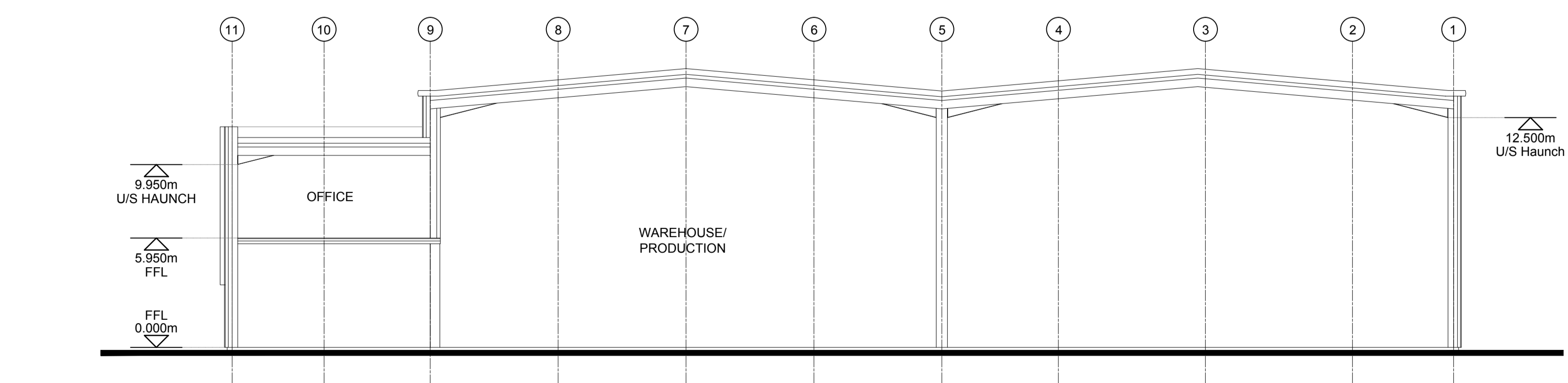


SOUTH ELEVATION

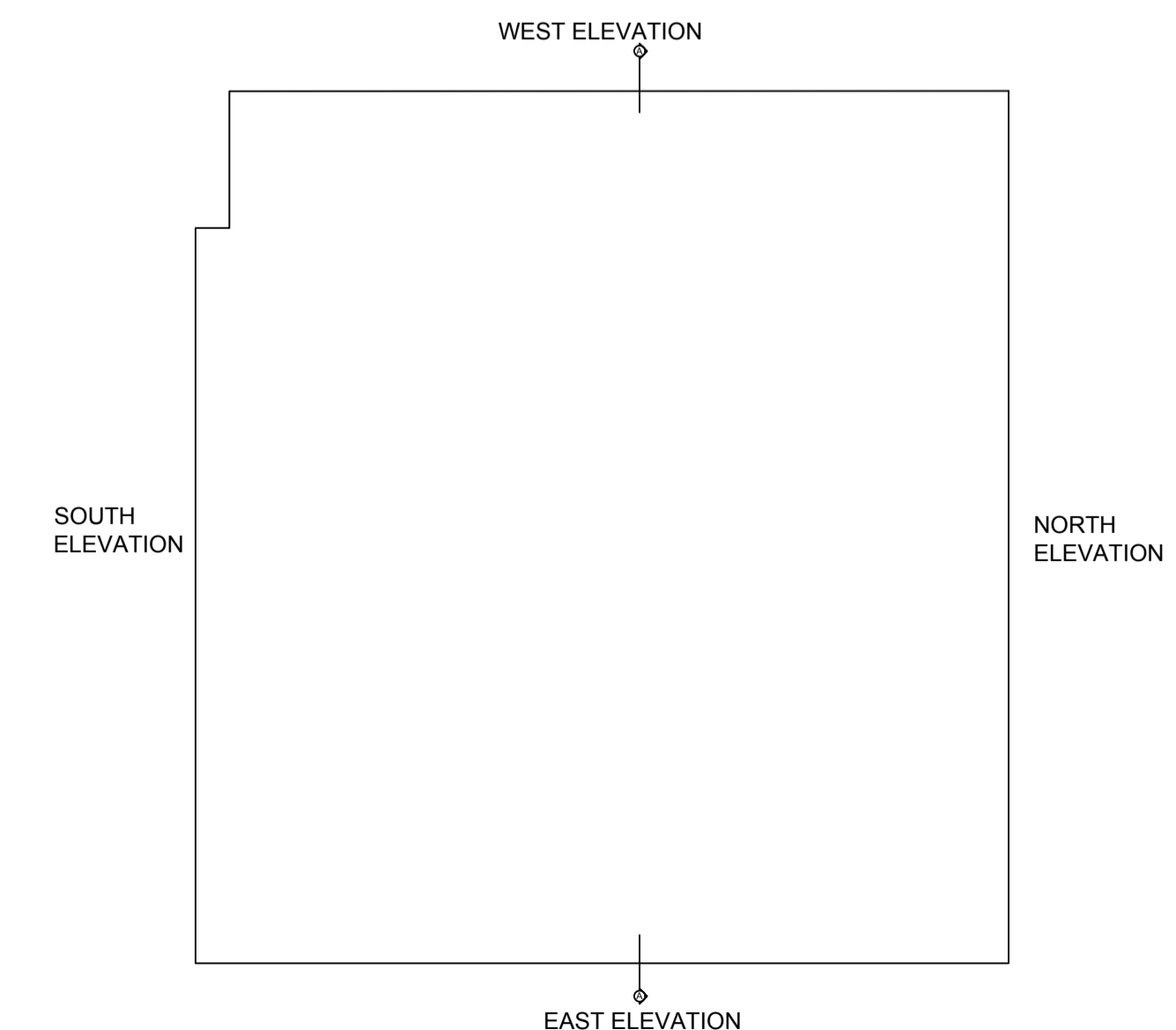
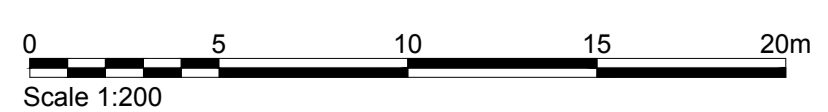


NORTH ELEVATION

- MATERIALS KEY**
- ① Profiled roof cladding (Goosewing grey RAL 7038)
  - ② Siberian larch cladding
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  - ⑩ Glazed canopy with gravity fed rainwater pipe
  - ⑪ Signage zone by others
  - ⑫ Backlit illuminated aluminium unit numeral (RAL 6018 face)
  - ⑬ Ceramic backed glazing infill panel
  - ⑭ Dock leveller (Anthracite RAL 7016)



SECTION A - A



REFERENCE PLAN

Client: HE2 N1 SALFORDS 1 GP LIMITED



Project: CROSS OAK LANE, SALFORDS

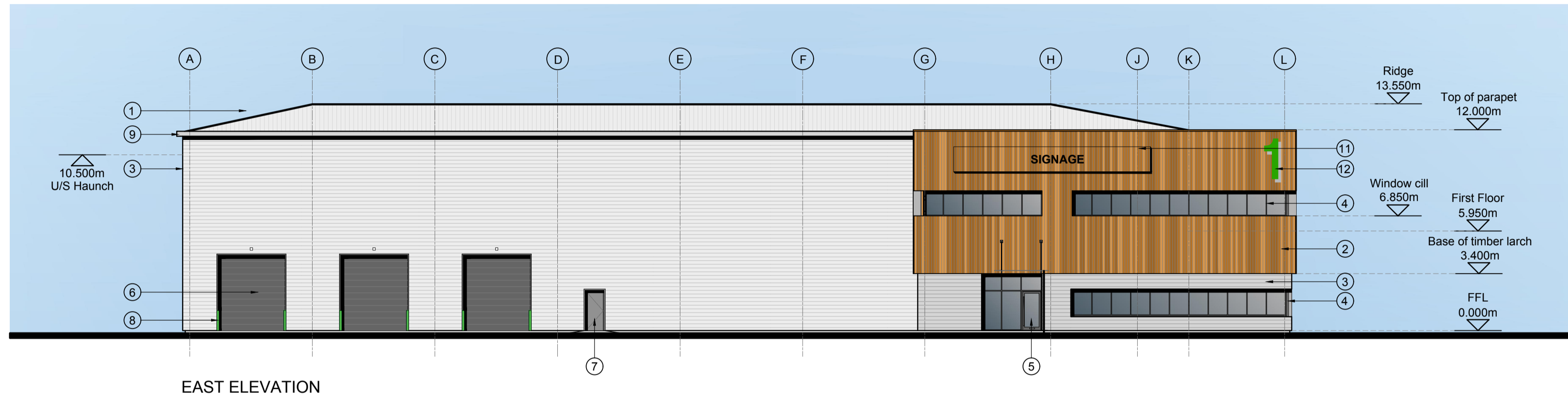
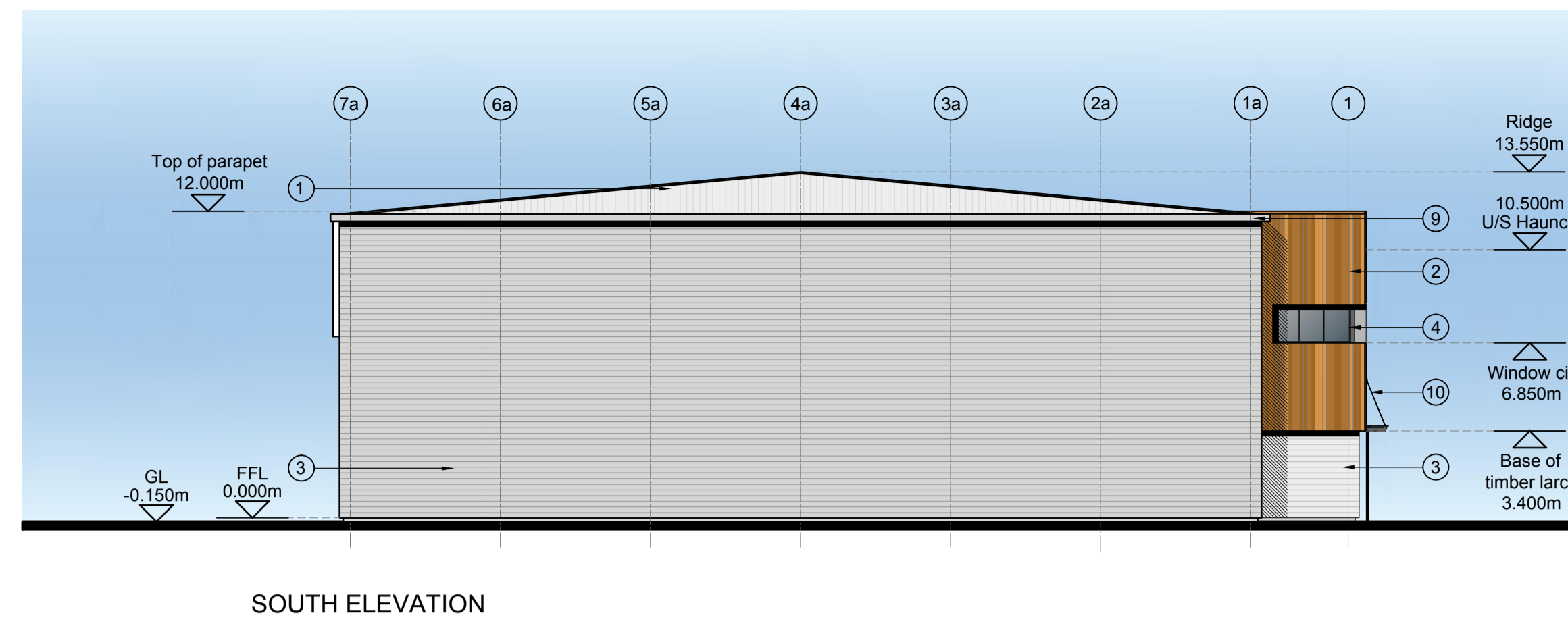
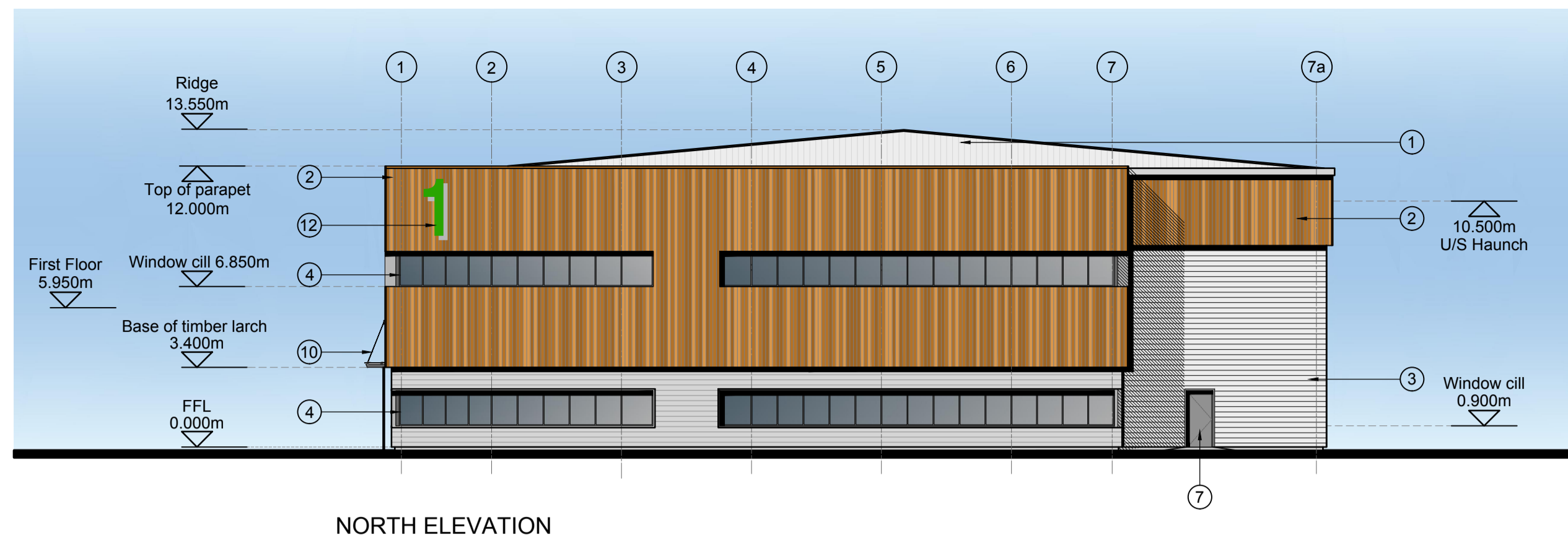
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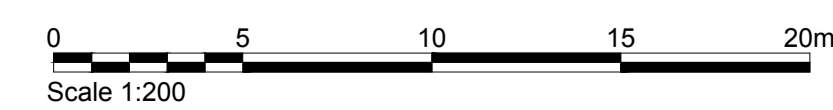
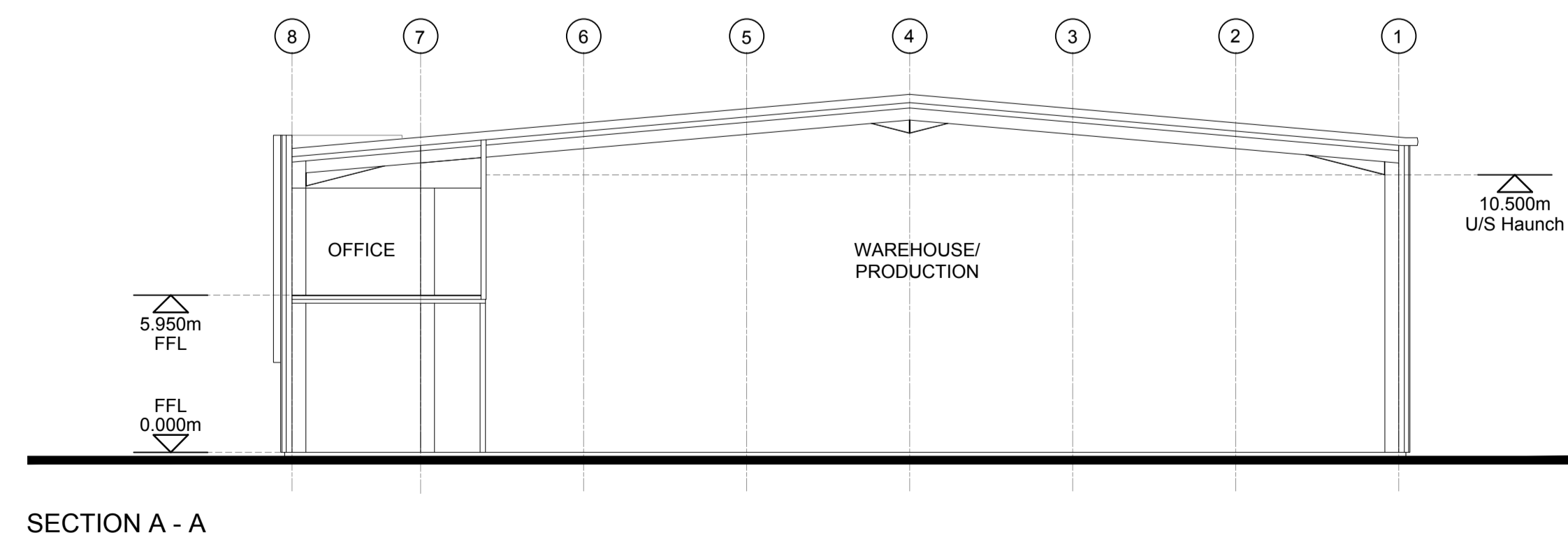
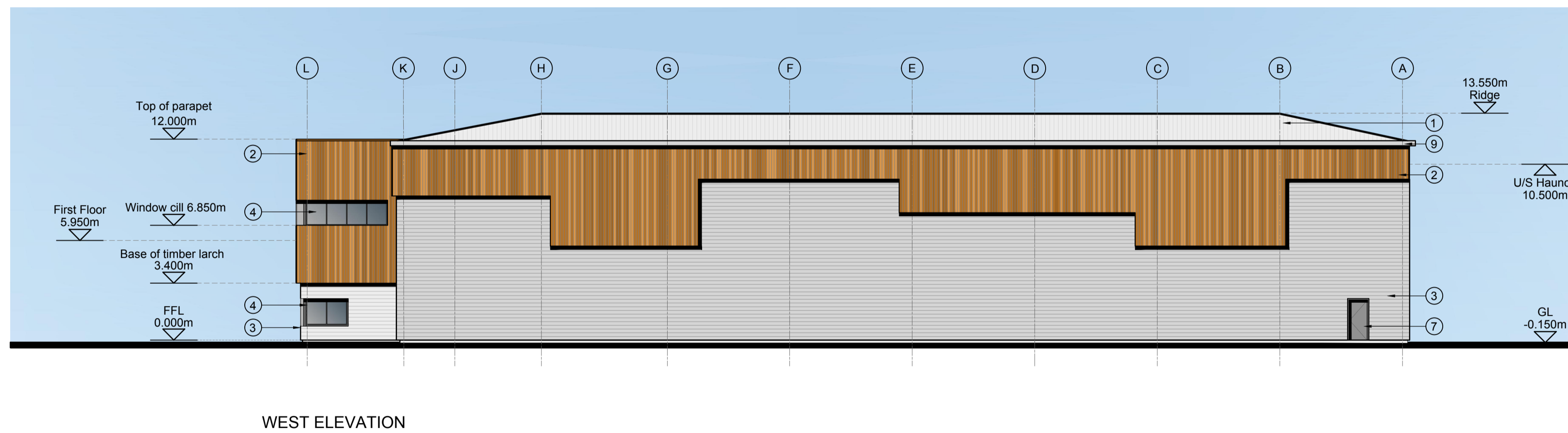
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Job No: 10947 Stage: PL 011 Drawing No: Rev: A

Issue Status: Construction  Preliminary  Information  Approval  Tender  Working London Milton Keynes Warsaw



- MATERIALS KEY**
- ① Profiled roof cladding (Goosewing grey RAL 7038)
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84

Client:  
HE2 N1 SALFORDS 1 GP LIMITED



Project:  
CROSS OAK LANE, SALFORDS

12 Warren Yard,  
Warren Park,  
Milton Keynes.  
MK12 5HW  
01908 305 248

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www.prc-group.com

Drawing Title:  
UNIT 1  
PROPOSED ELEVATIONS

Scale @ A1: 1:200  
Checked by: AMc  
Date: SEP 18

Job No: 10947  
Stage: PL 009  
Drawing No: A

Issue Status:  
 Construction  Preliminary   
 Information  Approval   
 Tender

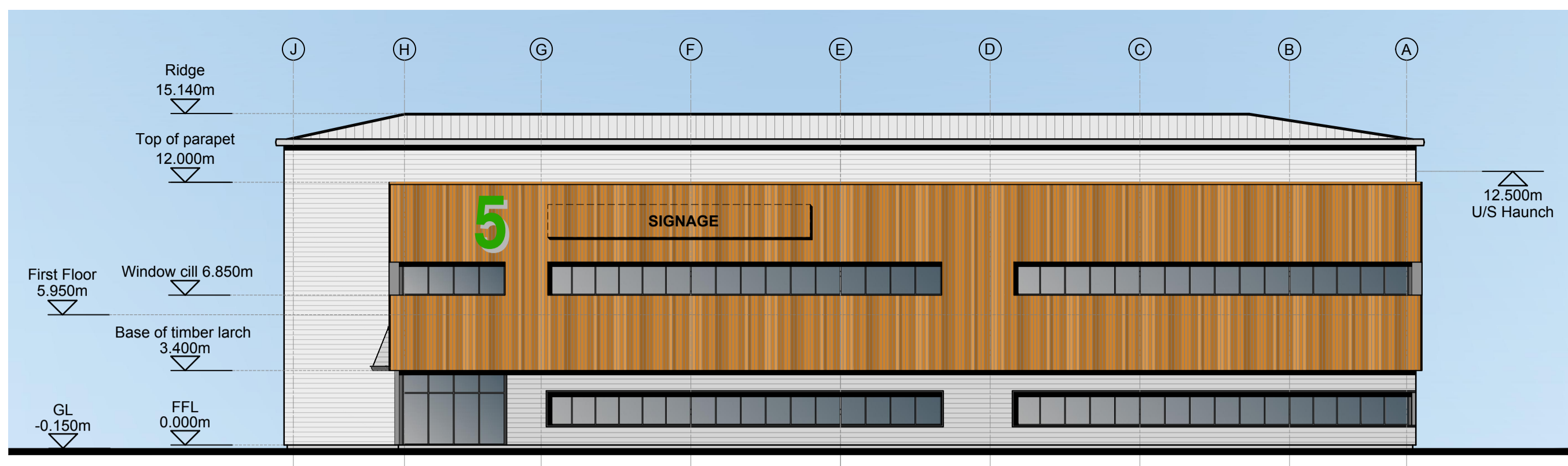
**Offices**  
 Working London  
 Milton Keynes  
 Warsaw



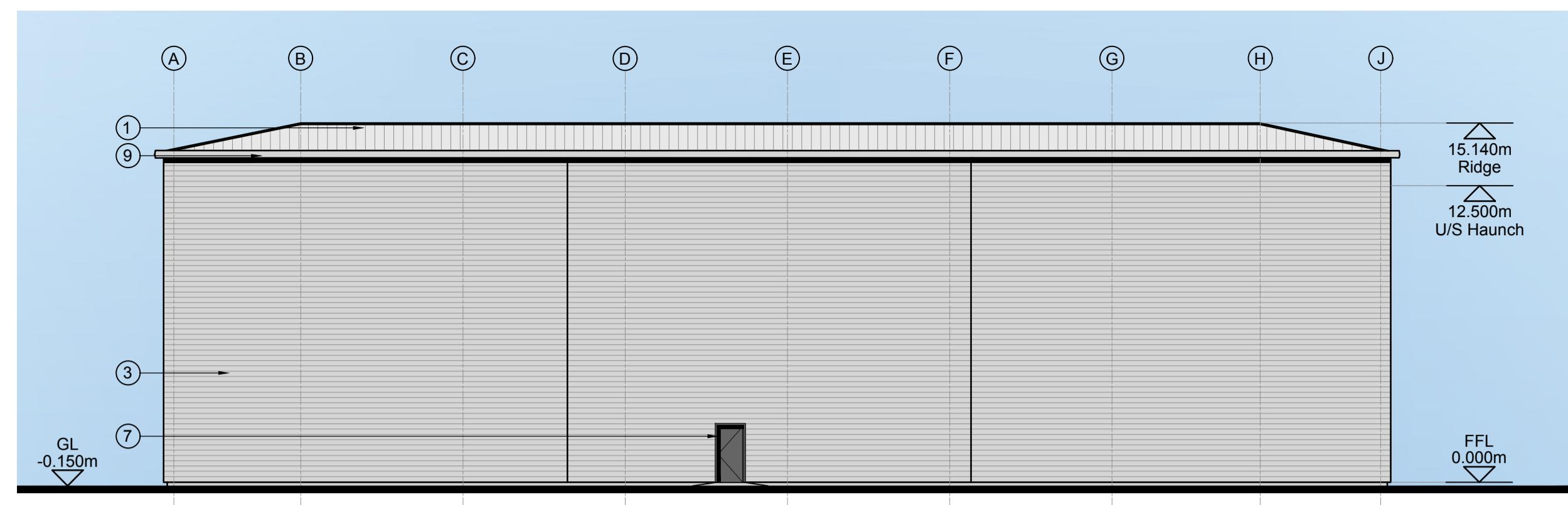
Figured dimensions only are to be used. All dimensions to be checked onsite. Differences between drawings and between drawings and specification or bills of quantities to be reported to the PRC Group.

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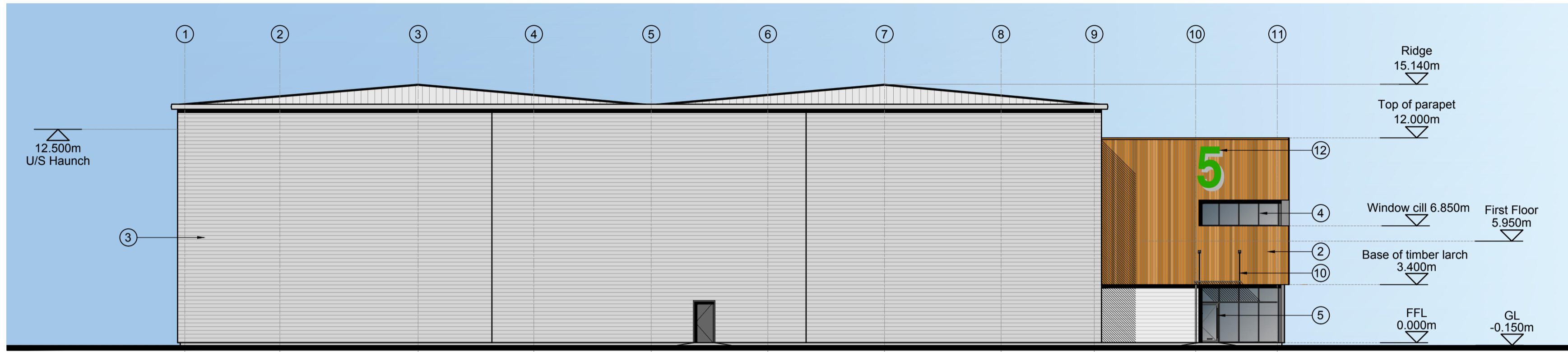
Revisions: Drawn/Chkd: Date:  
 A. Changes made in accordance with planners comments. Building areas adjusted accordingly. HB SEP 18



WEST ELEVATION



EAST ELEVATION



NORTH ELEVATION

**MATERIALS KEY**

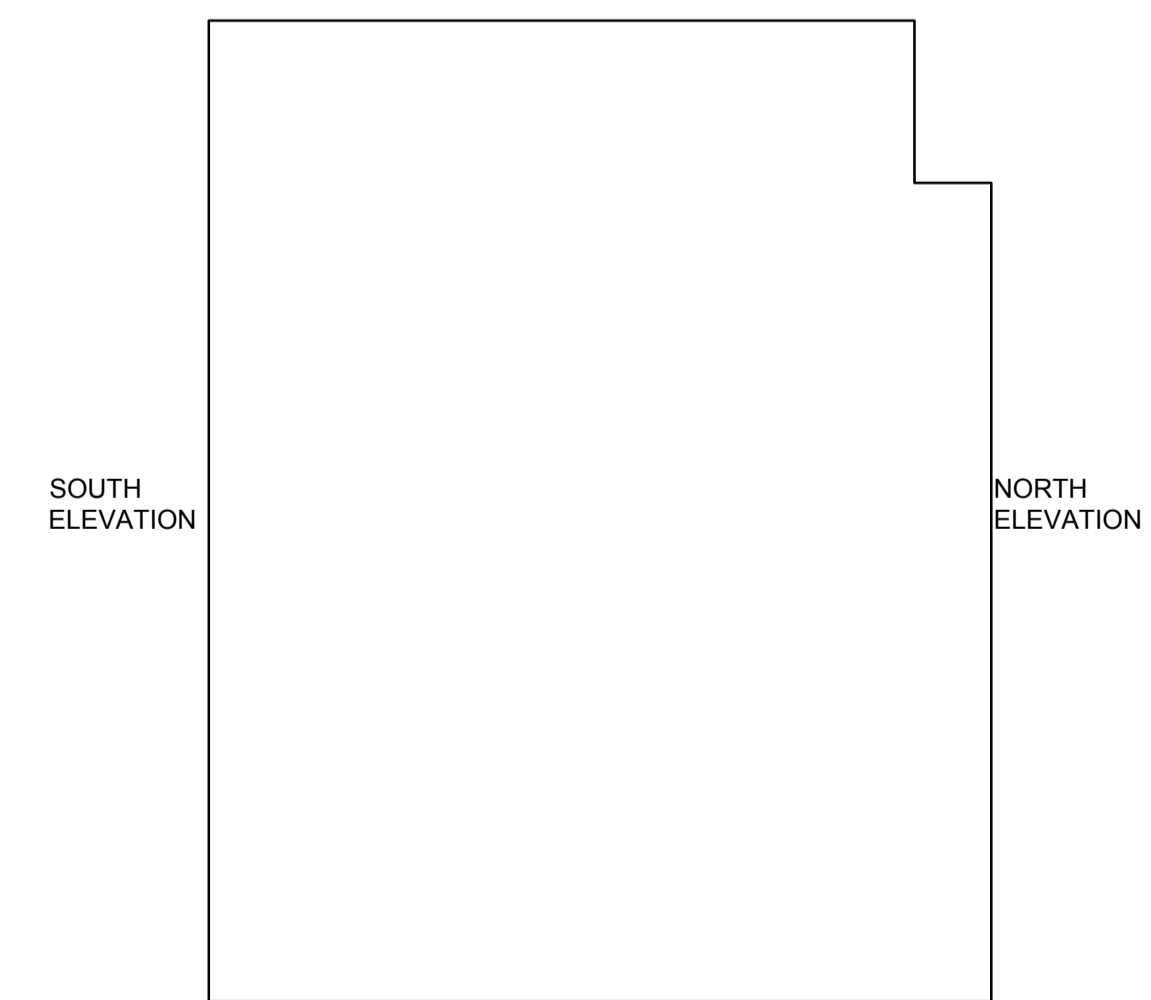
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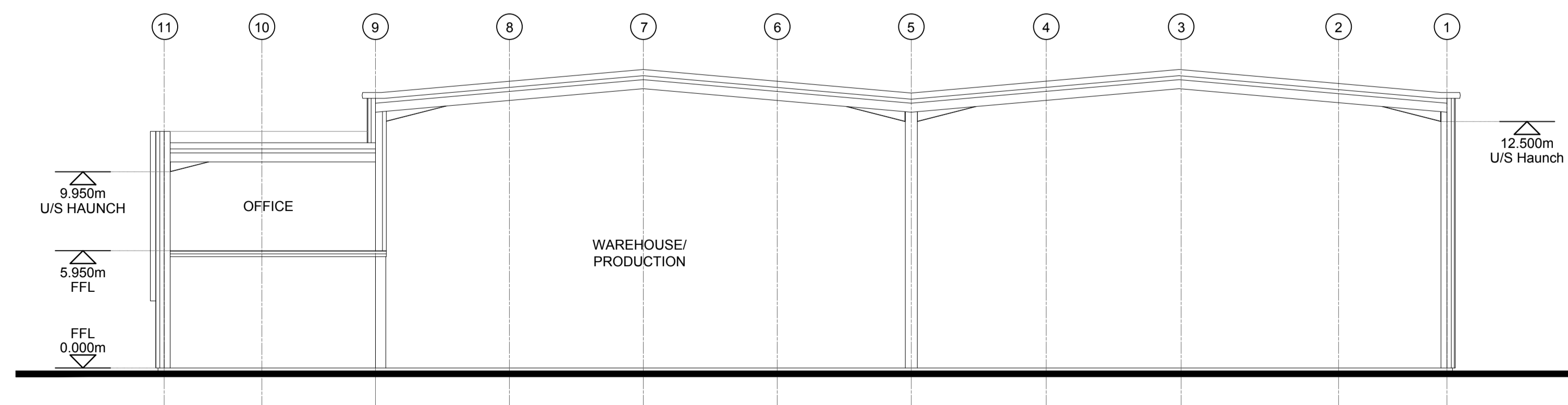


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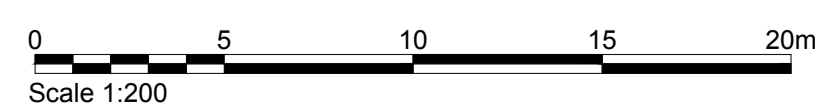
WEST ELEVATION



REFERENCE PLAN



SECTION A - A



Client:  
HE2 N1 SALFORDS 1 GP LIMITED



Project:  
CROSS OAK LANE, SALFORDS

12 Warren Yard,  
Warren Park,  
Milton Keynes.  
MK12 5HW  
01908 305 246

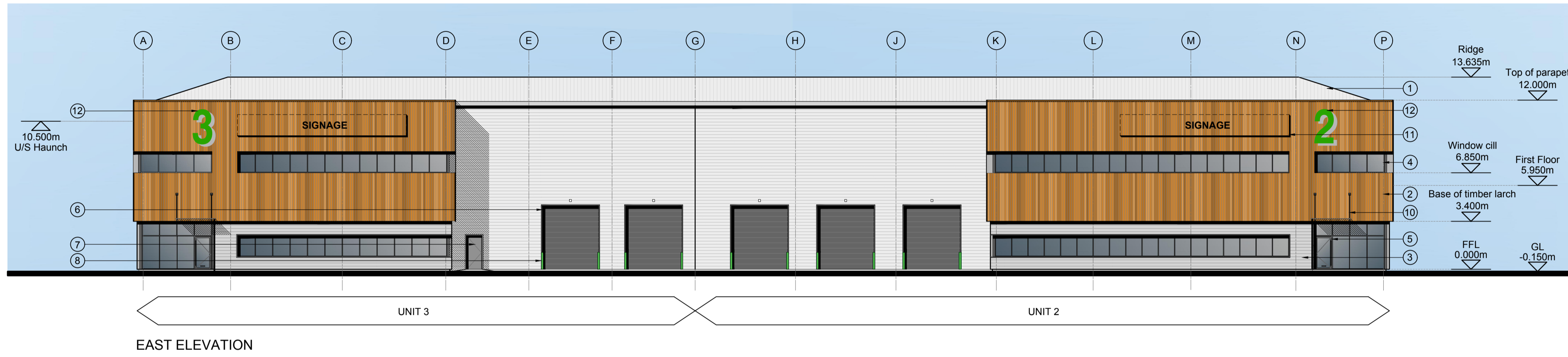
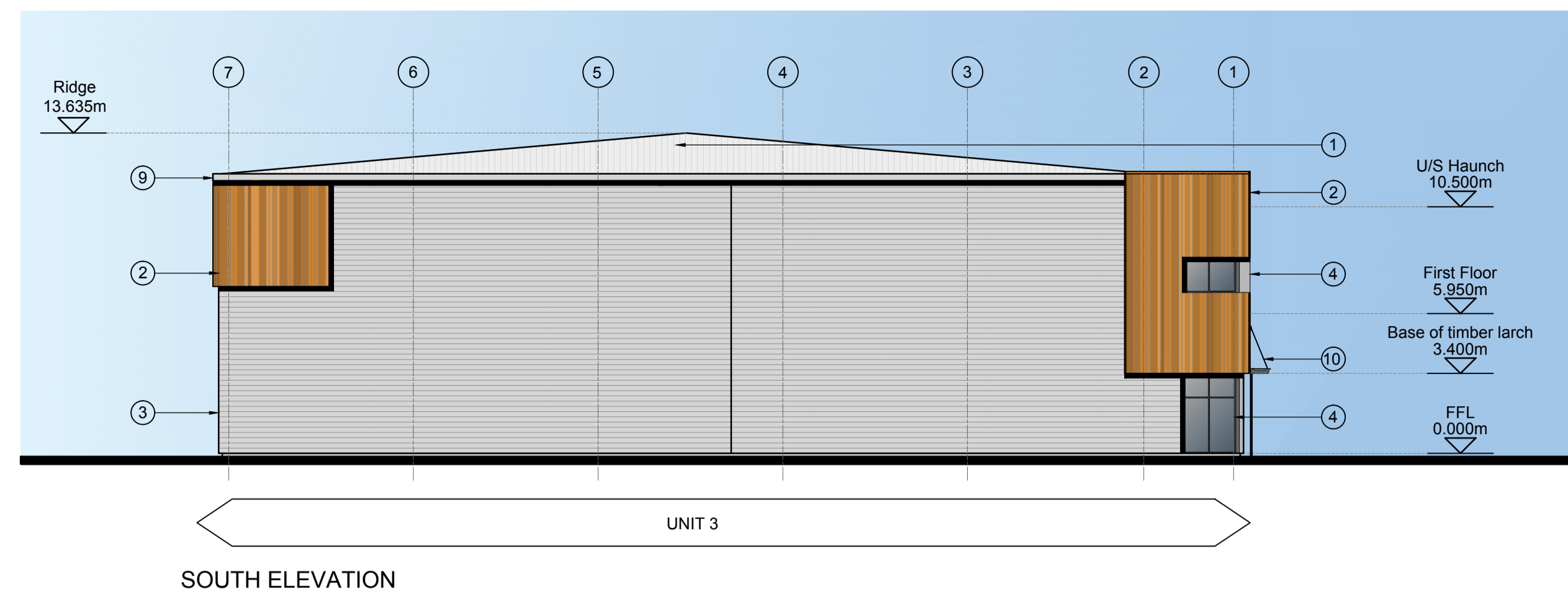
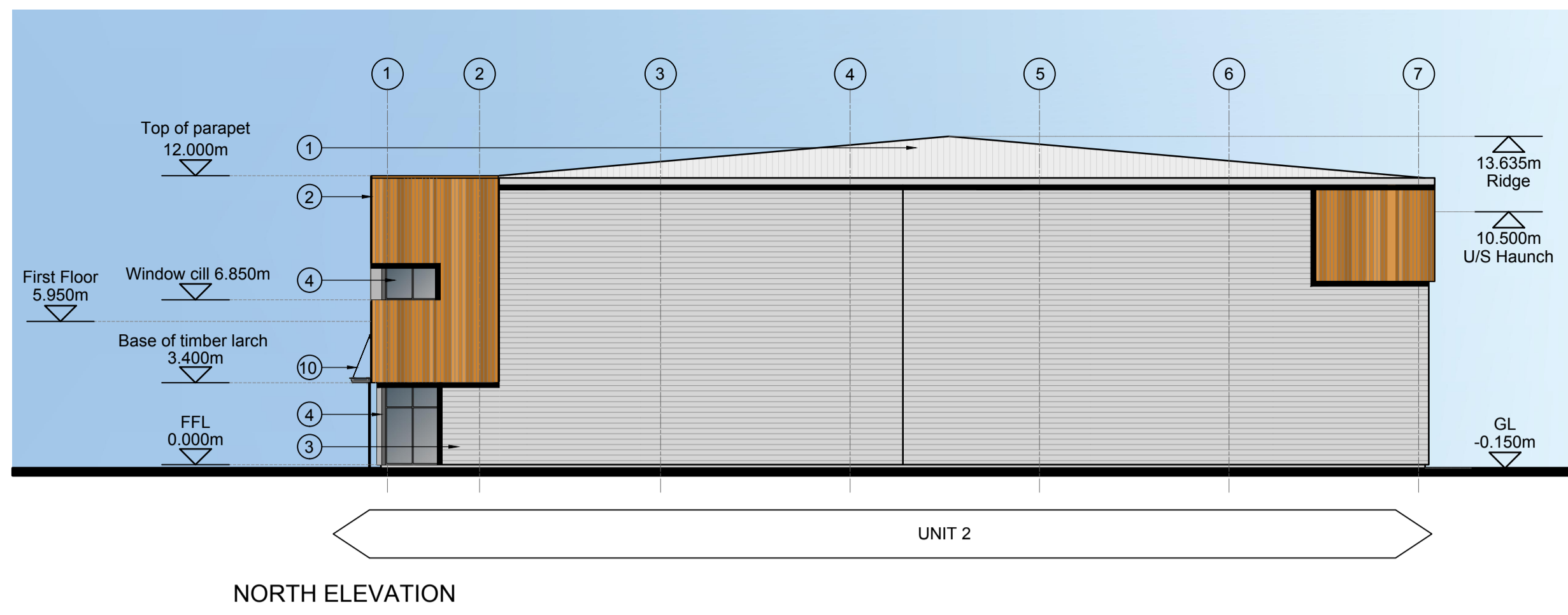
info@prc-group.com  
www.prc-group.com

Drawing Title:  
UNIT 5  
PROPOSED ELEVATIONS

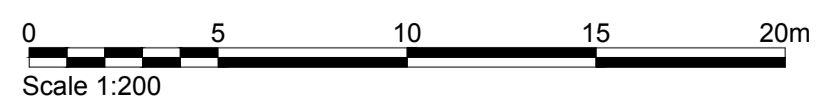
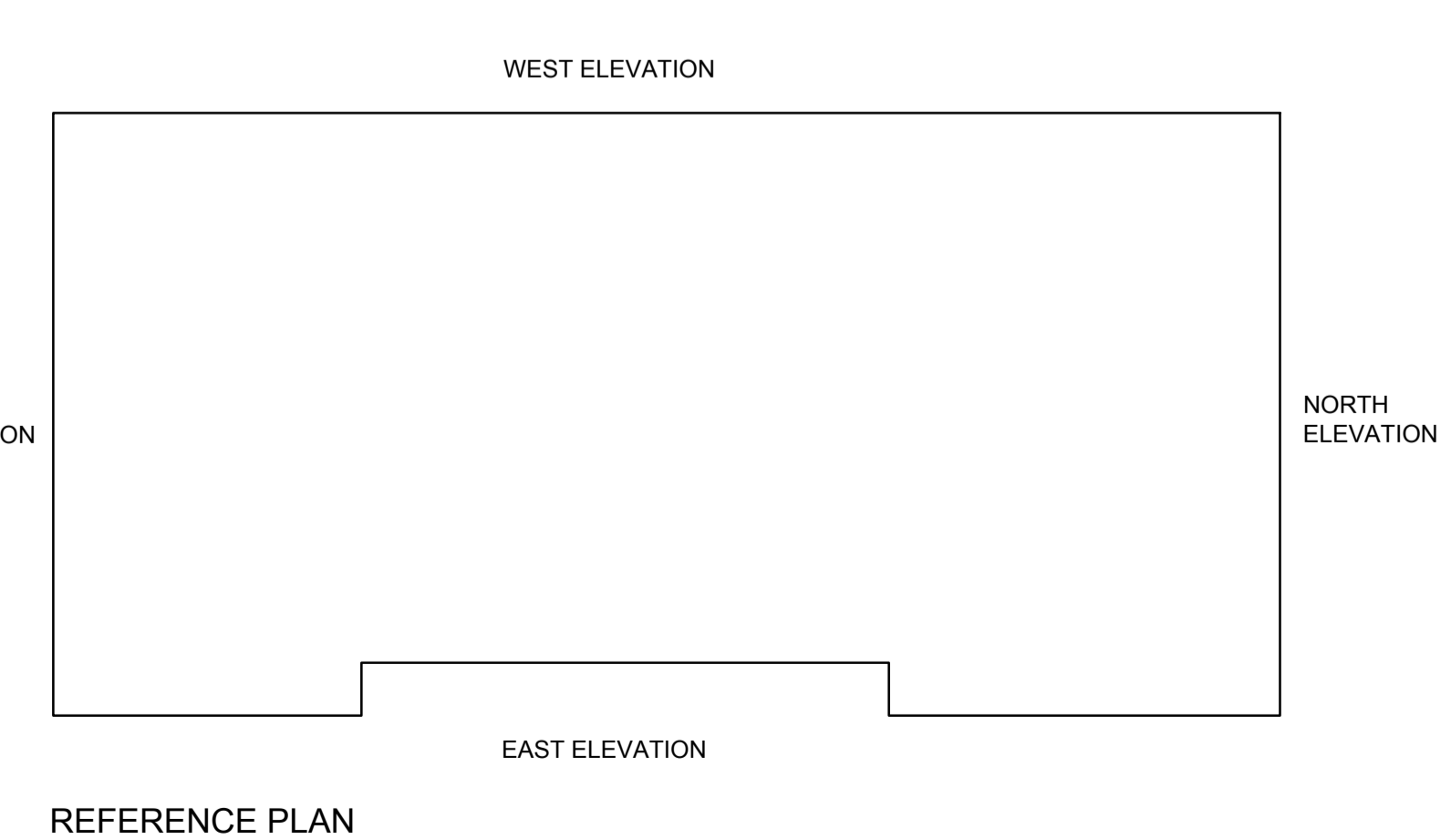
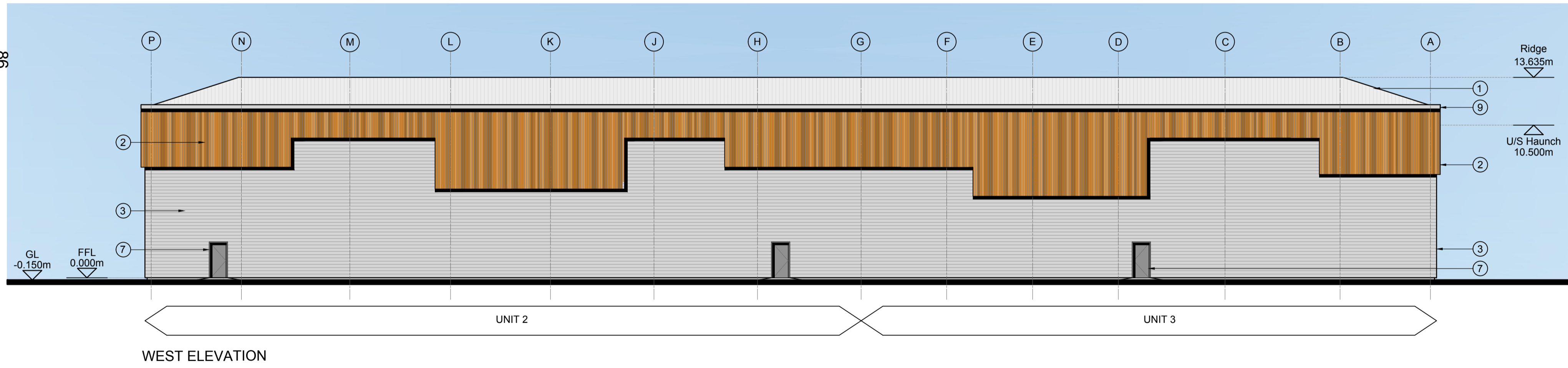
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Date: MAY 18

Job No: 10947  
Stage: Drawing No: PL 012  
Rev:

Issue Status:  
 Construction  Preliminary   
 Information  Approval   
 Tender   
**Offices**  
 Working London Milton Keynes Warsaw



- MATERIALS KEY**
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Client:  
HE2 N1 SALFORDS 1 GP LIMITED



Project:  
CROSS OAK LANE, SALFORDS

12 Warren Yard,  
Warren Park,  
Milton Keynes,  
MK12 5HW  
01908 305 248  
info@prc-group.com  
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Drawing Title:  
UNITS 2 - 3  
PROPOSED ELEVATIONS

Scale @ A1: 1:200  
Checked by: AMC  
Date: MAY 18

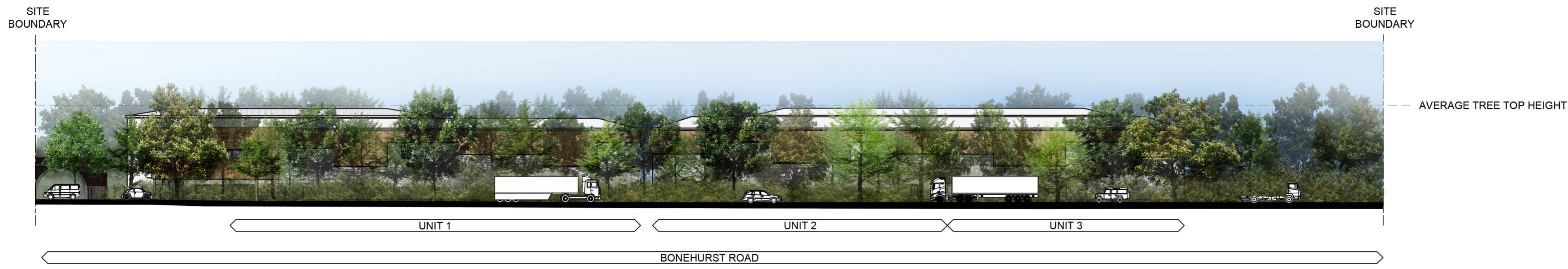
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Rev:

Issue Status:  
Construction  Preliminary   
Information  Approval   
Tender

**Architecture**  
Planning  
Master Planning  
Urban Design  
Interiors  
Landscape

**Offices**  
Woking  
London  
Milton Keynes  
Warsaw





SECTION D - D



SECTION B - B



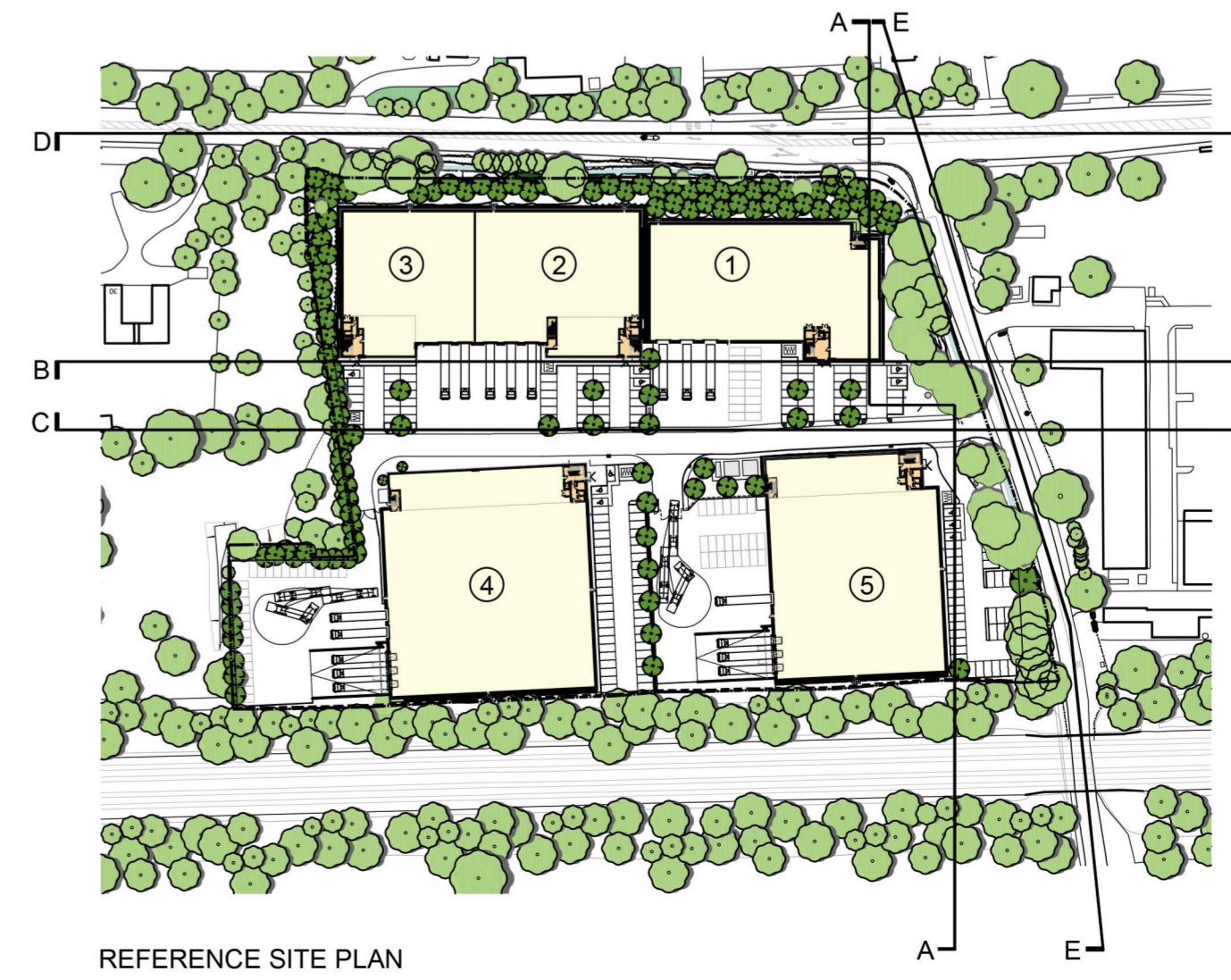
SECTION C - C



SECTION E - E



SECTION A - A



Client: HE2 N1 SALFORDS 1 GP LIMITED



Project: CROSS OAK LANE, SALFORDS

12 Warren Yard, Warren Park, Milton Keynes, MK12 5NW, 01908 305 246

info@prc-group.com www.prc-group.com

Drawing Title: SITE SECTIONS

Scale @ A1: 1:500

Checked by: AMC

Date: MAY 18

Job No: 10947

Stage: PL 017

Issue Status:  
 Construction  Preliminary   
 Information  Approval   
 Tender

Architecture  
 Planning  
 Master Planning  
 Urban Design  
 Interiors  
 Landscape

Offices  
 Woking  
 London  
 Milton Keynes  
 Warsaw




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# Agenda Item 7

Planning Committee  
31<sup>st</sup> October 2018

Agenda Item: 7  
18/01134/F

 <b>Reigate &amp; Banstead</b> BOROUGH COUNCIL Banstead   Horley   Redhill   Reigate	<b>TO:</b>	PLANNING COMMITTEE
	<b>DATE:</b>	31 October 2018
	<b>REPORT OF:</b>	HEAD OF PLACES & PLANNING
	<b>AUTHOR:</b>	John Ford
	<b>TELEPHONE:</b>	01737 276112
	<b>EMAIL:</b>	john.ford@reigate-banstead.gov.uk
<b>AGENDA ITEM:</b>	7	<b>WARD:</b> Tadworth and Walton

<b>APPLICATION NUMBER:</b>	18/01134/F	<b>VALID:</b>	<b>30/07/2018</b>
<b>APPLICANT:</b>	Devine Homes PLC	<b>AGENT:</b>	
<b>LOCATION:</b>	<b>LAND TO REAR 19-29 SHELVERS WAY, TADWORTH</b>		
<b>DESCRIPTION:</b>	<b>Erection of 6 dwellings comprising 2 x pair of semi detached dwellings, and 2 x detached dwellings along with access to Shelvers Way utilising the approved access from 17/00016/S73, and all associated landscaping, and ancillary work.</b>		
<b>All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail.</b>			

## SUMMARY

The application seeks planning permission for 6 detached/semi-detached dwellings of a style, scale and layout similar to the approved schemes to the east.

The application relates to a backland site within the urban area towards the eastern end of Shelvers Way, a residential thoroughfare composed mainly of properties fronting the road. The site is to the west of land to the rear of 1-7 Shelvers Way, which has the benefit of planning permission for 6 dwellings, now completed (17/00016/S73 following allowing of appeal under ref.15/02752/F); and to the rear of 9-17 for 4 dwellings (17/02097/F) involving an elongation of the access drive permitted under application ref: 17/00016/S73. Various trees on the site are the subject of a Tree Preservation Order (TPO).

The proposal would provide a layout and a scale of development with similar house designs and opportunity for landscaping consistent with the approved development as identified above. The Inspector, in determining the appeal on application 15/02752/F, concluded in that case that whilst *“The two detached and four semi-detached dwellings would occupy smaller plots than is characteristic along the south side of Shelvers Way and the spacing between the four building blocks would be less generous. However in the context of a new group of buildings that would be seen primarily from within the development, rather than in association with the Shelvers Way street scene, the plot sizes and the spacing between the buildings would not be unusual for this type of development: they would not result in an unduly cramped form of development.”* With regard to the layout and landscaping

the Inspector concluded also that: *“The width of the corridor for the access road and the overall layout in front of the houses would create opportunities for additional landscaping despite the relatively small spaces of differing sizes and shapes directly in front of each house.”*

The proposal represents an acceptable continuation of the approved schemes. In light of this and the similar residential impacts on amenity and privacy with neighbouring houses to that already considered to be consistent with policy and approved it is concluded that this proposal would provide a suitable residential environment and not be harmful to the local character or amenity of neighbouring properties.

As regards intensification of use of the access onto Shelveys Way that this proposal would result in, the impact on the highway network has been the subject of survey analysis and detailed review by SCC as the County Highway Authority (CHA). The CHA is satisfied that the access is safe and suitable to accommodate the cumulative traffic that would be generated by the current proposal and the development already approved and, in the case of the dwellings to the rear of nos. 1-7, completed/occupied.

## **RECOMMENDATION**

Planning permission is **GRANTED** subject to conditions.



## Consultations:

Highway Authority: recommends conditions relating to parking provision and Construction Transport Management Plan. The Highway Authority advises that improvements to the visibility and entry layout for the Shelveys Way junction may be required should there any further incremental increase in the number of dwellings.

Neighbourhood Services: recommends bin presentation points.

Banstead Commons Conservators: wishes to draw the applicant's attention to the proximity of the site to Banstead and the need for the Commons' protection.

Tadworth and Walton Residents' Association: objects on grounds of cramped form of development, threat to trees' wellbeing and highway safety.

## Representations:

Letters were sent to neighbouring properties 8 August 2018 and a site notice was posted 23 August 2018.

Two letters of support have been submitted. Twenty-three responses have been received raising the following issues:

<b>Issue</b>	<b>Response</b>
Inadequate parking	See paragraphs 6.8 - 6.10
No need for the development	Consideration on merits
Noise & disturbance	See paragraph 6.7
Inconvenience during construction	See paragraph 6.7
Overdevelopment	See paragraphs 6.3 - 6.5
Increase in traffic and congestion	See paragraphs 6.8 - 6.10
Out of character with locality	See paragraphs 6.3 - 6.5
Hazard to highway safety	See paragraphs 6.8 - 6.10
Harm to Conservation Area	Not in a Conservation Area
Conflict with covenant	Not a planning consideration
Crime Fears	Site capable of surveillance
Harm to MGB	Not in MGB
Harm to wildlife habitat	No wildlife designation
Overbearing effect	See paragraph 6.6
Loss of/harm to trees	See paragraph 6.7
Overlooking and Loss of privacy	See paragraph 6.7
Property devaluation	Not a planning consideration
Poor design	See paragraphs 6.3 - 6.5

## **1.0 Site and Character Appraisal**

- 1.1 The application site with an area of 0.31ha is on the south side of Shelveys Way approaching its eastern end, near the junction with the A217 (Brighton Road) dual carriageway in a predominantly residential neighbourhood approximately 1.7 km to the south east of Tattenham Corner Local Shopping Centre and approximately 1km to the east of Shelveys Way Local Shopping Centre. Immediately to the south is Urban Open Land. The site is immediately to the west of the approved development of four houses to the rear of 9-17 Shelveys Way (ref. 17/02097/F) and, to the east of that, six houses on the demolished Stanton Lodge and to the rear of 1-7 Shelveys Way, which have been completed (ref. 17/00016/S73) following the grant of planning permission on appeal, Ref: 15/02752/F. The current proposal would be served by an extension of the access drive serving the development to the rear of nos. 9-17.
- 1.2 The Council's Local Distinctiveness Design Guide identifies the locality as 1930s-1950s Suburbia. The development pattern comprises detached dwellings in long plots interspersed with small infill post 1970's cul-de-sacs and more recent infill development. Regard must also be had to the approved and completed backland development to the east to the rear of 1-7 Shelveys Way and the approved scheme to the rear of 9-17: this is a new element modifying the neighbourhood's traditional character of frontage houses with long rear gardens.
- 1.3 To the east, beyond the approved development, is a landscaped buffer with mature trees alongside the A217, within the Metropolitan Green Belt (MGB). Land immediately to the south is designated as Urban Open Land, in the ownership of The Tadworth Children's Trust. Land to the north and west consists of the residential curtilages of properties in Shelveys Way.
- 1.4 A Tree Preservation Order (TPO) RE1462A covers 1-31 Shelveys Way. The site does not fall within a Conservation Area or Residential Area of Special Character (RASC) nor is it recognised as a significant wildlife habitat.
- 1.5 Vegetation along the site boundaries provides some screening.

## **2.0 Added Value**

- 2.1 Improvements secured at the pre-application stage: the applicant did not approach the Council for pre-application advice therefore the opportunity to secure improvements did not arise.
- 2.2 Further improvements could be secured by conditions relating to landscaping, materials and detailing.

### 3.0 Relevant Planning and Enforcement History

3.1	15/02752/F	6 dwellings, Stanton Lodge and r/o 1-7 Shelveys Way	Refused 31 March 2016 Appeal allowed 14 November 2016
3.2	16/01437/F	5 dwellings, Stanton Lodge and r/o 1-7 Shelveys Way	Refused 1 September 2016 Appeal allowed 28 December 2016
3.3	17/00016/S73	6 dwellings, Stanton Lodge and r/o 1-7 Shelveys Way	Granted 1 March 2017
3.4	17/02097/F	4 dwellings, r/o 9-17 Shelveys Way	Granted 21 December 2017
3.5	18/00082/F	Pair of 3 bedroom semi-detached dwellings and parking to replace the single detached 4 bedroom house located at plot 2 permitted under application reference 17/02097/F	Refused 9 March 2018 Appeal lodged

### 4.0 Proposal and Design Approach

- 4.1 This is a full application for six 2 storey pitched roofed dwellinghouses to the rear of dwellings at 19 to 29 Shelveys Way, involving the rearmost parts of the gardens of those properties. The houses, all 3 bedroom, are arranged in an east/west line with a pair of semi-detached (Plots 1 & 2) towards the eastern end of the site, two detached in the middle grouped around a centrally placed turning head and two attached (by a garage) towards the western boundary. As regards parking provision, each house would be served by a single attached garage and a parking space, twelve spaces in total. Plots 1/2, 3, 4 and 5 would feature catslide roofs. Principal habitable room windows would be to the north and south elevations. The designs are traditional in appearance with a variety of materials including unspecified facing brickwork, render, plain roof and hanging tiles, uPVC windows and doors and timber boarding. The houses would be similar in scale to the approved four dwellings on land to the east in the applicant's ownership and with the benefit of permission ref. 17/2097/F. Access to the site from Shelveys Way would be via an extension to the access drive to serve the above-mentioned approved development.
- 4.2 The scheme reflects the scale, design, massing and layout of the approved schemes to the east. The proposal has parking provision arranged so as to avoid undue car dominance, comfortable accommodation of the protected beech and the opportunity for additional planting/landscaping particularly

along the northern boundary running along the remaining back gardens of the donor properties.

- 4.3 A design and access statement should illustrate the process that has led to the development proposal, and justify the proposal in a structured way, by demonstrating the steps taken to appraise the context of the proposed development. It expects applicants to follow a four-stage design process comprising:  
Assessment;  
Involvement;  
Evaluation; and  
Design.

- 4.4 Evidence of the applicant's design approach is set out below:

Assessment	The character of the surrounding area is assessed as predominantly residential, reflecting 1930s-1950s suburbia with detached dwellings in large elongated plots are interspersed with small infill post 1970s cul-de-sac's and more recent infill development.
	No site features worthy of retention were identified.
Involvement	No community consultation took place.
Evaluation	The statement does not include any evidence of other development options being considered.
Design	The applicant's reasons for choosing the proposal from the available options were an addition to and access via approved development on land in the applicant's ownership

- 4.5 Further details of the development are as follows:

Site area	0.31ha
Existing use	Residential garden
Proposed use	C3 (Dwellinghouses)
Proposed parking spaces	12
Parking standard	12 (recommended maximum)
Number of affordable units	0
Net increase in dwellings	6
Proposed site density	19.3dph
Density of the surrounding area	8.8dph (extent of coverage of site location plan)

## 5.0 Policy Context

### 5.1 Designation

Urban area  
Tree Preservation Order RE1462A (1-31 Shelveys Way)  
Adjacent to Urban Open Land

### 5.2 Reigate and Banstead Core Strategy

CS1(Sustainable Development)  
CS2 (Valued Landscapes and Natural Environment)  
CS4 (Valued Townscapes and Historic Environment)  
CS5 (Valued People/Economic Development)  
CS10 (Sustainable Development)  
CS11 (Sustainable Construction)  
CS14 (Housing Needs)  
CS15 (Affordable Housing)

### 5.3 Reigate & Banstead Borough Local Plan 2005

Landscape & Nature Conservation	Pc4
Housing	Ho9, Ho9A, Ho13, Ho14, Ho16
Movement	Mo5, Mo7

### 5.4 Other Material Considerations

National Planning Policy Framework	
National Planning Practice Guidance	
Supplementary Planning Guidance	Surrey Design Local Distinctiveness Design Guide A Parking Strategy for Surrey Parking Standards for Development Planning Obligations and Infrastructure SPD
Other	Human Rights Act 1998 Community Infrastructure Levy Regulations 2010

## 6.0 Assessment

6.1 The site is within the urban area in a residential neighbourhood, where in principle there would be no objection to residential development. However this must be tempered with how effectively the proposed development reflects the character of its surroundings and is sensitive to local amenities.

6.2 The main issues to consider are:

- Design appraisal
- Neighbour amenity
- Access and parking
- Impact on trees
- CIL
- Affordable Housing

### Design

- 6.3 The scale, massing and external traditional appearance of the proposed dwellings, whilst on smaller plots, are comparable with the existing predominantly two storey semi-detached properties fronting Shelveys Way. They are also in keeping with the approved dwellings on the sites immediately to the east to which the present proposal forms a “sequel”.
- 6.4 The scheme constitutes backland development which was atypical of the vicinity until that approved for the sites to the east (rear of 1-7 & 9-17 Shelveys Way) also in the applicant’s ownership, planning permission for which has now been implemented. This development can also be viewed as putting into effect the NPPF’s advice that developments should be sympathetic to local character and establish a strong sense of place. Policy Ho14 of the Reigate and Banstead Borough Local Plan 2005 sets out criteria for acceptable backland development, for example that “...*the general pattern and form of the development in the area is maintained...*” and that “...*the proposed plot styles and spacings between buildings reflects the predominating in the surrounding area...*”. The Amplification to this Policy states inter alia that “...*proposal will only be acceptable to the Borough Council where existing back gardens are excessively long and the back gardens created for the new dwellings and those left for the existing properties would still conform to that prevalent in the general area...*”. The Council’s Local Distinctiveness Design Guide recommends that, as regards 1930s-1950s Suburbia, “...*New infill development should not provide parking or garaging which dominates the street frontage...*” and “...*Building form and massing should reflect dwellings within the vicinity...*”. It is considered that the present scheme presents an identifiable sense of frontage with the “public” sides of the dwellings oriented to the new street and constituting a spacious layout with any car parking domination avoided and giving the benefit of acceptable planting and landscaping.. This is a scheme that, taking into account the features itemised above, adheres to the above advice and respects local distinctiveness.
- 6.5 The first phase of the site, already built, is on the edge of the urban area beyond which, to the east of the approved backland development, there is the Metropolitan Green Belt (MGB). The proposal comprises a built complex on a further phase away from the margin of the built up area but shares the access. The increased use of the access would not result in any material harm to the setting of the MGB which is as per the previously approved scheme.
- 6.6 The Council’s Tree Officer’s views are as follows.

*"I am familiar with the application site and the neighbouring woodland and the information provided by Bourne Landscapes is an accurate reflection of the quality of the trees within site and in the woodland. The trees shown to be removed are low quality specimen while the remaining trees outside the site will be retained ensuring there is still a strong network of established tree and vegetation. To compensate for the removal of the trees to facilitate this development it is necessary for a detailed landscape scheme which can be secured by condition.*

*Prior to this application and the approved schemes being submitted an area TPO was served on the trees in 1-31 Shelveys Way; this was modified upon confirmation to only include individual trees of good quality. The layout for this scheme has been designed around the protected tree T38 and does not involve any excavation within its RPA; furthermore the buildings are located far enough away from T38 to allow it to mature and continue to enhance the site for the foreseeable future without the need to undertake pruning works. The rear gardens are not dominated by the canopies from the trees within the woodland but it will be necessary to manage the trees from time to time. Whilst it is not possible to predict the frequency and nature of future works the fact that they are protected means that consent is required from the council before undertaking any works..."*

Therefore based on the proposed layout and arboricultural information the Tree Officer supports the application subject to conditions and informatives relating to landscaping scheme, tree protection measures and their supervision, use of arboricultural consultant and tree specifications. These are similar to those attached to the permissions for the approved residential layouts immediately to the east and are considered to be reasonable and enforceable.

#### Neighbour amenity

- 6.7 As has been noted, the traditional pattern of existing development has been typified by long rear gardens. Given this feature and the separation distances therefore between existing properties and the proposal which are comparable to the development which has been built to the rear of 1-7 Shelveys Way and approved to the rear of 9-17 Shelveys Way. The impact from the development would therefore be comparable and not harmful to neighbouring residential amenities as regards overlooking, overshadowing or overbearing effect. Disturbance as a result of construction would be a temporary aberration which could be managed by a condition requiring a Construction Transport Management Plan. As regards living conditions for future occupiers, the layout and positioning of habitable room windows (to the north and south elevations) are such that no potential for overlooking, overshadowing or overbearing effect would arise.

#### Access and parking

- 6.8 Access to the site is via the drive serving the two tranches of approved backland development to the east. This drive debouches at a point close to the junction of Shelvers Way with the A217. The County Highway Authority's (CHA) comments have been sought and have been received, as below.
- 6.9 The Highway Authority "...acknowledges that concerns and objections have been raised by local residents in relation to the proposed extended development, including impacts upon the highway. It is noted that the access from Shelvers Way was part of the initial application approved for this site, with a visibility of 2.4m by 27.5m visibility splay to the east in a reflection of for the speed of traffic travelling westbound from the A217 along Shelvers Way. It is noted that this visibility was acceptable in context of the original six dwellings approved. With this application, the overall number of dwellings would be increased again beyond the original six dwellings. The CHA have noted the gradual increase of dwellings being served from this single restricted access point, which was appropriate to support the original six dwellings. The CHA flag a general note that improvements to the visibility and entry layout for this junction may be required should the incremental increase in the number of dwellings continue to increase, to respond on the cumulative impacts of this access junction operation and that of the A217. It is also noted that the pedestrian inter-visibility splays, connected to the access, should be kept permanently clear of any obstruction above 0.6m high, and had been requested to be included on any future drawings, this has not been noted in this application submission."
- 6.10 It is clear that the Highway Authority is satisfied that the access from Shelvers Way is satisfactory to serve the cumulative traffic generated by a total of 16 dwellings (the current proposal plus the two approved schemes to the east of that). These views are endorsed: there is adequate parking within the layout to assuage the (admittedly unlikely) demand for any on-street parking in Shelvers Way. As regards the Highway Authority's concern about traffic implications for any further adding to the residential layout served from Shelvers Way, The applicant has informally indicated that he does not intend to make any further applications for this.

### CIL

- 6.11 The Community Infrastructure Levy (CIL) is a fixed charge which the Council will be collecting from some new developments from 1 April 2016. It will raise money to help pay for a wide range of infrastructure including schools, roads, public transport and community facilities which are needed to support new development. This development would be CIL liable although the exact amount would be determined and collected after the grant of planning permission. Affordable Housing

### Affordable Housing

- 6.12 Core Strategy Policy CS15 and the Council's Affordable Housing SPD require financial contributions towards affordable housing to be provided on housing developments of 1-9 units. However, in November 2014, the Government



introduced policy changes through a Written Ministerial Statement and changes to the national Planning Practice Guidance which restrict the use of planning obligations to secure affordable housing contributions from developments of 10 units or less. These changes were given legal effect following the Court of Appeal judgement in May 2016.

- 6.13 In view of this, and subsequent local appeal decisions which have afforded greater weight to the Written Ministerial Statement than the Council's adopted policy, the Council is not presently requiring financial contributions from applications such as this resulting in a net gain of 10 units or less. The absence of an agreed undertaking does not therefore warrant a reason for refusal in this case.

#### Other matters

- 6.14 The Council's Neighbourhood Services seek a location on the site for presentation of refuse bins. It is not possible under planning legislation to require this nor indeed is it a planning consideration: however an informative would be attached regarding provision of wheeled bins to each household.

#### **CONDITIONS**

Note: It is intended to impose pre-commencement conditions, relating in this case to a construction transport management plan and tree protection measures. Under the provisions of the Town and Country Planning (Pre-commencement) Regulations 2018, the Council is obliged to seek the applicant's agreement to these conditions in writing: the applicant has done so.

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.  
Reason:  
To comply with Section 91(1) of the Town and Country Planning Act 1990 as amended by Section 51 (1) of the Planning and Compulsory Purchase Act 2004.
2. The development hereby permitted shall be carried out in accordance with the following approved plans:

<b>Plan Type</b>	<b>Reference</b>	<b>Version</b>	<b>Date Received</b>
Site Location Plan	PL01	A	24.05.2018
Prop Site Layout (PSL)	PL02	A	24.05.2018
PSL (coloured)	PL03	A	24.05.2018
Block Plan	PL04	A	24.05.2018
Plots 1/2 Floor Plans & Elevations	PL05	A	24.05.2018
Plot 3 Floor Plans & Elevations	PL06	A	24.05.2018
Plot 4 Floor Plans & Elevations	PL07	A	24.05.2018

Plots 5/6 Floor Plans/EIs	PL08	A	24.05.2018
Exg/Prop street sections	PL09	A	24.05.2018
Proposed site sections	PL10	A	24.05.2018
Street scene (coloured)	PL11	A	24.05.2018
Vehicle tracking	PL12	B	20.09.2018
Location plan	PL13	A	24.05.2018
Site layout (coloured)	PL14	A	15.08.2018
Site layout	PL15	A	15.08.2018
Tree protection plan	BLC180118		24.05.2018
Tree protection plan	BLC180019		24.05.2018

Reason:

To define the permission and ensure the development is carried out in accord with the approved plans and in accordance with National Planning Practice Guidance.

3. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015, (or any Order revoking and re-enacting that Order with or without modification), no first floor windows, dormer windows or rooflights other than those expressly authorised by this permission shall be constructed.

Reason:

To ensure that the development does not affect the amenity of the neighbouring property by overlooking and to protect the visual amenities of the area in accordance with Reigate and Banstead Borough Local Plan 2005 policy Ho9.

4. Prior to the construction reaching slab level, details of materials to be used in the construction of the external surfaces, including fenestration and roof, must be submitted to and approved in writing by the Local Planning Authority. Development shall be carried out in accordance with the approved details.

Reason:

To ensure that a satisfactory external appearance is achieved with regard to Reigate and Banstead Borough Local Plan 2005 policies Ho9 and Ho13.

5. The first floor windows in the west facing (side) elevation of Plot 4 of the development hereby permitted shall be glazed with obscured glass which shall be fixed shut, apart from a top hung opening fanlight whose cill height shall not be less than 1.7 metres above internal floor level, and shall be maintained as such at all times.

Reason:

To ensure that the development does not affect the amenity of the neighbouring property by overlooking with regard to Reigate and Banstead Borough Local Plan 2005 policy Ho9.

6. The development hereby approved shall not be first occupied unless and until space has been laid out within the site in accordance with the approved plans for vehicles to be parked and for vehicles to turn so that they may enter and

leave the site in forward gear. Thereafter the parking / turning areas shall be retained and maintained for their designated purposes.

Reason:

in order that the development should not prejudice highway safety nor cause inconvenience to other highway users to meet the objectives of the National Planning Policy Framework and having regard to policies Mo5 and Mo7 of the Reigate and Banstead Borough Local Plan 2005.

7. No development shall commence until a Construction Transport Management Plan, to include details of:

(a) parking for vehicles of site personnel, operatives and visitors  
(b) loading and unloading of plant and materials  
(c) storage of plant and materials  
(d) programme of works (including measures for traffic management)  
(e) provision of boundary hoarding behind any visibility zones  
(f) HGV deliveries and hours of operation  
(g) measures to prevent the deposit of materials on the highway  
(h) before and after construction condition surveys of the highway and a commitment to fund the repair of any damage caused  
(i) on-site turning for construction vehicles

has been submitted to and approved in writing by the Local Planning Authority. Only the approved details shall be implemented during the construction of the development.

Reason:

in order that the development should not prejudice highway safety nor cause inconvenience to other highway users to meet the objectives of the National Planning Policy Framework and having regard to policies Mo5 and Mo7 of the Reigate and Banstead Borough Local Plan 2005.

8. Prior to construction reaching slab level, a scheme for the landscaping of the site including the retention of existing landscape features has been submitted to and approved in writing by the LPA. Landscaping schemes shall include details of hard and soft landscaping, including any tree removal/retention, planting plans, written specifications (including cultivation and other operations associated with tree, shrub, and hedge or grass establishment), schedules of plants, noting species, plant sizes and proposed numbers/densities and an implementation and management programme.

All hard and soft landscaping work shall be completed in full accordance with the approved scheme, prior to occupation or within the first planting season following completion of the development hereby approved or in accordance with a programme agreed in writing with the local planning authority.

Any trees shrubs or plants planted in accordance with this condition which are removed, die or become damaged or become diseased within five years of planting shall be replaced within the next planting season by trees, shrubs of the same size and species.

Reason:

To ensure good arboricultural and landscape practice in the interests of the maintenance of the character and appearance of the area and to comply with policies Pc4, Ho9 of the Reigate and Banstead Borough Local Plan 2005.

9. No development shall commence including groundworks, preparation and demolition until all related arboricultural matters, including arboricultural supervision, monitoring and tree protection measures are implemented in strict accordance with the approved details contained in the Arboricultural Impact Assessment, Tree Survey, Tree Protection Plan and Arboricultural Method Statement compiled by Bourne Landscapes dated 22<sup>nd</sup> May 2018.

Reason:

To ensure good arboricultural practice in the interests of the maintenance of the character and appearance of the area and to comply with policy Pc4 of the Reigate and Banstead Borough Local Plan 2005 and the recommendations within British Standard 5837.

10. No development, groundworks or demolition processes shall be undertaken until an agreed scheme of supervision for the arboricultural protection measures have been submitted to and approved in writing by the local planning authority. The pre commencement meeting, supervision and monitoring shall be undertaken in accordance with these approved details. The submitted details shall include:

Pre commencement meeting between the retained arboricultural consultant, local planning authority, Tree Officer and individuals and personnel responsible for the implementation of the approved development;  
Timings, frequency of the supervision and monitoring regime and an agreed reporting process to the local planning authority;  
The supervision monitoring and reporting process shall be undertaken by a qualified arboriculturist.

Reason:

To ensure good arboricultural practice in the interests of the maintenance of the character and appearance of the area and to comply with British Standard 5837:2012 'Trees in Relation to Design, demolition and Construction – Recommendations' and policies Pc4 and Ho9 of the Reigate and Banstead Borough Local Plan.

11. The development shall not be occupied until a plan indicating the positions, design, materials and type of boundary treatment to be erected has been submitted to and approved in writing by the Local Planning Authority. The boundary treatment shall be completed before the occupation of the development hereby permitted.

Reason:

To preserve the visual amenity of the area and protect neighbouring residential amenities with regard to the Reigate and Banstead Borough Local Plan 2005 policies Pc4 and Ho9.

## INFORMATIVES

1. Your attention is drawn to the safety benefits of installing sprinkler systems as an integral part of new development. Further information is available at [www.firesprinklers.info](http://www.firesprinklers.info).
2. The applicant is encouraged to provide renewable technology within the development hereby permitted in order to reduce greenhouse gas emissions.
3. The applicant is advised that prior to the initial occupation of any individual dwelling hereby permitted, a 140 litre wheeled bin conforming to British Standard BSEN840 and a 60 litre recycling box should be provided for the exclusive use of the occupants of that dwelling. Prior to the initial occupation of any communal dwellings or flats, wheeled refuse bins conforming to British Standard BSEN840, separate recycling bins for paper/card and mixed cans, and storage facilities for the bins should be installed by the developer prior to the initial occupation of any dwelling hereby permitted. Further details on the required number and specification of wheeled bins and recycling boxes is available from the Council's Neighbourhood Services on 01737 276501 or 01737 276097, or on the Council's website at [www.reigate-banstead.gov.uk](http://www.reigate-banstead.gov.uk). Bins and boxes meeting the specification may be purchased from any appropriate source, including the Council's Neighbourhood Services Unit on 01737 276775.
4. You are advised that the Council will expect the following measures to be taken during any building operations to control noise, pollution and parking:
  - (a) Work that is audible beyond the site boundary should only be carried out between 08:00hrs to 18:00hrs Monday to Friday, 08:00hrs to 13:00hrs Saturday and not at all on Sundays or any Public and/or Bank Holidays;
  - (b) The quietest available items of plant and machinery should be used on site. Where permanently sited equipment such as generators are necessary, they should be enclosed to reduce noise levels;
  - (c) Deliveries should only be received within the hours detailed in (a) above;
  - (d) Adequate steps should be taken to prevent dust-causing nuisance beyond the site boundary. Such uses include the use of hoses to damp down stockpiles of materials, which are likely to generate airborne dust, to damp down during stone/slab cutting; and the use of bowsers and wheel washes;
  - (e) There should be no burning on site;
  - (f) Only minimal security lighting should be used outside the hours stated above; and
  - (g) Building materials and machinery should not be stored on the highway and contractors' vehicles should be parked with care so as not to cause an obstruction or block visibility on the highway.

Further details of these noise and pollution measures can be obtained from the Council's Environmental Health Services Unit.

In order to meet these requirements and to promote good neighbourliness, the Council recommends that this site is registered with the Considerate Constructors Scheme - [www.ccscheme.org.uk/index.php/site-registration](http://www.ccscheme.org.uk/index.php/site-registration).

5. The applicant is advised that the essential requirements for an acceptable communication plan forming part of a Method of Construction Statement are viewed as: (i) how those likely to be affected by the site's activities are identified and how they will be informed about the project, site activities and programme; (ii) how neighbours will be notified prior to any noisy/disruptive work or of any significant changes to site activity that may affect them; (iii) the arrangements that will be in place to ensure a reasonable telephone response during working hours; (iv) the name and contact details of the site manager who will be able to deal with complaints; and (v) how those who are interested in or affected will be routinely advised regarding the progress of the work. Registration and operation of the site to the standards set by the Considerate Constructors Scheme (<http://www.ccscheme.org.uk/>) would help fulfil these requirements.
6. The permission hereby granted shall not be construed as authority to carry out any works on the highway. The applicant is advised that prior approval must be obtained from the Highway Authority Local Highways Service Group (0300 200 1003) before any works are carried out on any footway, footpath, carriageway, or verge to form a vehicle crossover or to install dropped kerbs. Please see: [www.surreycc.gov.uk/roads-and-transport/road-permits-and-licences/vehicle-crossovers-or-dropped-kerbs](http://www.surreycc.gov.uk/roads-and-transport/road-permits-and-licences/vehicle-crossovers-or-dropped-kerbs).
7. A pedestrian inter-visibility splay of 2m by 2m shall be provided on each side of the proposed access, the depth measured from the back of the footway and the widths outwards from the edges of the access. No fence, wall or other obstruction to visibility between 0.6m and 2m in height above ground level shall be erected within the area of such splays.
8. The developer is reminded that it is an offence to allow materials to be carried from the site and deposited on or damage the highway from uncleaned wheels or badly loaded vehicles. The Highway Authority will seek, wherever possible, to recover any expenses incurred in clearing, cleaning or repairing highway surfaces and prosecutes persistent offenders. (Highways Act 1980 Sections 131, 148, 149).
9. The developer is advised that as part of the detailed design of the highway works required by the above conditions, the County Highway Authority may require necessary accommodation works to street lights, road signs, road markings, highway drainage, surface covers, street trees, highway verges, highway surfaces, surface edge restraints and any other street furniture/equipment.
10. The use of a suitably qualified arboricultural consultant is essential to provide acceptable supervision and monitoring in respect of the arboricultural issues

in respect of the above condition. All works shall comply with the recommendations and guidelines contained within British Standard 5837.

11. The planting of trees and shrubs shall be in keeping with the character and appearance of the locality. There is an opportunity to incorporate substantial sized trees into the scheme to provide for future amenity and long term continued structural tree cover in this area. It is expected that the replacement structural landscape trees will be of Extra Heavy Standard size with initial planting heights of not less than 4m, with girth measurements at 1m above ground level in excess of 14/16cm.
12. The site is on or near land forming part of Banstead Commons which are protected by an Act of Parliament and Commons' Byelaws. The applicant is requested to liaise with the Clerk to the Conservators in order to protect the commons against unlawful acts including driving, parking, excavating or storage of materials on the commons. The Clerk may be contacted by writing to PO Box 528, Dorking, Surrey RH4 1NY.

### **REASON FOR PERMISSION**

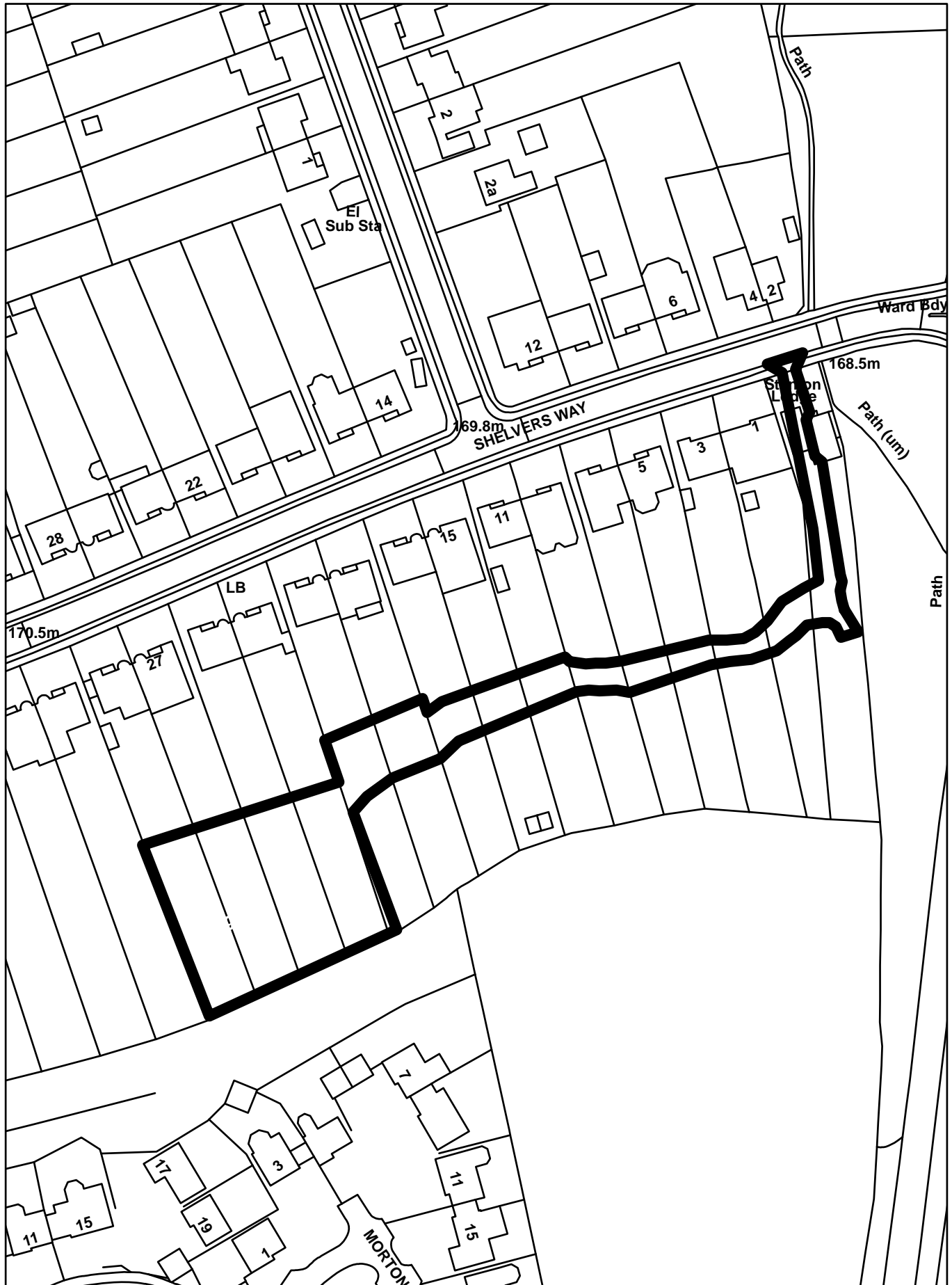
The development hereby permitted has been assessed against development plan policies CS1, CS2, CS4, CS10, CS11, CS12, CS14, CS15, Pc4, Ho9, Ho9A, Ho13, Ho14, Ho16, Mo5 and material considerations, including third party representations. It has been concluded that the development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.

### **Proactive and Positive Statement**

The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.



# 18/01134/F - Land To The Rear Of 19-29 Shelveys Way, Tadworth







0 2 4 6 8 10

Previous applications approved under ref: 17/00016/573 & 17/02097/F

ACCOMMODATION SCHEDULE	
Application Site Area = 3883 sq.m / 0.39 hectares / 0.96 acres	
PLOT / TYPE	QUANTITY
Plots 1, 2, 4, 5 & 6 - 3 Bedroom Houses (91.5 sq.m / 985 sq.ft)	5
Plot 3 - 3 Bedroom House (110.2 sq.m / 1186 sq.ft)	1
	TOTAL
Parking	12 spaces

scale: 1:200 @ A1  
 date: May 2018  
 drawn: A/JC  
 checked: A/JC  
 client: DEVINE HOMES PLC  
 project: Land at 19 to 29 Shelvers Way, Tadworth, KT20 5QJ.  
 drawing: Proposed Site Layout (Coloured)  
 status: PLANNING

rev.	description	date	by	ch
A	PLANNING ISSUE	2018.05.16	A/JC	A/C
P1	PRELIMINARY ISSUE	2018.05.14	A/JC	A/C

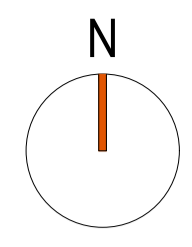
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 MODEL FILE REFERENCE - 18007-A-P-PL





108

0 2 4 6 8 10



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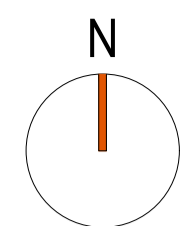
PLANNING ISSUE	2018.06.07	AJC	AJC
rev.	date	by	ch
A			

scale:	1:250 @ A1	client:	DEVINE HOMES PLC
date:	June 2018	project:	Land at 19 to 29 Shelvers Way, Tadworth, KT20 5QL.
drawn:	AJC	drawing:	Proposed Site Layout-Wider Context (Coloured)
checked:	AJC	status:	PLANNING





0 2 4 6 10



Previous applications approved under ref:  
17/00016/S73  
& 17/02097/F

scale:	1:250 @ A1	client:	DEVINE HOMES PLC
date:	June 2018	project:	Land at 19 to 29 Shelvers Way, Tadworth, KT20 5QL
drawn:	AJC	drawing:	Proposed Site Layout-Wider Context (Coloured)
checked:	AJC	status:	PLANNING

PLANNING ISSUE description	rev.	A	2018.06.21	AJC	by	AJC	ch
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MODEL FILE REFERENCE - 18007-A-P-PL





110

client:	DEVINE HOMES PLC
project:	Land at 19 to 29 Shelvers Way, Tadworth, KT20 5QJ.
drawing:	Existing & Proposed Street Sections from Shelvers Way
status:	PLANNING
scale:	1:200 @ A1
date:	May 2018
drawn:	AJC
checked:	AJC

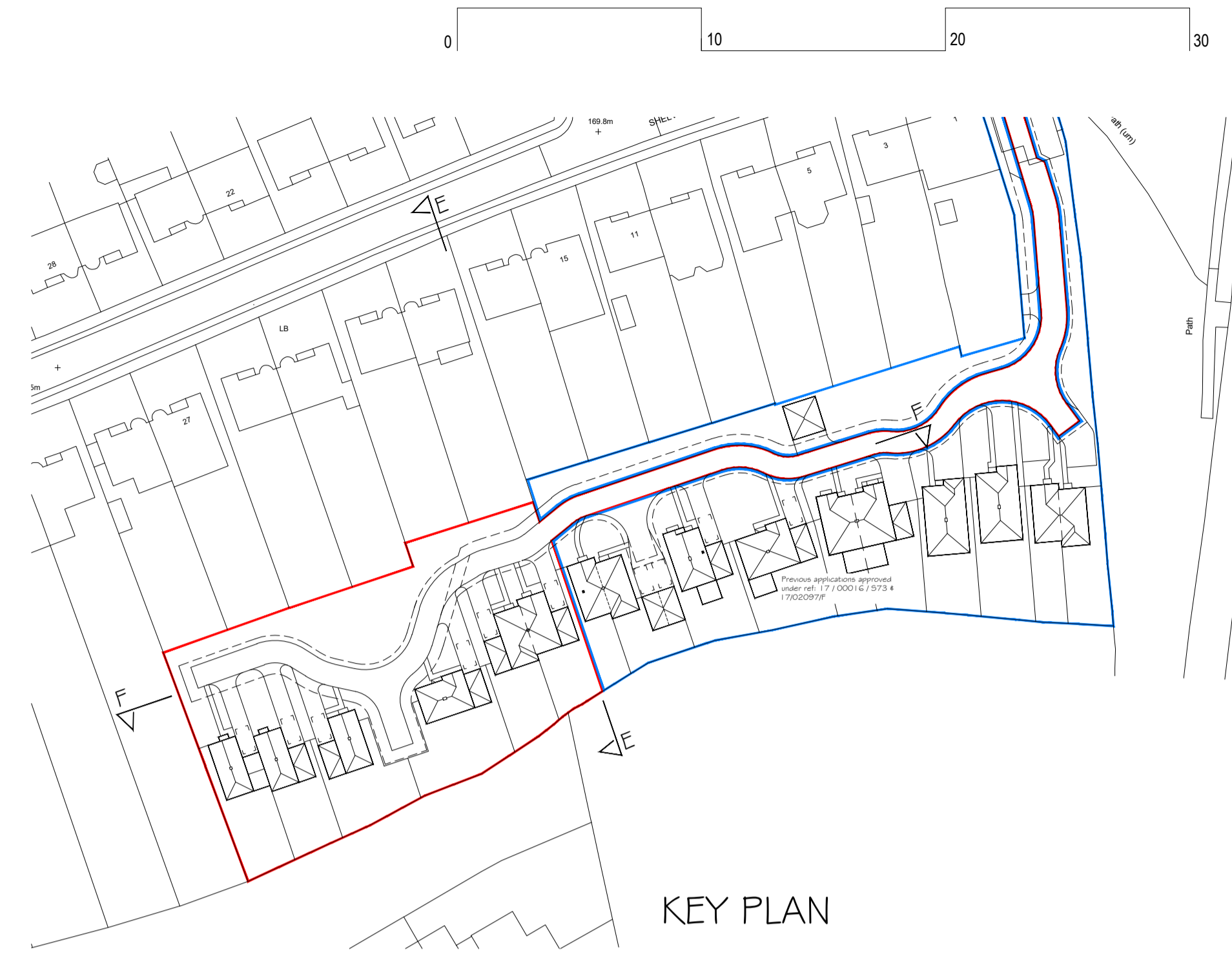
2018.05.16	AJC	AJC
2018.05.14	AJC	AJC
date	by	ch
A	PLANNING ISSUE.	
P1	PRELIMINARY ISSUE.	
rev.	description	

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client:	DEVINE HOMES PLC
project:	Land at 19 to 29 Stalvers Way, Tadworth, KT20 5QJ.
drawing:	Proposed Street Scene within site (coloured)
status:	PLANNING
scale:	1:200 @ A1
date:	May 2018
drawn:	AJC
checked:	AJC

2018.05.16	AJC	AJC
2018.05.14	AJC	AJC
date	by	ch
A	PLANNING ISSUE.	
P1	PRELIMINARY ISSUE.	
rev.	description	

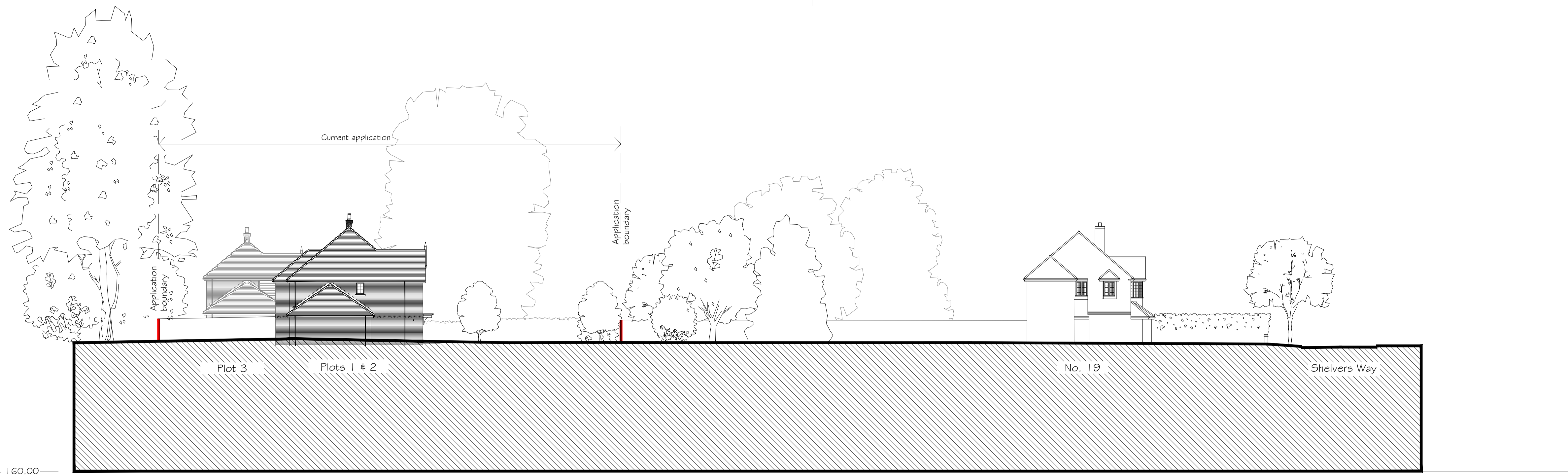
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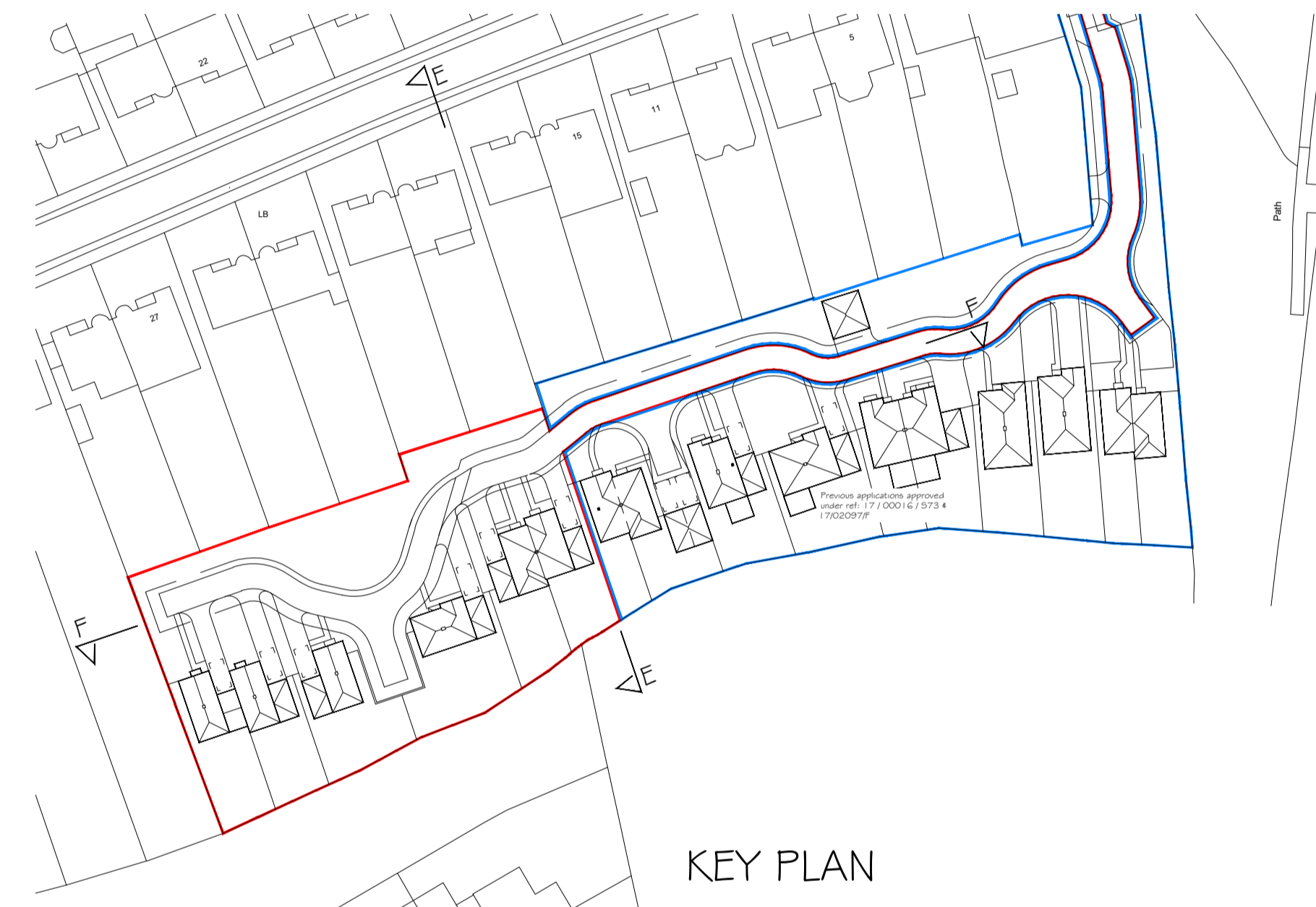
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SECTION EE



KEY PLAN



SECTION FF

112

client:	DEVINE HOMES PLC
project:	Land at 19 to 29 Shelvers Way, Tadworth, KT20 5QJ.
drawing:	Proposed Site Sections
status:	PLANNING
scale:	1:200 @ A1
date:	May 2018
drawn:	AJC
checked:	AJC

2018.05.16	AJC	AJC
2018.05.14	AJC	AJC
date	by	ch
A	PLANNING ISSUE.	
P1	PRELIMINARY ISSUE.	
rev.	description	

notes

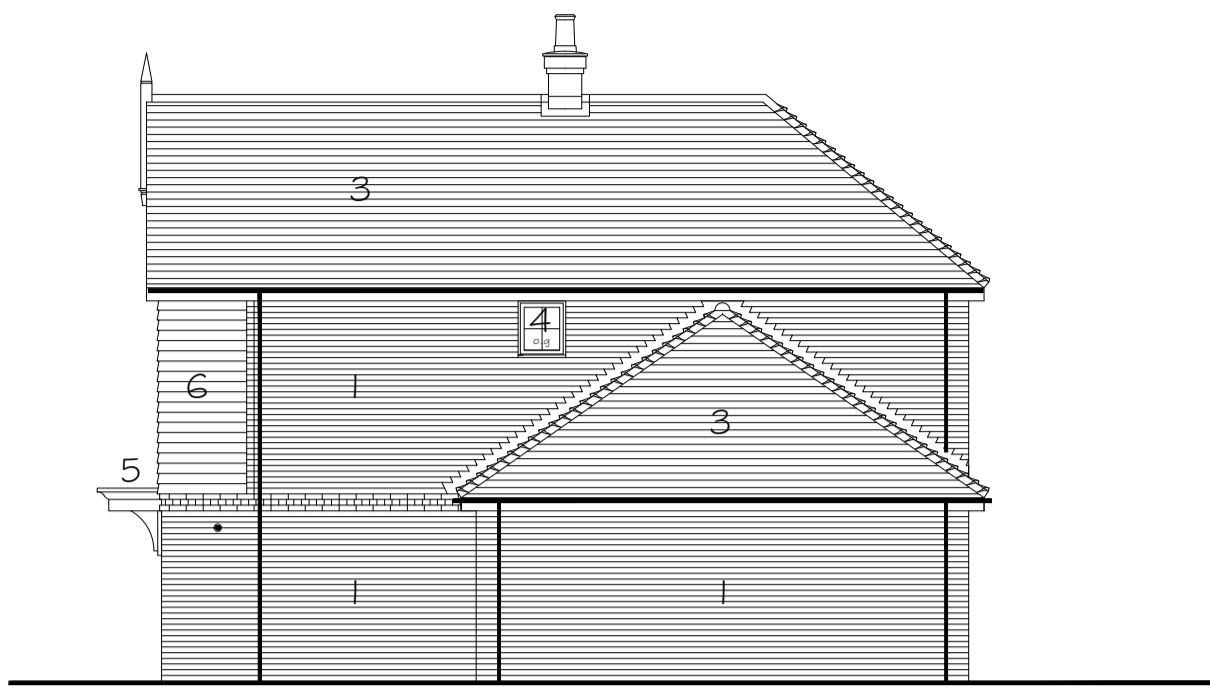
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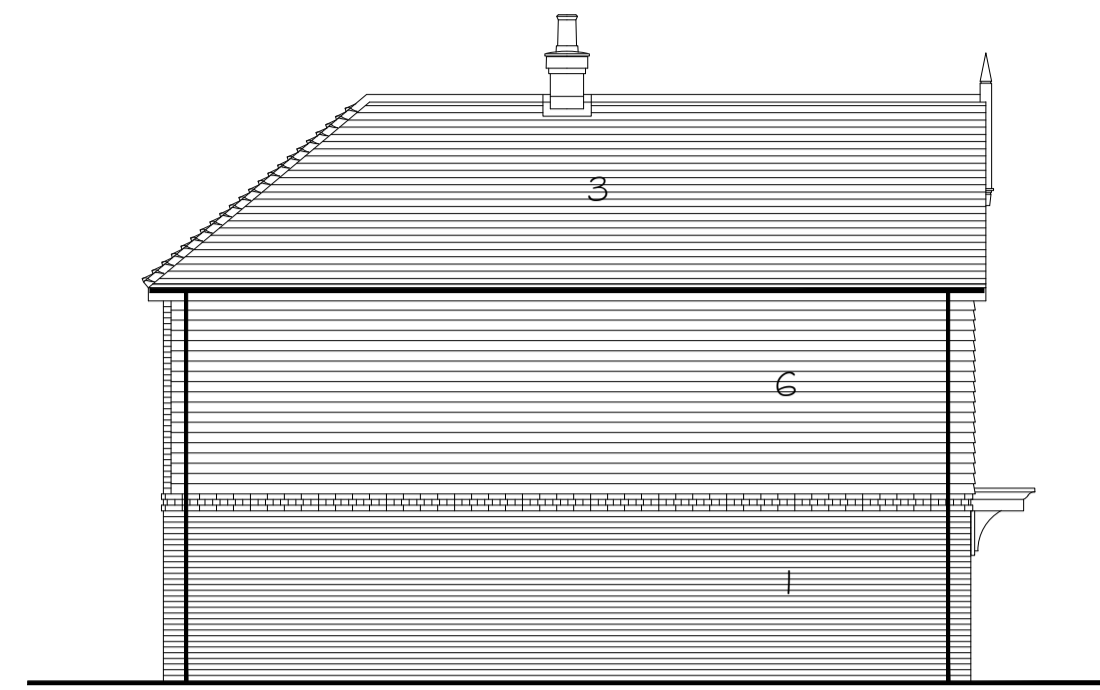
NORTH-WEST ELEVATION



NORTH-EAST ELEVATION



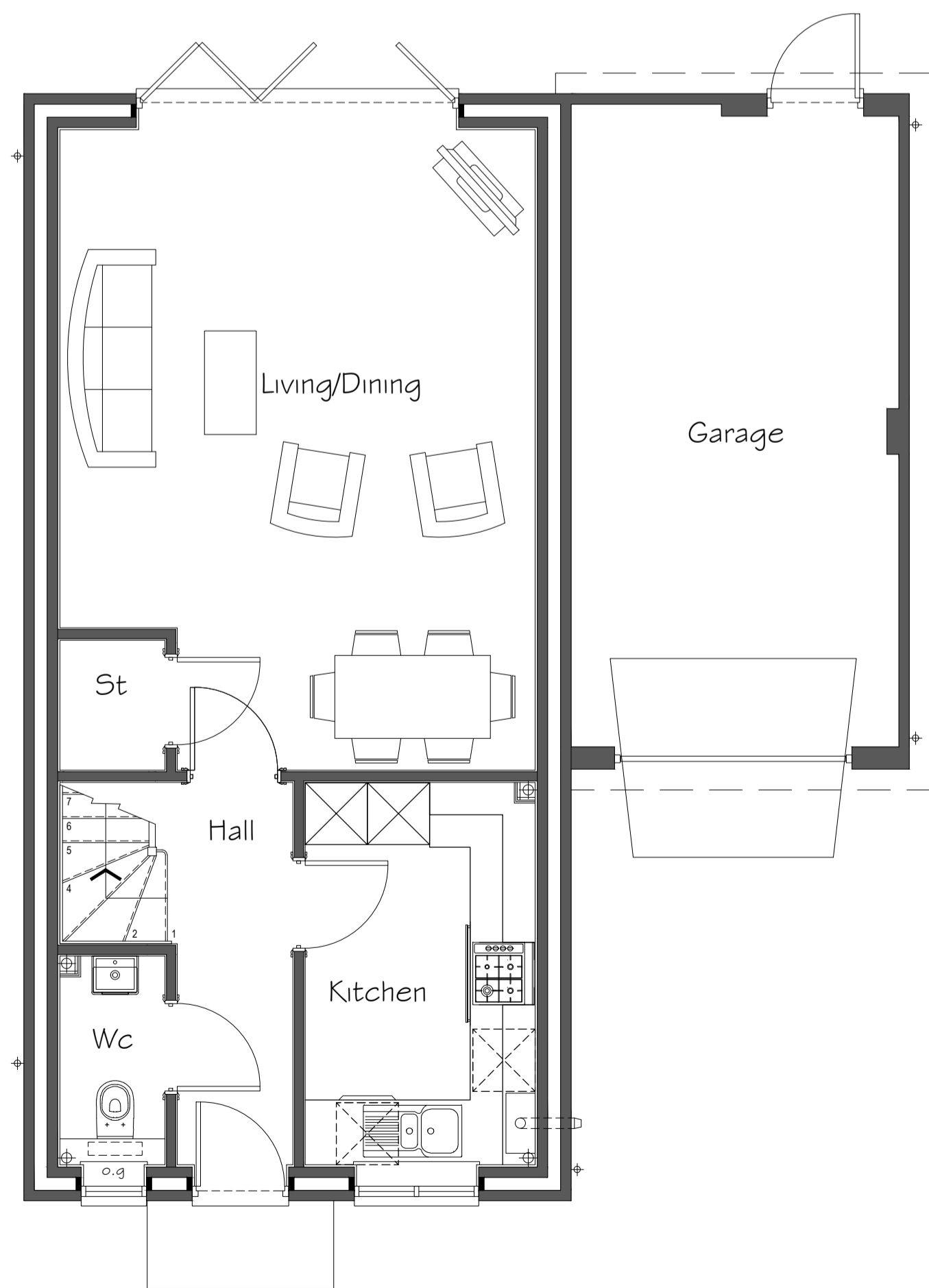
SOUTH-EAST ELEVATION



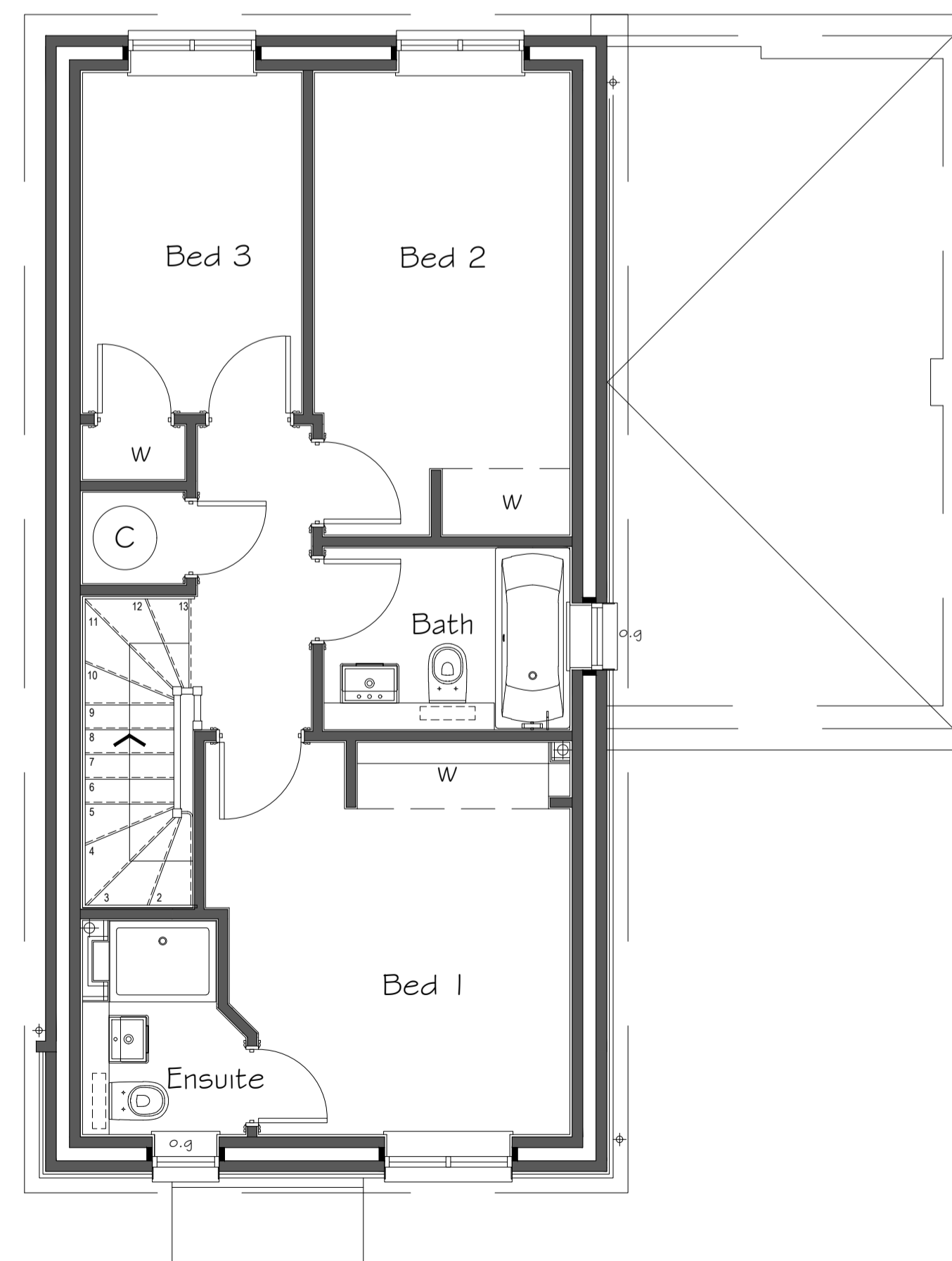
SOUTH-WEST ELEVATION



KEY TO MATERIALS	
1	Facing Brickwork
2	Render
3	Plain Tiles
4	uPVC Windows and Doors
5	Single Ply Membrane
6	Plain Tile Hanging
7	Timber Boarding



GROUND FLOOR PLAN



FIRST FLOOR PLAN

client: DEVINE HOMES PLC  
 project: Land at 19 to 29 Shelveys Way, Tadworth, KT20 5QJ.  
 drawing: Plot 4 - Proposed Floor Plans & Elevations  
 status: PLANNING  
 scale: 1:50 / 1:100 @ A1  
 date: May 2018  
 drawn: AJC  
 checked: AJC

rev.	description	date	by	ch
A	PLANNING ISSUE.	2018.05.16	AJC	AJC
P1	PRELIMINARY ISSUE.	2018.05.14	AJC	AJC

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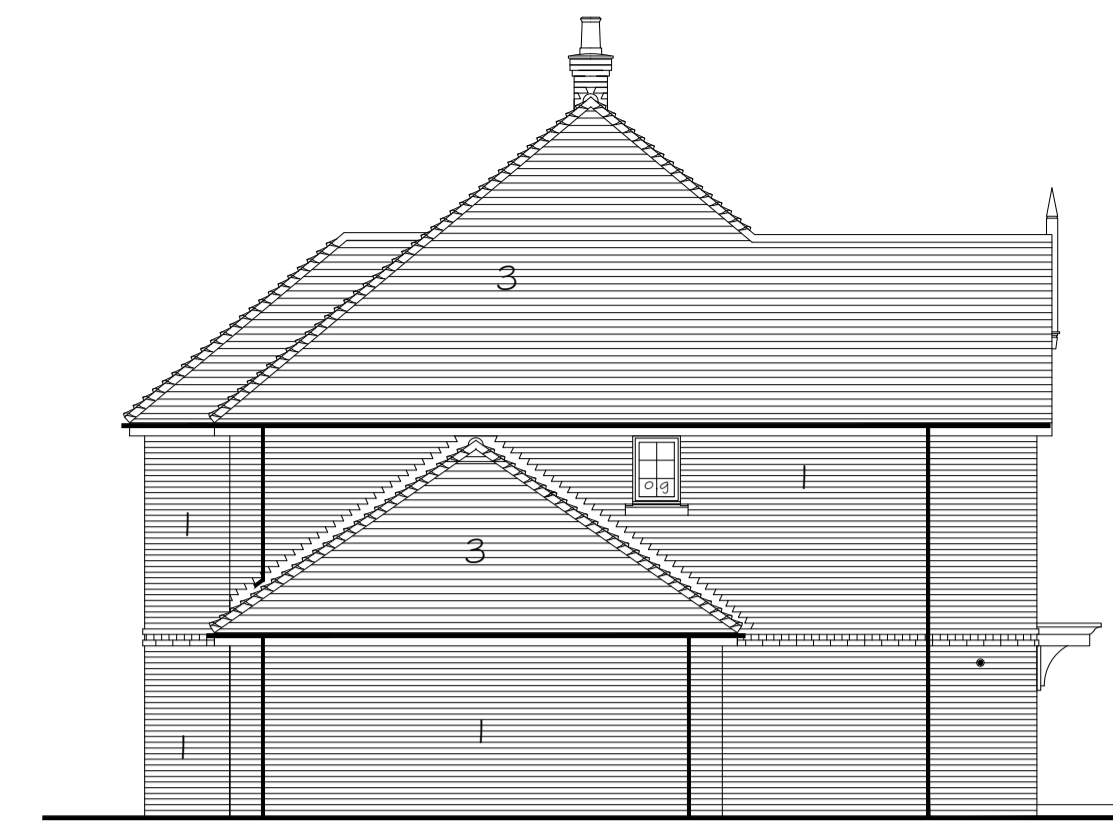
NORTH-WEST ELEVATION



SOUTH-WEST ELEVATION

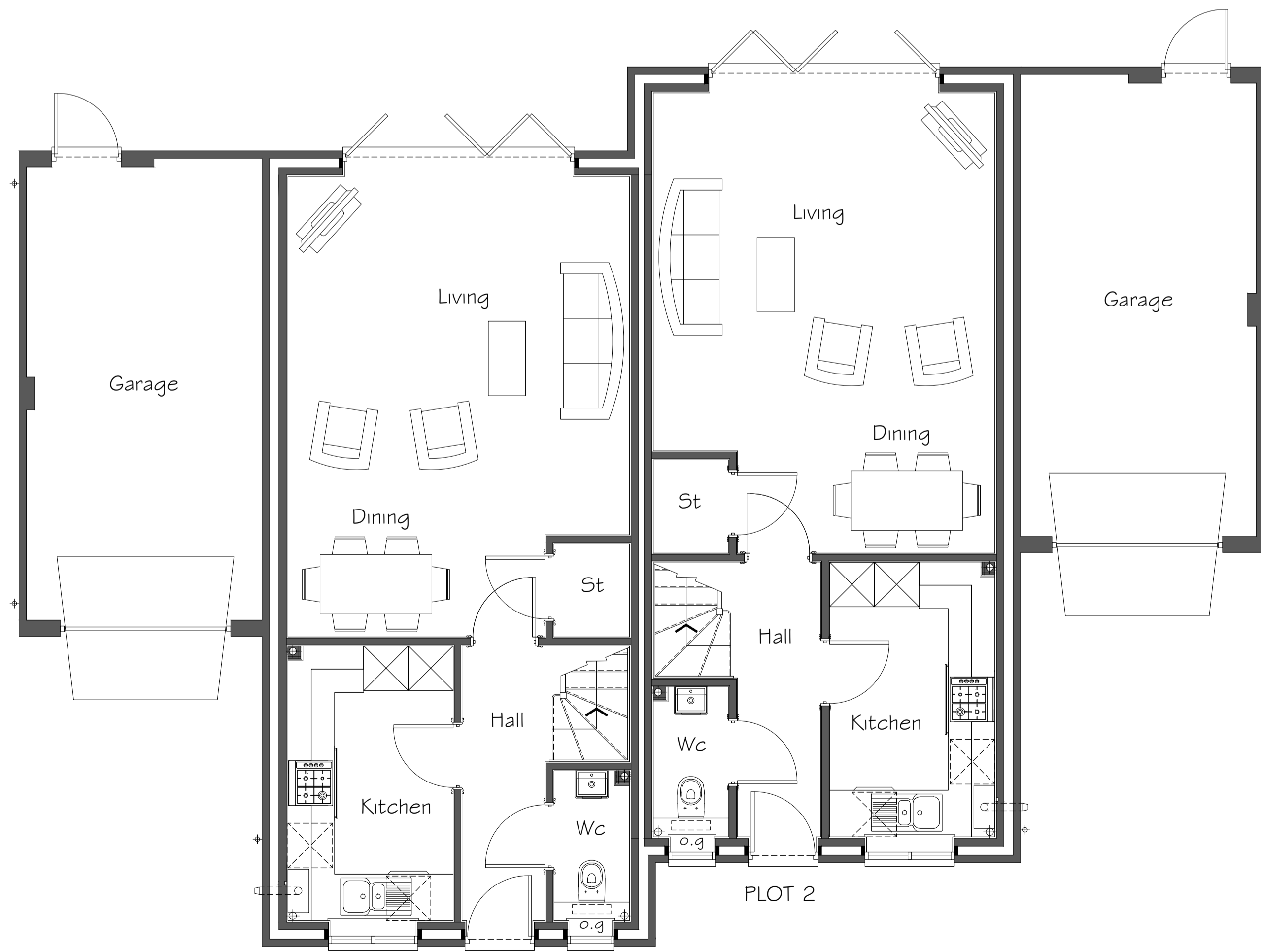
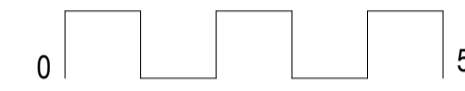


SOUTH-EAST ELEVATION

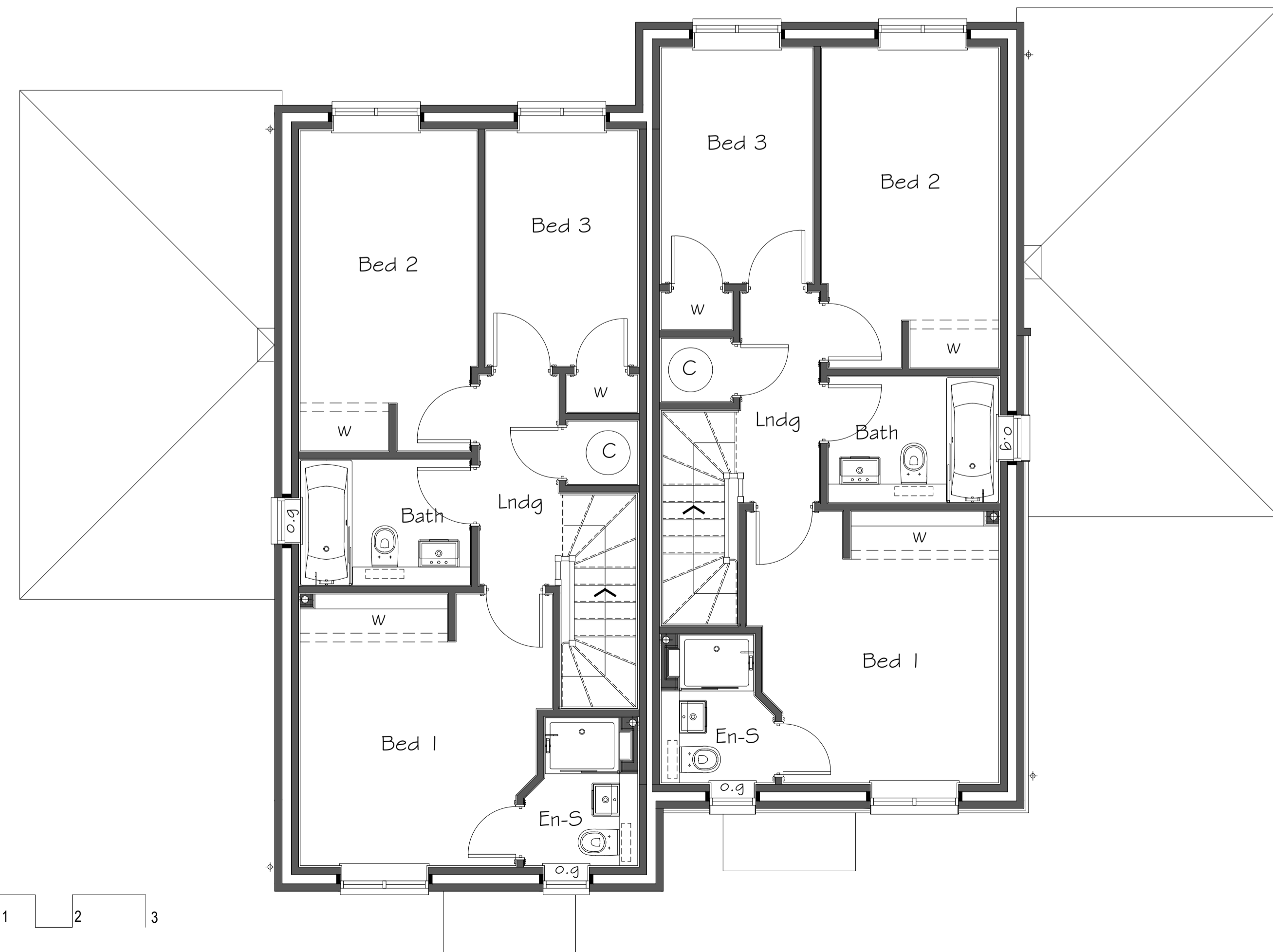


NORTH-EAST ELEVATION

KEY TO MATERIALS	
1	Facing Brickwork
2	Render
3	Plain Tiles
4	uPVC Windows and Doors
5	Single Ply Membrane
6	Plain Tile Hanging
7	Timber Boarding



GROUND FLOOR PLAN



FIRST FLOOR PLAN

wessex house upper market street eastleigh hampshire SO50 9FD  
t: 01794 830660 e: enquiries@vision-architects.org.uk

**VISION**  
architects  
drawing No: 18007-A-PL05  
revision: A

client: DEVINE HOMES PLC  
project: Land at 19 to 29 Shelveys Way, Tadworth, KT20 5QJ.  
drawing: Plots 1 and 2 - Proposed Floor Plans & Elevations  
status: PLANNING  
scale: 1:50 / 1:100 @ A1  
date: May 2018  
drawn: AJC  
checked: AJC

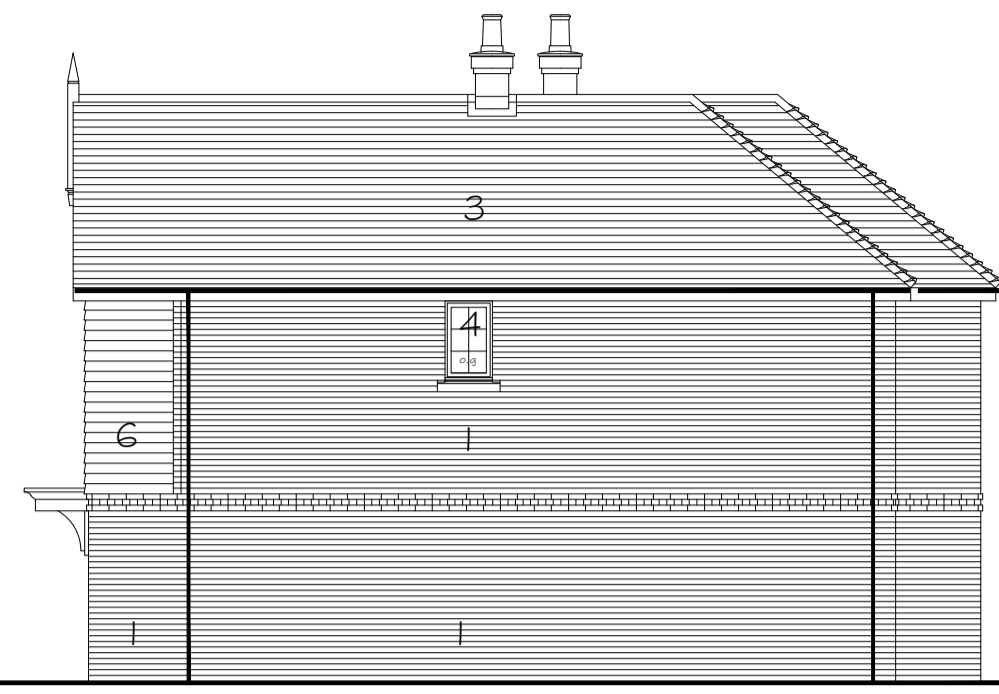
2018.05.16 AJC AJC  
2018.05.14 AJC AJC  
date by ch  
A PLANNING ISSUE.  
PT PRELIMINARY ISSUE.  
rev. description

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MODEL FILE REFERENCE: 18007-A-P-PL





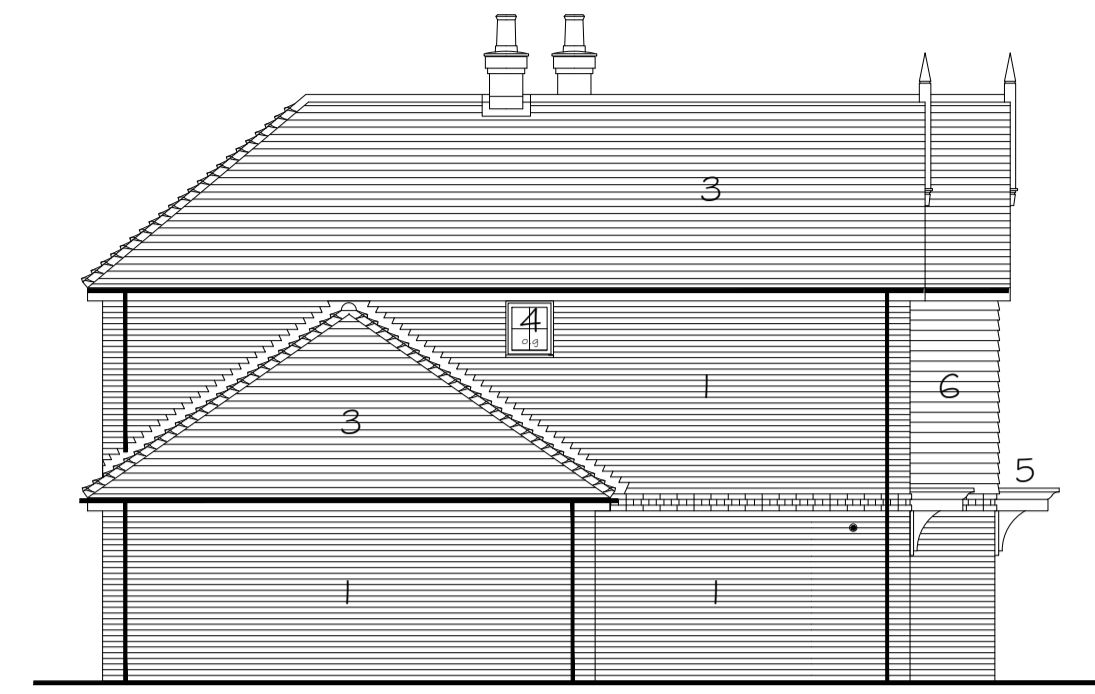
NORTH-WEST ELEVATION



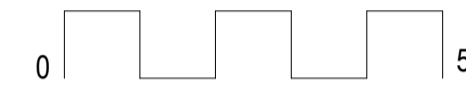
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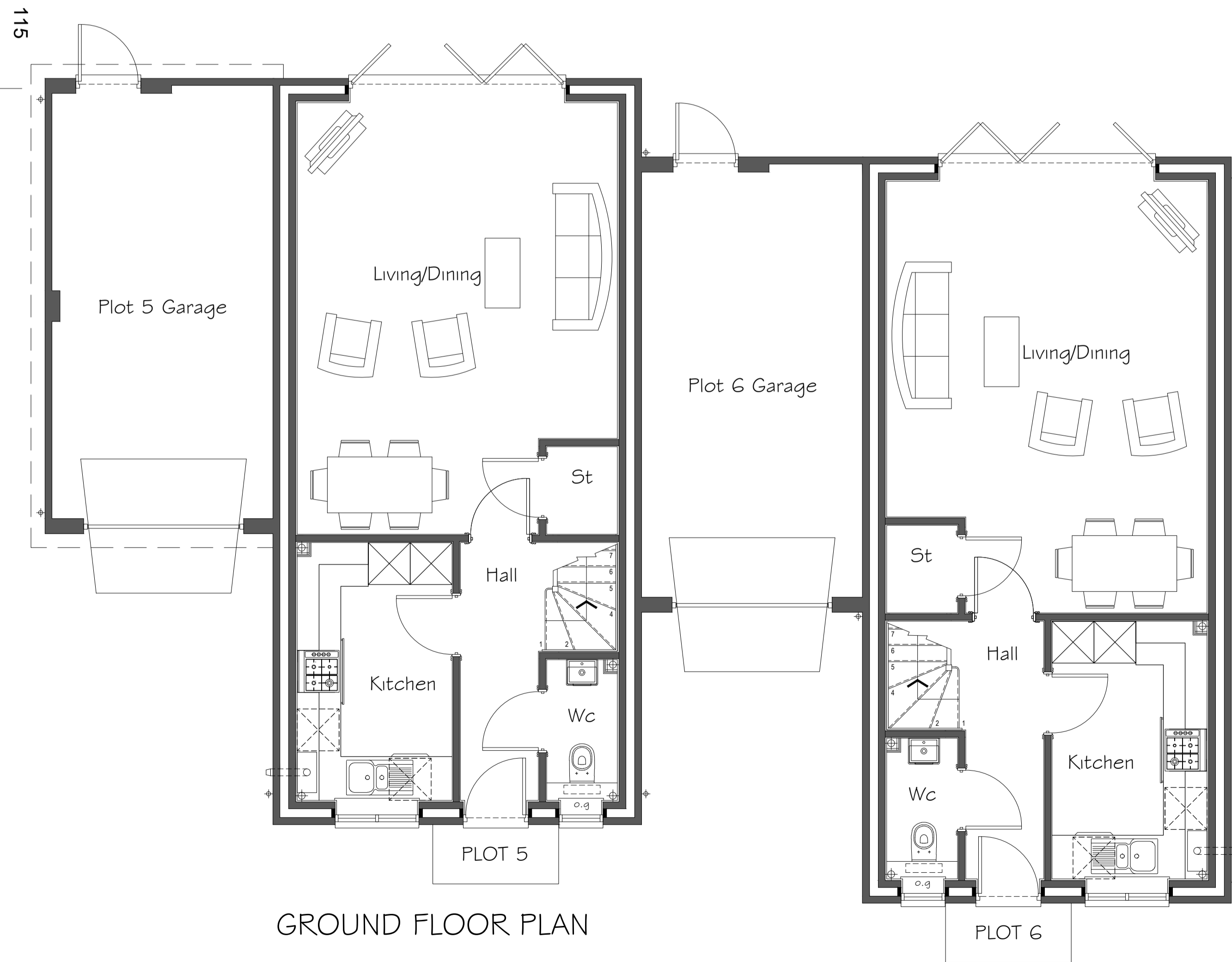
SOUTH-EAST ELEVATION



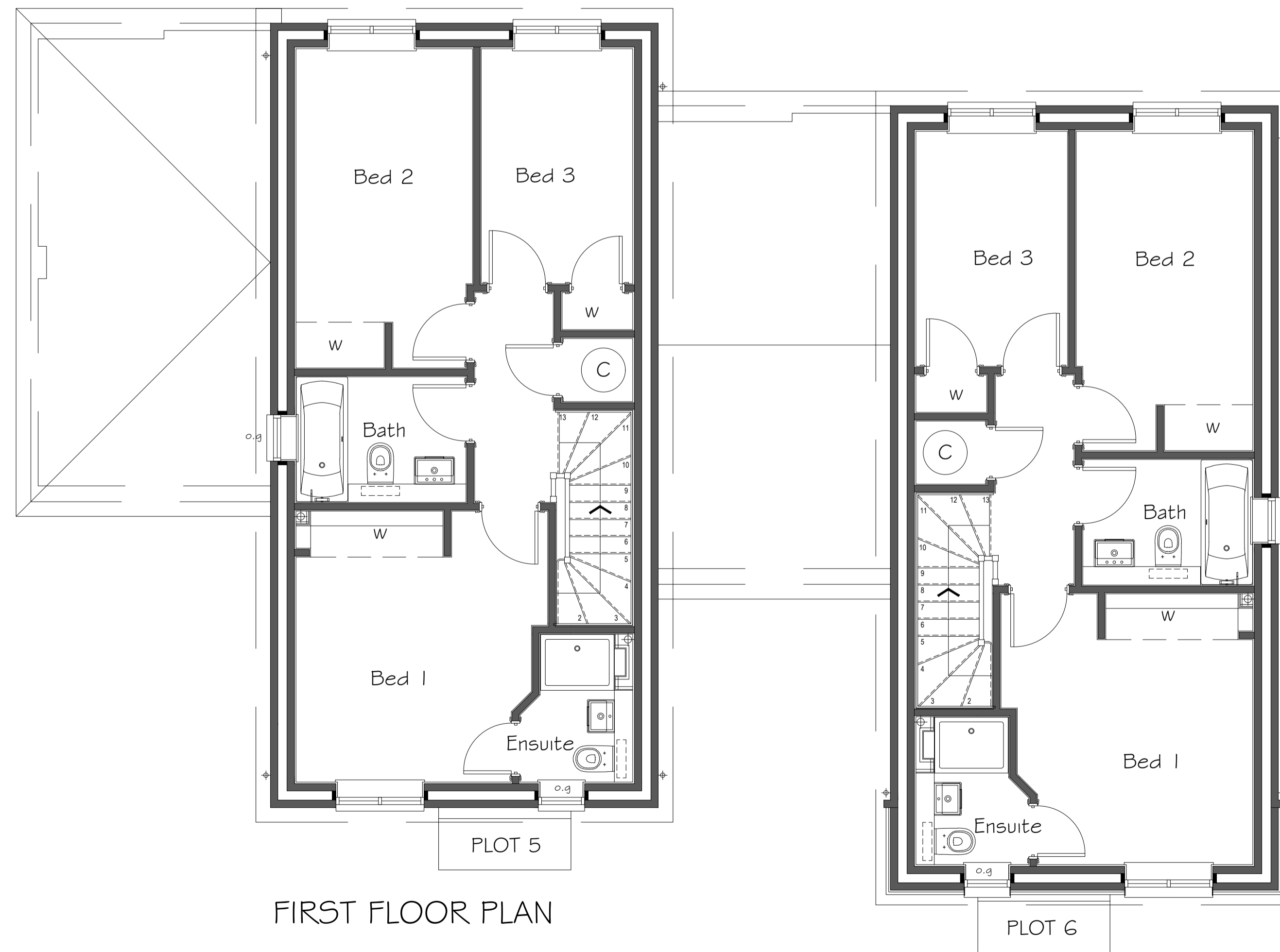
NORTH-EAST ELEVATION



KEY TO MATERIALS	
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GROUND FLOOR PLAN

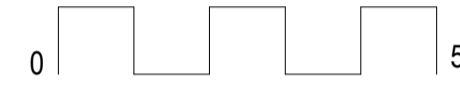


FIRST FLOOR PLAN

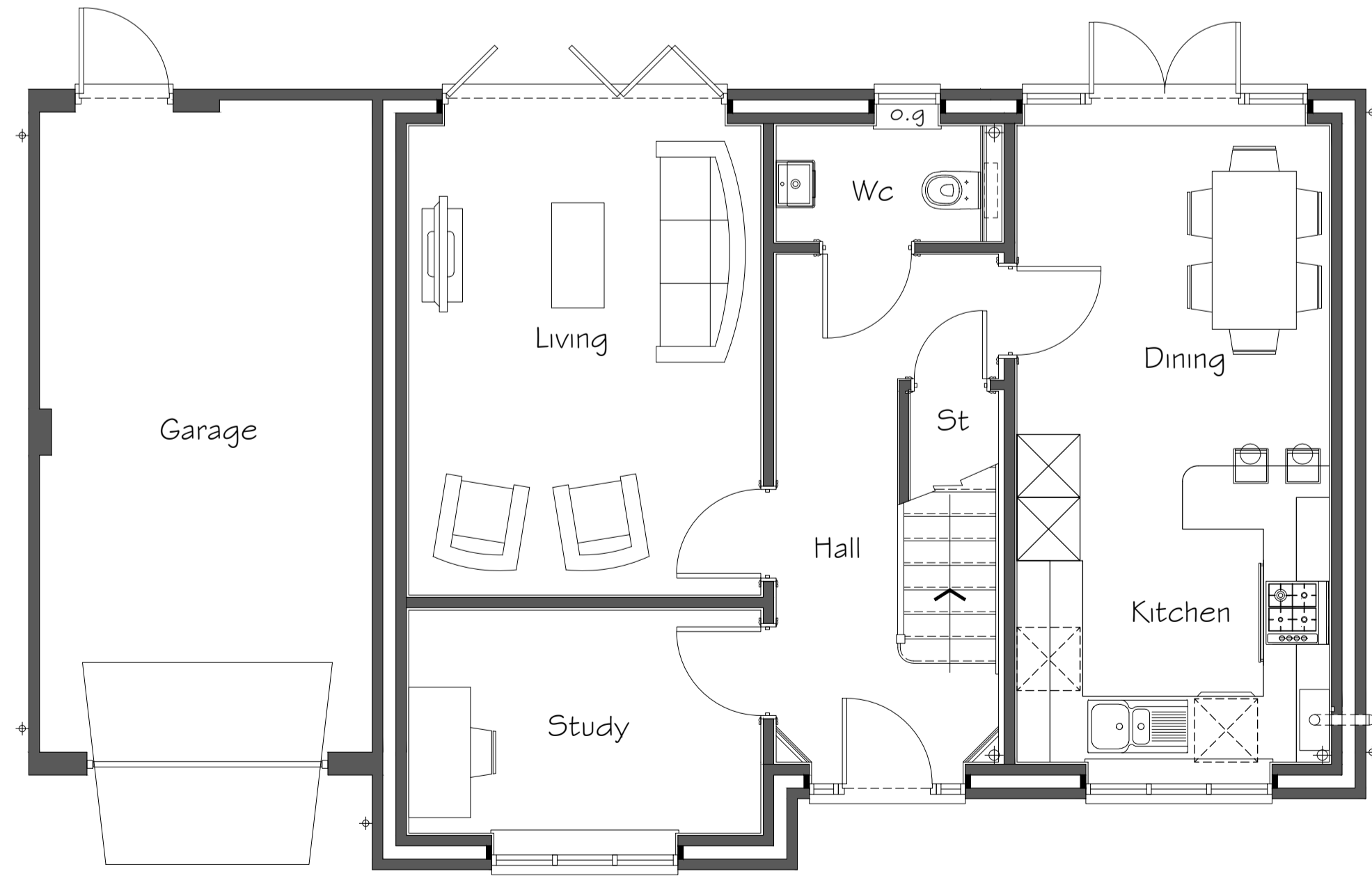
client: DEVINE HOMES PLC  
 project: Land at 19 to 29 Shelveys Way, Tadworth, KT20 5QJ.  
 drawing: Plots 5 & 6 - Proposed Floor Plans & Elevations  
 status: PLANNING  
 scale: 1:50 / 1:100 @ A1  
 date: May 2018  
 drawn: AJC  
 checked: AJC

2018.05.16 AJC  
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 date by ch  
 A PLANNING ISSUE.  
 P1 PRELIMINARY ISSUE.  
 rev. description

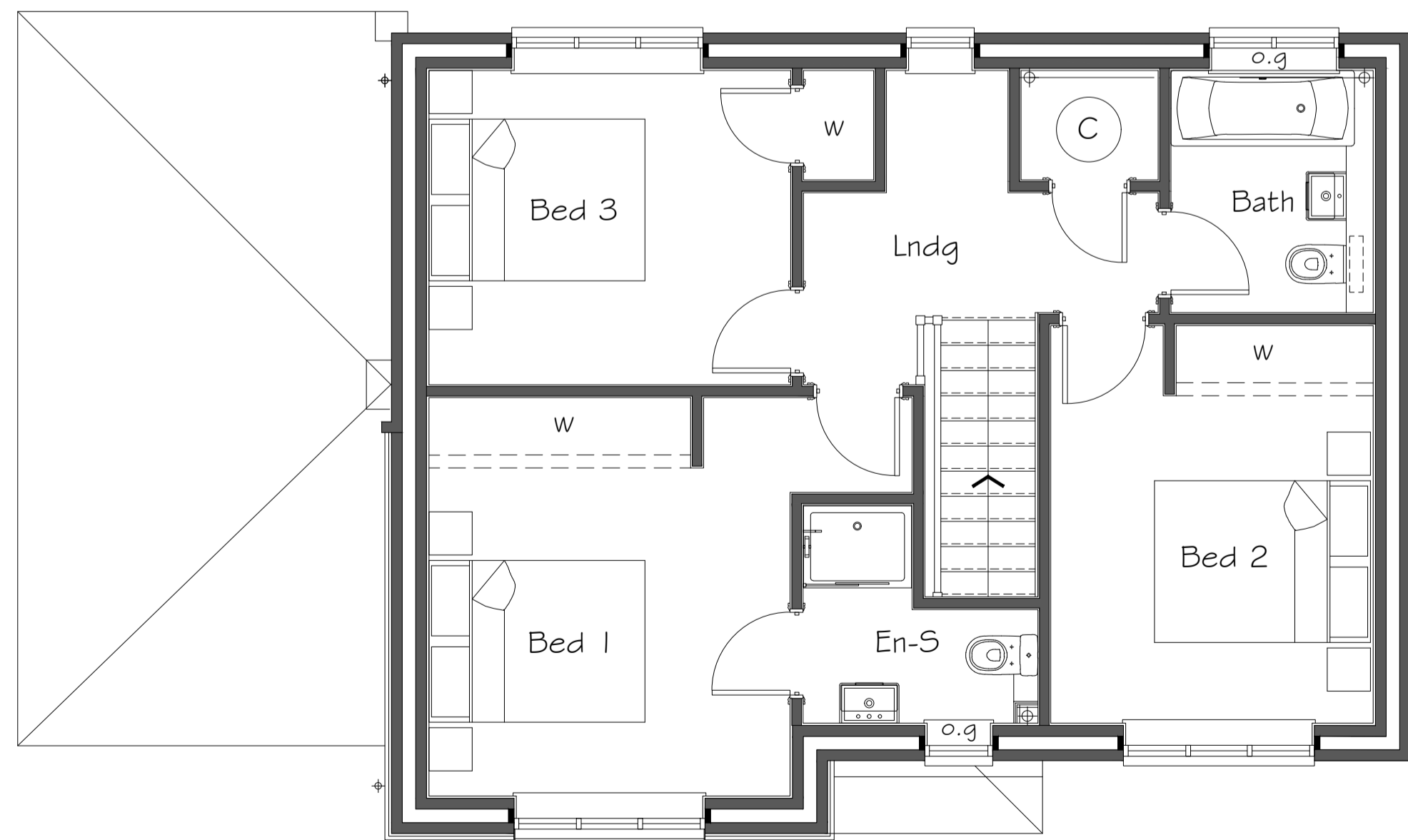
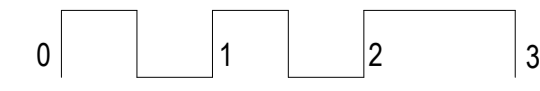
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KEY TO MATERIALS	
1	Facing Brickwork
2	Render
3	Plan Tiles
4	uPVC Windows and Doors
5	Single Ply Membrane
6	Plan Tile Hanging
7	Timber Boarding



GROUND FLOOR PLAN



FIRST FLOOR PLAN

scale:	1:50 / 1:100 @ A1	client:	DEVINE HOMES PLC
date:	May 2018	project:	Land at 19 to 29 Shelveys Way, Tadworth, KT20 5QJ.
drawn:	AJC	drawing:	Plot 3 - Proposed Floor Plans & Elevations
checked:	AJC	status:	PLANNING

2018.05.16	AJC	AJC
2018.05.14	AJC	AJC
date	by	ch

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MODEL FILE REFERENCE- 18007-A-P-PL

PLANNING ISSUE.	A
PRELIMINARY ISSUE.	P1
description	rev.



# Agenda Item 8

Planning Committee  
31<sup>st</sup> October 2018

Agenda Item: 8  
18/01361/F

 <p><b>Reigate &amp; Banstead</b> BOROUGH COUNCIL Banstead   Horley   Redhill   Reigate</p>	<b>TO:</b>	PLANNING COMMITTEE
	<b>DATE:</b>	31/10/2018
	<b>REPORT OF:</b>	HEAD OF PLACES & PLANNING
	<b>AUTHOR:</b>	Matthew Sheahan
	<b>TELEPHONE:</b>	01737 276514
	<b>EMAIL:</b>	Matthew.sheahan@reigate-banstead.gov.uk
<b>AGENDA ITEM:</b>	8	<b>WARD:</b> Nork

<b>APPLICATION NUMBER:</b>	18/01361/F	<b>VALID:</b>	04/07/2018
<b>APPLICANT:</b>	Denton Homes Ltd	<b>AGENT:</b>	WS Planning
<b>LOCATION:</b>	<b>REAR OF 86-90 PARTRIDGE MEAD BANSTEAD</b>		
<b>DESCRIPTION:</b>	<b>Erection of 4 dwellings. As amended on 08/10/2018</b>		
<b>All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail.</b>			

## SUMMARY

This is a full application for the construction of 4 houses located on land to the rears of 86-90 Partridge Mead located within a residential area to the north of the borough in Banstead. The area is typified by dwellings typical of the 1930s-1950s suburban style, consisting of a mix of semi-detached and detached dwellings, with a smaller number of chalet style bungalows. To the west and south of the site is Hornbeam Close, a new development consisting of detached and semi-detached dwellings.

The proposed dwellings have been designed in a traditional style, incorporating hipped roofs, and traditional plain tiles, tile hanging and render. This design style is considered to be appropriate and would be acceptable in terms of impact on the character of the area. They would be set within plots that reflect those found within the locality, particularly properties occupying Hornbeam Close.

Regarding impact on neighbouring residential amenity, it is considered that the separation distances and siting of the proposed dwellings would ensure that the presence of the development is not overbearing in nature, nor would it impact on light provision to neighbouring properties. Whilst it is acknowledged that there would be some overlooking allowed from front and rear windows over rear gardens of a number of properties on Hornbeam Close, it would not be to an extent that would be considered sufficiently harmful to warrant refused. Initial concerns raised in regards to the scale of development, particularly plot 4, and the impact of this on properties occupying Hornbeam Close, have been overcome by the revised design for two pairs of semi-detached dwellings, which moves the bulk of development away from these properties to a point where the level of harm to amenity would not be objectionable.

The scheme provides opportunity for landscaping to soften the harder design elements, particularly to the frontages. A condition would be attached to a grant of permission requiring further detail of a landscaping scheme to be submitted to the local planning authority prior to the commencement of development.

The Highway Authority has assessed the application and are satisfied that the development would not prejudice safety or the free flow of traffic on the adjoining highway network

In conclusion it is considered that the proposal would be acceptable in terms of the design, appearance and impact on the visual character of the area. The level of impact on the neighbouring residential amenity is deemed to be acceptable in this case. The proposal would therefore comply with policies Ho9, Ho13, Ho14, Ho16 and Ho18 of the Borough Local Plan.

### **RECOMMENDATION(S)**

Planning permission is **GRANTED** subject to conditions.

### **Consultations:**

Highway Authority: The County Highway Authority has undertaken an assessment in terms of the likely net additional traffic generation, access arrangements and parking provision and are satisfied that the application would not have a material impact on the safety and operation of the adjoining public highway. The County Highway Authority therefore has no highway requirements.

Nork Residents Association: Concerns have been raised that the development would be car dominated, with an increased volume of traffic and disturbance, resulting in issues of highway safety. The elevated ground level would give rise to opportunity for overlooking; the layout is cramped with minimum spacing between buildings; there is no place for children to play; there is inadequate parking and insufficient spaces for visitors and delivery or emergency vehicles; loss of wildlife habitats; loss of mature trees; this is a 'greenfield site'.

### **Representations:**

Letters were sent to neighbouring properties on 6<sup>th</sup> July 2018. Following receipt of amended drawings further letters were sent 10<sup>th</sup> October 2018.

10 responses have been received raising the following issues:

Overbearing relationship	See paragraph 6.9-6.21
Out of character with the surrounding area	See paragraph 6.3-6.6
Overdevelopment	See paragraph 6.3-6.6
Overlooking and loss of privacy	See paragraph 6.9-6.21
Poor design	See paragraph 6.3-6.6
Loss of light and overshadowing	See paragraph 6.9-6.21
No need for the development	See paragraph 6.3-6.6
Loss of/ harm to trees	See paragraph 6.22
Loss of private view	See paragraph 6.9-6.21
Hazard to highway safety	See paragraph 6.23-6.25
Inadequate parking	See paragraph 6.23-6.25
Increased traffic congestion	See paragraph 6.23-6.25
Car dominated	See paragraph 6.3-6.6
Poor access	See paragraph 6.3-6.6
Increased noise and disturbance	See paragraph 6.9-6.21
Conflict with a covenant	See paragraph 6.9-6.21
Harm to wildlife	See paragraph 6.9-6.21
Property values	See paragraph 6.9-6.21
Crime fears	See paragraph 6.9-6.21
Drainage capacity	See paragraph 6.9-6.21



Lack of play space  
Harm to greenbelt

See paragraph 6.9-6.21

## **1.0 Site and Character Appraisal**

1.1 The application site consists of the rear gardens of numbers 86-90 Partridge Mead. It is also located to the rear of the newly constructed development at Hornbeam Close, sited to the west of Reigate Road. The area is residential in character, comprised predominantly of semi-detached dwelling houses set within quite long narrow plots along Partridge Mead to the west. A mix of detached and semi-detached dwellings defines Hornbeam Close, though Reigate Road is similar in character to those of Partridge Mead. Properties along Partridge Mead were largely constructed in the 1930s-50s, typical of the suburban style. The character and appearance of properties along both Partridge Mead and Reigate Road do vary, particularly in terms of facing materials, though there are common features, such as the hipped roof forms. The ground level decreases gradually from south to north.

## **2.0 Added Value**

- 2.1 Improvements secured at the pre-application stage: The applicant did not approach the Council for pre-application advice prior to the submission of the current planning application.
- 2.2 Improvements secured during the course of the application: Amended plans have been received following concerns raised regarding overdevelopment of the site and impact on neighbouring residential properties.
- 2.3 Further improvements could be secured: Improvements to the scheme could be secured by way of suitably worded conditions.

## **3.0 Relevant Planning and Enforcement History**

There is no planning or enforcement history relating to the proposed application site. Relevant planning applications within the wider area are listed below.

17/00804/F	Construction of 2 semi-detached houses and 1 detached house, along with associated access, parking and landscaping. As amended on 22/06/2017 and on	Approved with Conditions 25.10.18
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31/08/2017.

14/01307/F	Demolition of 377 Reigate Road, Epsom Downs and the erection of 10 dwellings with associated access and parking Amended via planning portal 22/07/2014 and 11/08/2014.	Approved with Conditions 10.02.15
14/00373/F	Demolition of 377 Reigate Road, Epsom Downs and the erection of 15 dwellings with associated access and parking. Amended via the planning portal	Refused 05.06.14 Appeal Dismissed
14/01303/F	Demolition of 377 Reigate Road, Epsom Downs and the erection of 10 dwellings with associated access and parking. Amended via planning portal 11/8/2014	Refused 25.09.14 Appeal Dismissed

#### **4.0 Proposal and Design Approach**

- 4.1 This is a full application for the construction of a total of 4 dwellings on land to the rear of properties 86-90 Partridge Mead. The development would consist of 2 pairs of semi-detached dwellings, three of which would have attached garages to the flank elevations. This follows the initial proposal, which was to provide a pair of semi-detached dwellings flanked by two detached dwellings. Each property would be afforded 4 bedrooms, one of which would have an en-suite bathroom. Each would have a rear garden of approximately 12m in depth, with small areas of patio approximately 3.5m in depth. Parking for two vehicles to the front of each property would be provided.
- 4.2 The properties have been traditionally designed, with hipped roofs of plain tile, with hanging tile to the first floor front elevation. Brickwork defines the ground floor, with soldier brick arches above the window head. They would feature single storey pitched roof elements to the rear elevations. The dwellings measure 8.6m in height. The properties would have a depth of 12.6m, sited in an irregular shaped plot. The upper floors would feature side facing windows serving bathrooms. Landscaping would be provided to the front, with existing trees and hedges remaining within the site to the proposed

rear and side boundaries. It was initially proposed to provide a pair of semi-detached dwellings flanked by two 2 storey detached dwellings.

- 4.3 A design and access statement should illustrate the process that has led to the development proposal, and justify the proposal in a structured way, by demonstrating the steps taken to appraise the context of the proposed development. It expects applicants to follow a four-stage design process comprising:  
Assessment;  
Involvement;  
Evaluation; and  
Design.

- 4.4 Evidence of the applicant’s design approach is set out below:

Assessment	The design and access statement submitted in support of the application identifies the surrounding area as being characterised by 1930s-50s suburbia with deep rectangular gardens. Properties are described as being red brick buildings with some comprising render and tile hanging. There is an identified mix of 2 storey detached and semi-detached houses with a smaller number of bungalows. A number of infill developments are referenced.
	No site features worthy of retention were identified.
Involvement	No community consultation is intimated as having taken place.
Evaluation	It is not indicated that alternative development options have been considered.
Design	It is considered within the statement that the plot sizes and widths would be comparable to those that prevail within the surrounding area, as would the level of separation between dwellings. The design of the dwellings is considered to utilise materials and palette of colours that reflects the character of the wider area, particularly those of Hornbeam Close

- 4.5 Further details of the development are as follows:

Site area	0.145 hectares
Proposed parking spaces	6
Parking standard	2 car parking spaces per three bedroom dwelling unit



## 5.0 Policy Context

### 5.1 Designation

Urban area

### 5.2 Reigate and Banstead Core Strategy

CS1(Sustainable Development)  
CS10 (Sustainable Development),

### 5.3 Reigate & Banstead Borough Local Plan 2005

Housing	Ho9, Ho13, Ho16, Ho18
Trees	Pc4
Movement	Mo5, Mo7

### 5.4 Other Material Considerations

National Planning Policy Framework  
National Planning Practice Guidance  
Supplementary Planning Guidance

Surrey Design  
Local Distinctiveness Design Guide  
A Parking Strategy for Surrey  
Parking Standards for Development

Other	Human Rights Act 1998 Community Infrastructure Levy Regulations 2010
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## 6.0 Assessment

6.1 The application seeks permission for the erection of four dwellings.

6.2 The main issues to consider are:

- Design and Impact on the character of the area;
- Amenity of future occupiers;
- Neighbour amenity;
- Trees and Landscaping;
- Highway Impact, Access and parking

### Design and Character

6.3 The properties have been designed to reflect the style that is typical of the 1930s-1950s suburban style found in this area. This is considered to be an appropriate design approach. The semi-detached properties would incorporate hipped roof forms of traditional plain tiles, brickwork to the ground floor. The fenestration would match the character of the dwelling. The

attached garages to plots 4, 5 and 6 would be low scale with a roof pitch to match the existing. The overall appearance of the dwellings would continue the character of the new constructed dwellings in Hornbeam Close, which features a mix of detached and semi-detached dwellings which utilise hanging tile, brick and render of a tawny colour palette typical of the Surrey vernacular.

- 6.4 The proposed plot sizes, whilst smaller than is typical of the majority of Partridge Mead, would reflect the plot sizes established by Hornbeam Close to the west, in the context of which the development should also be read. Policy Ho14 of the Local Plan requires new development within back garden land to conform to the general pattern and form of development within the area, and that plot sizes and spacing between dwellings is also reflective of that established within the wider area. Previous development in the immediate locality has established the acceptability of plot of this size. The level of separation between the two pairs of semi-detached dwellings is considered acceptable. Whilst there would only be 2m between the garages of plots 5 and 6, this gap would increase to 8m at the first floor. It is considered that the proposed development would comply with these requirements and would be acceptable. The gaps between 4 and 6 Hornbeam close as well as 6 and 8 is noticeably closer.
- 6.5 It is a fundamental objective of planning policy and stated within the National Planning Policy Framework that we provide high quality housing that is well designed and built to a high standard. The advice is amplified further by policies Ho9 and Ho18 of the Borough Local Plan which states that the environment created for residents of the proposed development must be satisfactory. Although policy does not specifically require that new dwellings/conversions be built to a specific minimum size, it is implicit in the advice mentioned above that they are of a size to provide adequate standards of living for the future residents. Government guidance exists in the form of technical standards regarding the appropriate size of new dwellings. It is considered that the standard of living space provided would be acceptable and consistent with that typical of a residential area of this kind, where there is something of a mix of property sizes. The level of outdoor amenity space provision would also be acceptable.
- 6.6 The plot frontages would feature additional landscaping, as well as maintaining existing vegetation within the site to the proposed side and rear boundaries. This level of planting is broadly reflective of that established as acceptable by the Hornbeam Close development. Indeed it is considered that the level of proposed planting would exceed that of these neighbouring properties.

#### Amenity for future occupiers

- 6.7 It is a fundamental objective of planning policy and stated within the National Planning Policy Framework that we provide high quality housing that is well designed and built to a high standard. The advice is amplified further by policies Ho9 and Ho18 of the Borough Local Plan which states that the

environment created for residents of the proposed development must be satisfactory. Although policy does not specifically require that new dwellings/conversions be built to a specific minimum size, it is implicit in the advice mentioned above that they are of a size to provide adequate standards of living for the future residents. Government guidance exists in the form of technical standards regarding the appropriate size of new dwellings.

- 6.8 It is considered that the standard of living space provided would be acceptable and consistent with that typical for properties of this kind, where there is something of a mix of property sizes in the area. The level of outdoor amenity space provision would also be acceptable, providing sufficient space for the enjoyment of any future occupiers, reflecting the size of gardens throughout Hornbeam Close.

Impact on neighbouring amenity

- 6.9 The rear elevations of the proposed dwellings would face towards numbers 3 to 13 Hornbeam Close at an angle. Plot 4 would be in closest proximity to 7, 9, 11 and 13. Regarding separation distance, the dwelling would be 4.7m from the west boundary of the site at the closest point as measured from the attached garage. It would be approximately 19m from the rear elevation of number 11, 18m from number 9, and 20m from number 7. The front of plot 4 would be angled towards number 13. Two upper floor side windows, which serve bathrooms, would face these properties. The rear bedroom windows would face the direction of the rear gardens of numbers 3, 5 and 7 Hornbeam Close.
- 6.10 It is considered that the two proposed side facing windows would not result in harmful overlooking. The windows would serve bathrooms and therefore it is expected that these would be obscure glazed. In any case a condition would be attached to any grant of planning permission requiring these windows to be obscured glazed. As regards to the rear facing windows, by virtue of the position of the dwellings within site, they are angled away from the rear of these adjoining properties. Whilst an element of overlooking could occur from the window serving bedroom 3 of plot 4, it would not be significant enough to warrant refusal on this basis. Plots 5, 6, and 7 are angled further away still, considerably limiting opportunity for views over properties of Hornbeam Close. Whilst they would face more towards 78-84 Partridge Mead, the distance between these dwellings would exceed 40m, with intervening trees screening some of the views. Therefore it is considered that on the grounds of overlooking and loss of privacy, refusal could not be justified.
- 6.11 Concern was initially raised with the original layout of the site, which proposed a pair of semi-detached dwellings flanked by two 2 storey detached dwellings, would have been particularly dominant and overbearing in nature, particularly on 7, 9 and 11 Hornbeam Close. In order to address this concern amendments were submitted to allow for a pair of semi-detached dwellings. It is considered that, on balance, this has addressed these concerns. The bulk and height of the dwellings has been moved away from the west boundary, with the attached garage of plot 4 being in closest proximity to Hornbeam



Close, reducing the level of overbearing to a point that would be acceptable. Whilst plot 7 would remain two storeys at the boundary, it would be a sufficient distance from the donor properties on Partridge Mead that it would not be overbearing in nature.

- 6.12 Turning to the issue of loss of light and overshadowing, whilst some overshadowing would occur to properties on Hornbeam Close in the early part of the day it would not be for a sufficient enough period of the day to warrant refusal on these grounds. The Councils' Householder Extensions and Alterations SPG advises that any new development or building that faces the rear window of a neighbouring property should not extend in to a 25 degree line as measure 2m above the floor level as measured from this window. In the event that this were to occur, the likelihood of harmful loss of light to these rear windows is likely to increase. The ridge of plot 4 complies with the 25 degree line and with this and the juxtaposition it is considered that the proposed built relationship, whilst result in in change, would accord with adopted policy.
- 6.13 Objection has been made on the grounds that the development would result in a poor outlook for neighbouring properties. It is recognised that the development would result in change but it is not considered that this would be materially harmful as the proposed dwellings would be a sufficient distance away. It is also the case that a 'right to a view' is not a material planning consideration in the assessment of a planning application.
- 6.14 Regarding inconvenience and increased noise and disturbance during construction, whilst it is accepted that this can be an issue for residents, it is a temporary impact. Statutory noise legislation is in place to deal with excessive noise levels.
- 6.15 Regarding impact on the existing drainage arrangements the area is not identified as suffering from surface water flooding. Issues affecting the drainage would be considered at the building control stage.
- 6.16 On the issue of harm to wildlife habitats, the site is not within a dedicated site of special scientific interest, Special Area of Conservation or nature reserve, and no protected species have been identified within the submission as being present on site. In any case separate legislation is in place to protect such species during the course of development.
- 6.17 Concern has been raised regarding harm to the green belt. This site is not located within the Green Belt.
- 6.18 It has been identified that the proposed development does not provide play space for children. There is not a policy requirement for a development of this kind to provide such spaces; therefore it would not be reasonable to refuse the application on these grounds.
- 6.19 As regards to concern about impact on property values, this is not a material planning consideration that can be taken in to account.

- 6.20 Any concern around crime fears would be a police matter.
- 6.21 The proposal is therefore considered to comply with policies Ho9, Ho13, Ho14 and Ho16 of the Borough Local Plan 2005.

#### Trees and Landscaping

- 6.22 The Councils' Tree Officer has been consulted on the application. He has reviewed the arboricultural report from ACS and the trees shown to be removed, which includes to B grade specimens, will have minimal impact on the character of the local area. The proposed layout allows replacement planting to be carried out and to ensure adequate sized specimens are secured a landscape condition is necessary. Conditions would be applied to a grant of permission requiring a tree protection plan to be implemented on site in accordance with submitted plans, and landscaping scheme to be submitted to and approved in writing prior to development commencing. The proposal would therefore comply with policies Ho9 and Pc4 in this regard.

#### Highway Impact, Access and parking

- 6.23 The Highway Authority has been consulted on the application and is satisfied that the application would not have a material impact on the safety and operation of the adjoining public highway. The application site is accessed via Hornbeam Close, a private road which does not form part of the public highway network. Therefore the Highway Authority has to consider the wider impact of the proposed development and considers that there would not be a sufficient impact on the safe operation of the highway or the free flow of traffic on the public highway network to warrant a refusal of the application on these grounds. The County Highway Authority therefore has no highway requirements and the application would comply with policy Mo 7 of the Borough Local Plan in this regard. The existing access to Hornbeam Close would be utilised which would be acceptable.
- 6.24 Concern has been raised that the cumulative impact of additional traffic resulting from the proposed development and that generated by the extant permission opposite the site granted under reference 17/00804/F would result in an increased risk to highway safety. This development has not yet commenced therefore the proposal can only be assessed on its own merits and it is considered that the addition of four new dwellings would not generate a level of traffic to warrant refusal on highway safety grounds.
- 6.25 Regarding parking, a C3 dwelling unit with 3 or more bedrooms would be required to provide a maximum of 2 spaces. Plots 5,6, and 7 would provide two spaces as well as the garage. Plot 4 is indicated as having a single space although the front of the plot is large and could adequately provide for additional parking.

## CONDITIONS

1. The development hereby permitted shall be carried out in accordance with the following approved plans:

<b>Plan Type</b>	<b>Reference</b>	<b>Version</b>	<b>Date Received</b>
Elevation Plan	073-01-19	A	08.10.2018
Elevation Plan	073-01-18	A	08.10.2018
Block Plan	073-01-13	A	08.10.2018
Floor Plan	073-01-16	A	08.10.2018
Floor Plan	073-01-15	A	08.10.2018
Combined Plan	073-01-14	A	08.10.2018
Location Plan	073-01-11	A	08.10.2018
Other Plan	181020/TR/02	A	08.10.2018
Street Scene	UNNUMBERED	A	08.10.2018
Site Layout Plan	UNNUMBERED		08.10.2018
Arboricultural method statement	reference jc/aiams1/86- 90pm		22.06.2018

Reason:

To define the permission and ensure the development is carried out in accord with the approved plans and in accordance with National Planning Practice Guidance.

2. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason:

To comply with Section 91(1) of the Town and Country Planning Act 1990 as amended by Section 51 (1) of the Planning and Compulsory Purchase Act 2004.

3. No development shall take place until samples of the materials to be used in the construction of the external surfaces, including fenestration and roof, have been submitted to and approved in writing by the Local Planning Authority, and on development shall be carried out in accordance with the approved details.

Reason: To ensure that a satisfactory external appearance is achieved of the development with regard to Reigate and Banstead Borough Local Plan 2005 policies Ho9 and Ho13.

4. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015 (or any order revoking and re-enacting that Order with or without modification), no extensions permitted by Classes A, B and C of Part 1 of the Second Schedule of the 2015 Order shall be constructed.



Reason: To control any subsequent enlargements in the interests of the visual and residential amenities of the locality with regard to Reigate and Banstead Borough Local Plan 2005 policies Ho9, Ho13, and Ho16.

5. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015, (or any Order revoking and re-enacting that Order with or without modification), no first floor windows, dormer windows or rooflights other than those expressly authorised by this permission shall be constructed.

Reason: To ensure that the development does not affect the amenity of the neighbouring properties by overlooking and to protect the visual amenities of the area in accordance with Reigate and Banstead.

6. The first floor windows in the side elevations of the development hereby permitted shall be glazed with obscured glass which shall be fixed shut, apart from a top hung opening fanlight whose cill height shall not be less than 1.7 metres above internal floor level, and shall be maintained as such at all times.

Reason: To ensure that the development does not affect the amenity of the neighbouring property by overlooking with regard to Reigate and Banstead Borough Local Plan 2005 policy Ho9.

7. No development shall commence including groundworks preparation and demolition until all related arboricultural matters, including arboricultural supervision, monitoring and tree protection measures are implemented in strict accordance with the approved details contained in the Tree Protection Plan and Arboricultural Method Statement compiled by ACS (Trees) Consulting dated 11<sup>th</sup> April 2018, reference jc/aiams1/86-90pm

Reason

To ensure good arboricultural practice in the interests of the maintenance of the character and appearance of the area and to comply with policy Pc4 of the Reigate and Banstead Borough Local Plan 2005 and the recommendations within British Standard 5837.

8. No development shall commence on site until a scheme for the landscaping of the site including the retention of existing landscape features has been submitted to and approved in writing by the LPA. Landscaping schemes shall include details of hard and soft landscaping, including any tree removal/retention, planting plans, written specifications (including cultivation and other operations associated with tree, shrub, and hedge or grass establishment), schedules of plants, noting species, plant sizes and proposed numbers/densities and an implementation and management programme.

All hard and soft landscaping work shall be completed in full accordance with the approved scheme, prior to occupation or within the first planting season following completion of the development hereby approved or in accordance with a programme agreed in writing with the local planning authority.

Any trees shrubs or plants planted in accordance with this condition which are removed, die or become damaged or become diseased within five years of planting shall be replaced within the next planting season by trees, shrubs of the same size and species.

Reason:

To ensure good arboricultural and landscape practice in the interests of the maintenance of the character and appearance of the area and to comply with policies Pc4, Pc12, Ho9 of the Reigate and Banstead Borough Local Plan 2005.

## INFORMATIVES

1. Your attention is drawn to the safety benefits of installing sprinkler systems as an integral part of new development. Further information is available at [www.firesprinklers.info](http://www.firesprinklers.info).
2. The applicant is encouraged to provide renewable technology within the development hereby permitted in order to reduce greenhouse gas emissions.
3. The applicant is advised that prior to the initial occupation of any individual dwelling hereby permitted, a 140 litre wheeled bin conforming to British Standard BSEN840 and a 60 litre recycling box should be provided for the exclusive use of the occupants of that dwelling. Prior to the initial occupation of any communal dwellings or flats, wheeled refuse bins conforming to British Standard BSEN840, separate recycling bins for paper/card and mixed cans, and storage facilities for the bins should be installed by the developer prior to the initial occupation of any dwelling hereby permitted. Further details on the required number and specification of wheeled bins and recycling boxes is available from the Council's Neighbourhood Services on 01737 276501 or 01737 276097, or on the Council's website at [www.reigate-banstead.gov.uk](http://www.reigate-banstead.gov.uk). Bins and boxes meeting the specification may be purchased from any appropriate source, including the Council's Neighbourhood Services Unit on 01737 276775.
4. You are advised that the Council will expect the following measures to be taken during any building operations to control noise, pollution and parking:
  - (a) Work that is audible beyond the site boundary should only be carried out between 08:00hrs to 18:00hrs Monday to Friday, 08:00hrs to 13:00hrs Saturday and not at all on Sundays or any Public and/or Bank Holidays;
  - (b) The quietest available items of plant and machinery should be used on site. Where permanently sited equipment such as generators are necessary, they should be enclosed to reduce noise levels;
  - (c) Deliveries should only be received within the hours detailed in (a) above;
  - (d) Adequate steps should be taken to prevent dust-causing nuisance beyond the site boundary. Such uses include the use of hoses to damp down stockpiles of materials, which are likely to generate airborne dust,

to damp down during stone/slab cutting; and the use of bowsers and wheel washes;

- (e) There should be no burning on site;
- (f) Only minimal security lighting should be used outside the hours stated above; and
- (g) Building materials and machinery should not be stored on the highway and contractors' vehicles should be parked with care so as not to cause an obstruction or block visibility on the highway.

Further details of these noise and pollution measures can be obtained from the Council's Environmental Health Services Unit.

In order to meet these requirements and to promote good neighbourliness, the Council recommends that this site is registered with the Considerate Constructors Scheme - [www.ccscheme.org.uk/index.php/site-registration](http://www.ccscheme.org.uk/index.php/site-registration).

5. The use of landscape/arboricultural consultant is considered essential to provide acceptable submissions in respect of the above relevant conditions. The planting of trees and shrubs shall be in keeping with the character and appearance of the locality. There is an opportunity to incorporate substantial sized trees into the scheme to provide for future amenity and long term continued structural tree cover in this area. It is expected that the replacement structural landscape trees will be of Extra Heavy Standard size with initial planting heights of not less than 4m with girth measurements at 1m above ground level in excess of 14/16cm.

## **REASON FOR PERMISSION**

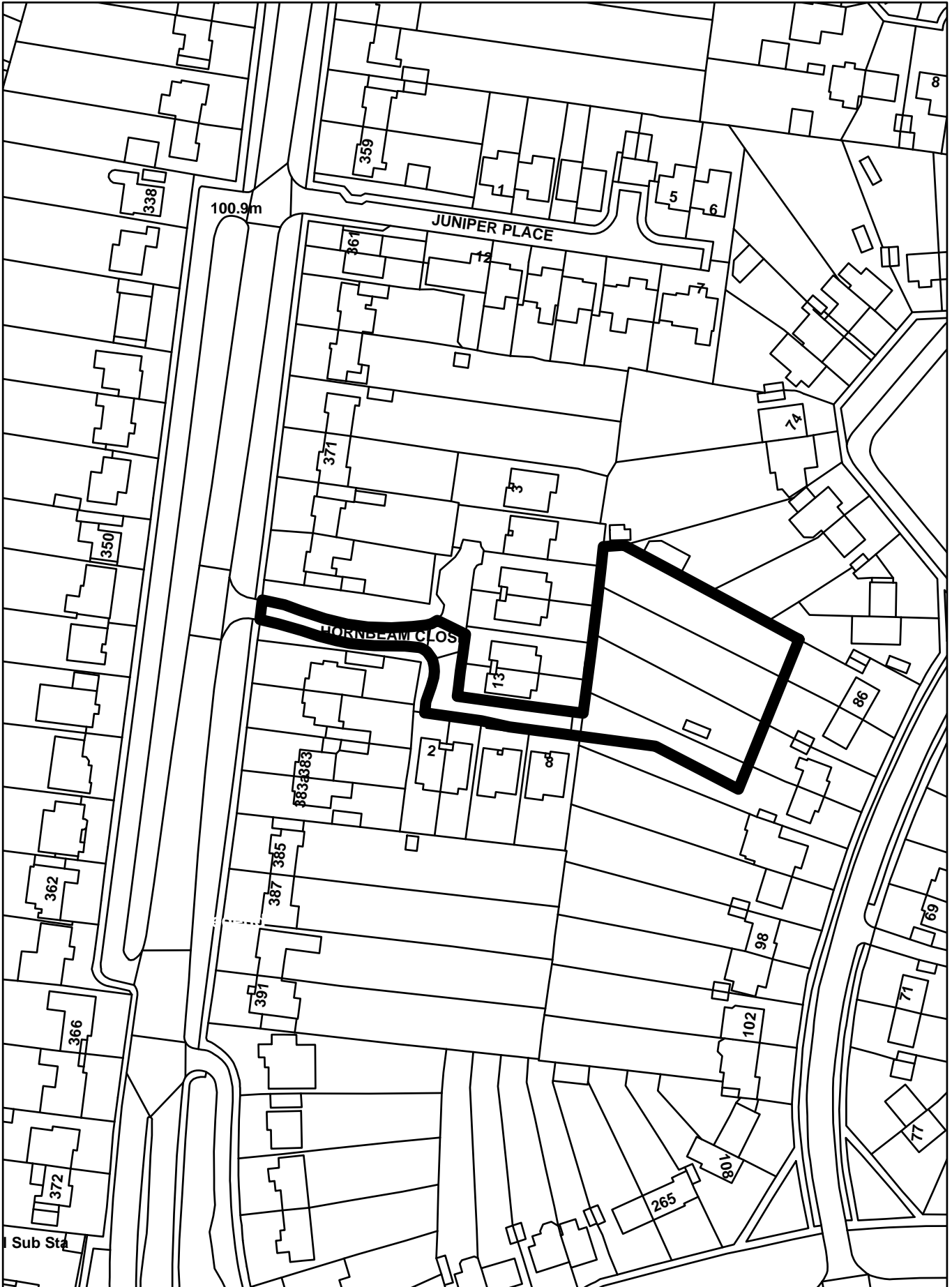
The development hereby permitted has been assessed against development plan policies Ho9, Ho13, Ho16, Mo5 and Mo7 and material considerations, including third party representations. It has been concluded that the development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.

## **Proactive and Positive Statements**

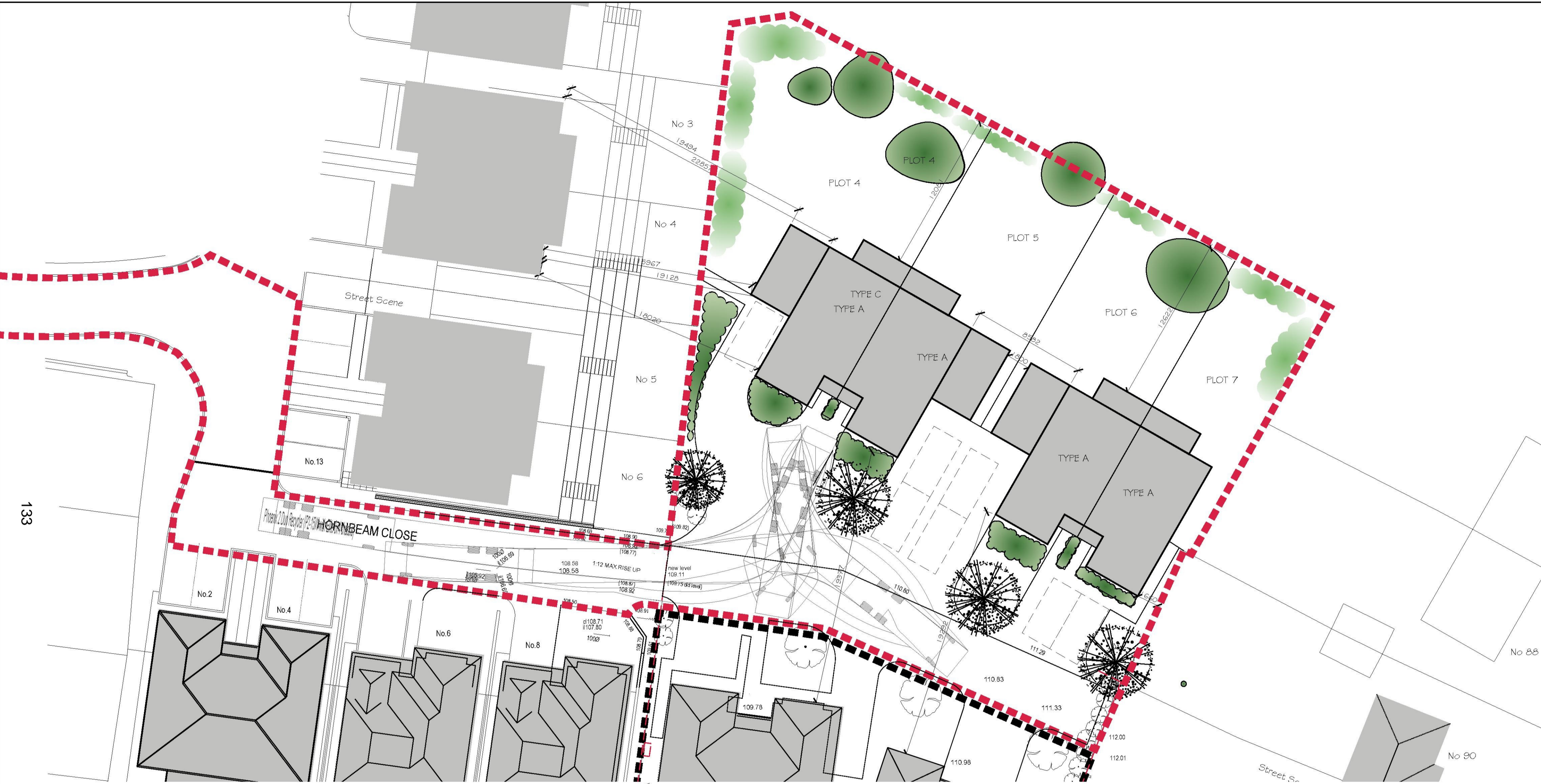
The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.



# 18/01361/F - Rear Of 86 - 90 Partridge Mead, Banstead







Revision A 27.09.18 Unit Types amended and garages added

V:\Business Operations\Developments\02\_Planning Projects\0073\_Partridge Mead\2\04\_Planning Applications\2\_Drawings\073\_Partridge Mead Extension Revised Application.dwg

0 5 10 15 20


	Project	LAND TO REAR OF 86 - 90 PARTRIDGE MEAD					
	Drawing Title	PROPOSED SETTING OUT & STREET SCENE					
<small>(c) COPYRIGHT IN THE DRAWING AND WORK EXECUTED IS AND REMAINS THE PROPERTY OF THE DENTON HOMES LIMITED AND OR ITS SUBSIDIARIES IN ACCORDANCE WITH THE PROVISIONS OF THE COPYRIGHT ACT. PERMISSION TO PRODUCE COPIES OR MODIFIED DRAWINGS IN WHOLE OR IN PART FOR ANY PURPOSE SHOULD BE OBTAINED FROM DENTON HOMES LIMITED &amp; ANY SUCH REPRODUCTIONS SHOULD INCLUDE THIS &amp; THE FOREGOING STATEMENT.</small>	Date	Feb 2018	Scale	1:200@A2	Status	Drawing number	Rev
						073-01-14	A
		<b>DENTON HOMES LTD</b> THE REAR BARN, THE MANOR FARM, 124 MANOR ROAD NORTH THAMES DITTON, SURREY, KT7 0BH					



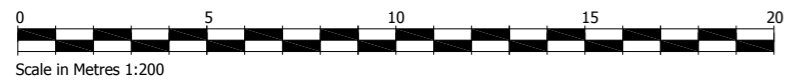


Revision A 27.09.18 Unit Types amended and garages added

0 5 10 15 20  
Scale in Metres 1:200

	Project <b>LAND TO REAR OF 86 -90 PARTRIDGE MEAD</b>				
	Drawing Title <b>PROPOSED SITE PLAN</b>				
Date Feb 2018		Scale 1:200@A2	Status	Drawing number 073-01-13	Rev A
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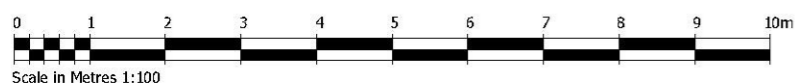








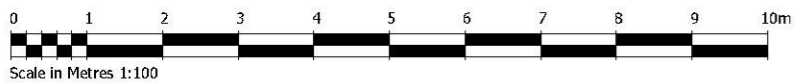
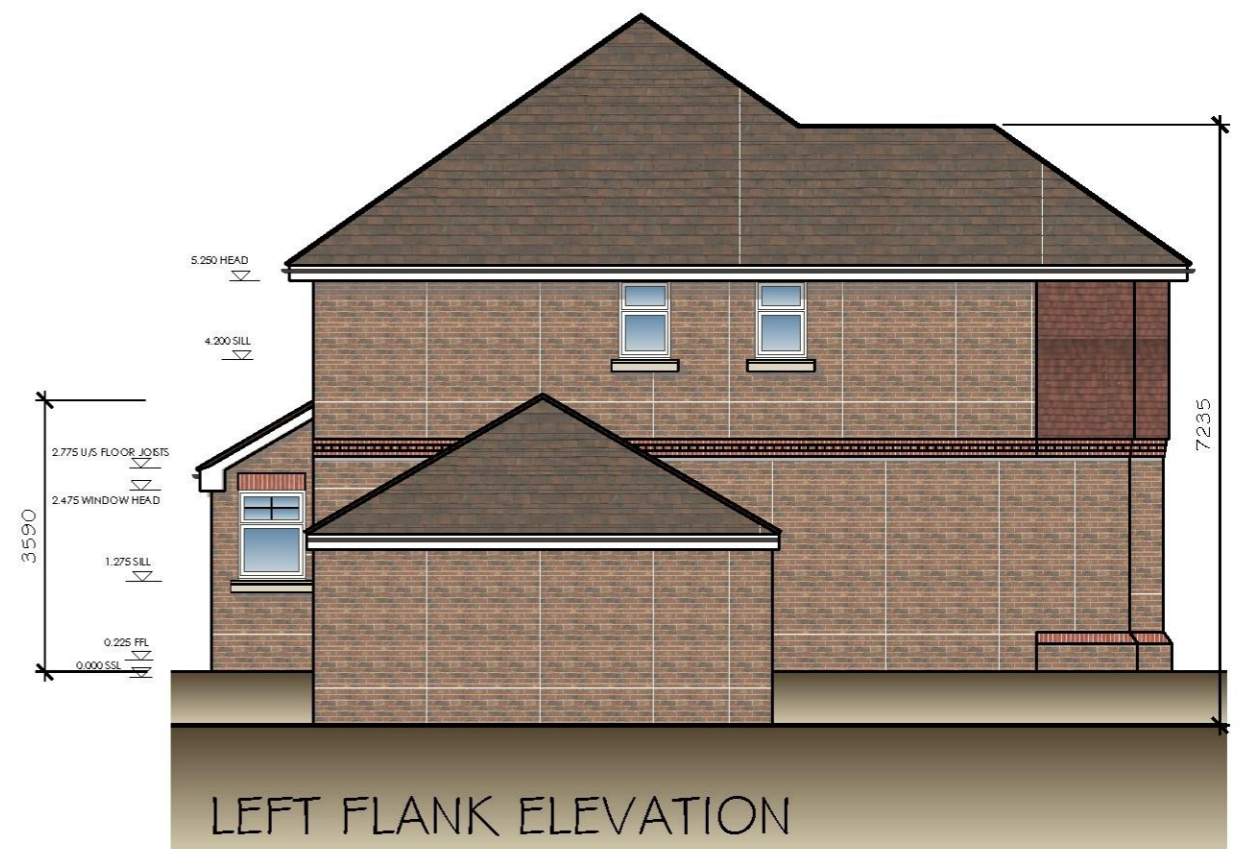
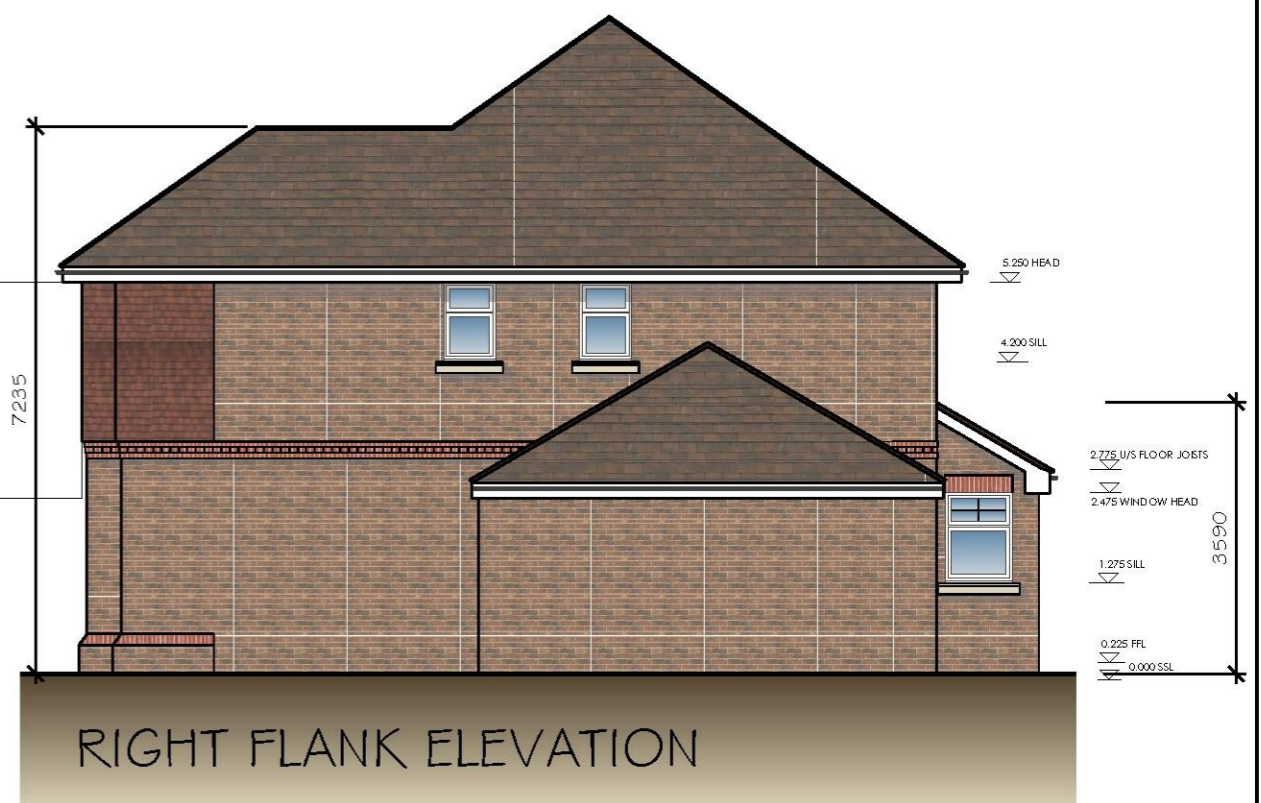
Revision A 27.09.18 Unit Types amended and garages added



V:\Business Operations\Developments\02\_Planning Projects\0073\_Partridge Mead 2\04\_Planning Applications\2\_Drawings\073 Partridge Mead Extension Revised Application.dwg

	Project <b>LAND TO REAR OF 86 - 90 PARTRIDGE MEAD</b>				
	Drawing Title <b>PROPOSED ELEVATIONS - Plots 6 &amp; 7</b>				
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	Feb 2018	1:100@A3		073-01-19	A
<b>DENTON HOMES LTD</b> THE REAR BARN, THE MANOR FARM, 124 MANOR ROAD NORTH THAMES DITTON, SURREY, KT7 0BH					





Project  
**LAND TO REAR OF 86 - 90 PARTRIDGE MEAD**

Drawing Title  
**PROPOSED ELEVATIONS - Plots 4 & 5**

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	Feb 2018	1:100@A3		073-01-18	A
<b>DENTON HOMES LTD</b> THE REAR BARN, THE MANOR FARM, 124 MANOR ROAD NORTH THAMES DITTON, SURREY, KT7 0BH					




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# Agenda Item 9

Planning Committee  
31st October 2018

Agenda Item: 9  
18/01901/F

 <b>Reigate &amp; Banstead</b> BOROUGH COUNCIL Banstead   Horley   Redhill   Reigate	<b>TO:</b>	PLANNING COMMITTEE
	<b>DATE:</b>	31 <sup>st</sup> October 2018
	<b>REPORT OF:</b>	HEAD OF PLACES & PLANNING
	<b>AUTHOR:</b>	Matthew Sheahan
	<b>TELEPHONE:</b>	01737 276514
	<b>EMAIL:</b>	Matthew.sheahan@reigate-banstead.gov.uk
<b>AGENDA ITEM:</b>	9	<b>WARD:</b> Redhill West

<b>APPLICATION NUMBER:</b>	18/01901/F	<b>VALID:</b>	05/10/2018
<b>APPLICANT:</b>	The South London Church Fund and So Southwark Diocesan Board of Finance	<b>AGENT:</b>	The Michael Blacker Partnership
<b>LOCATION:</b>	ST MATTHEWS CHURCH STATION ROAD REDHILL		
<b>DESCRIPTION:</b>	<b>Proposed replacement of existing timber shed with new timber shed</b>		
<b>All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/ referenced for detail.</b>			

**This application is referred to Planning Committee as the agent is a Councillor.**

## SUMMARY

The site is occupied by St Matthews Church, located on Station Road to the west of Redhill Town Centre. The church is a Listed Building. It is positioned at an elevated ground level from the road, with the gradient within the site rising in a northerly direction. The wider area is characterised by a mix of building styles, with older buildings such as the church and Stonecroft to the east, interspersed with modern 20th Century office buildings and retail units on the edge of Redhill Town Centre.

The proposal seeks permission for new shed to replace an existing shed associated with the Church food bank. It would be 2.1m in height with a footprint broadly equivalent to that of the existing shed. It would have a dual pitched roof and would be of a timber construction.

It is considered that the proposed shed would be acceptable in terms of its' impact on the character of the Locally Listed building. It would be sited to the rear where it would be out of view from the wider area, thus not detracting from the setting of the church, in accordance with Policy Pc10 of the Borough Local Plan. The small size of the shed would not result in harm to the amenity of any neighbouring properties.

**RECOMMENDATION(S)**

Planning permission is **GRANTED** subject to conditions.



## Consultations:

Highway Authority: *"The County Highway Authority has undertaken an assessment in terms of the likely net additional traffic generation, access arrangements and parking provision and are satisfied that the application would not have a material impact on the safety and operation of the adjoining public highway. The County Highway Authority therefore has no highway requirements"*

## Representations:

Letters were sent to neighbouring properties on 25th May 2018. A site notice was posted on 10<sup>th</sup> October 2018. No responses have been received.

### 1.0 Site and Character Appraisal

- 1.1 The site of is occupied by a prominent Locally Listed Anglican Church, built in 1867, sited to the north side of the A25 leading through Redhill town centre. The church is sited at an elevated position, with the ground level increasing relatively steeply in a northerly direction. The church carpark is sited to the west of the church, which extends to the very rear of the site. To the rear is the church hall, which is not listed, as well as a number of additional ancillary buildings located to the north east corner of the building associated with the food bank that operates from the church.
- 1.2 The surrounding area is quite varied in terms of character of buildings. Stonecroft to the east is of a similar age to the church and is also Locally Listed, which contrasts with the modern 21<sup>st</sup> Century Office opposite. There is also a considerable mix of land uses, with office and residential transitioning to retail located to the edge of Redhill Town Centre.

### 2.0 Added Value

- 2.1 Improvements secured at the pre-application stage: The local planning authority was not approached for formal advice prior to the submission of this application.
- 2.2 Improvements secured during the course of the application: Amendments were not sought as the proposal is deemed to be acceptable.
- 2.3 Further improvements could be secured: further improvements can be sought by way of suitably worded conditions.

### 3.0 Relevant Planning and Enforcement History

- |     |            |  |  |
|-----|------------|--|--|
| 3.1 | 99/18100/F | St Matthews Church Station Road<br>Redhill | Granted<br>3 <sup>rd</sup> February 2008 |
|-----|------------|--|--|

External floodlighting of Station Road elevation.

- 3.2 04/02844/F Construction of two storey link block between north aisle of church & southern flank of church hall to form entrance foyer, meeting room & ancillary accommodation. (Drwg No. 1593/E/35, 36A, 37C, 38B, 39C, 40A) Granted  
28<sup>th</sup> January 2005

#### 4.0 Proposal and Design Approach

- 4.1 This is a full application for the proposed replacement of an existing timber shed with a new timber shed. The existing shed is 2.1m high with a single pitch roof. The proposed shed would be the same height, albeit with a dual pitched roof. The existing shed has a floor area of 3.3 square metres. It would be sited to the rear of the church, next to another shed which is to be retained.
- 4.2 A design and access statement should illustrate the process that has led to the development proposal, and justify the proposal in a structured way, by demonstrating the steps taken to appraise the context of the proposed development. It expects applicants to follow a four-stage design process comprising:  
Assessment;  
Involvement;  
Evaluation; and  
Design.
- 4.3 Evidence of the applicant's design approach is set out below:

Assessment	The statement does not include an assessment of the local character of the area.
	No site features worthy of retention were identified.
Involvement	No community consultation took place.
Evaluation	The statement does not include any evidence of other development options being considered.
Design	The statement does not explain why the proposal was chosen.

## 5.0 Policy Context

### 5.1 Designation

Urban Area  
Locally Listed Building

### 5.2 Reigate and Banstead Core Strategy

CS1(Sustainable Development)  
CS4 (Valued Townscapes and Historic Environment)

### 5.3 Reigate & Banstead Borough Local Plan 2005

Community Facility	Cf2
Heritage Sites	Pc10

### 5.4 Other Material Considerations

National Planning Policy Framework  
2018

National Planning Practice Guidance

Supplementary Planning Guidance

Other

Human Rights Act 1998  
Community Infrastructure Levy  
Regulations 2010

## 6.0 Assessment

6.1 The proposal seeks permission to replace an existing timber shed with an existing timber shed.

6.2 The main issues to consider are:

- Design and Impact on the character of the area
- Impact on neighbouring residential amenity

### Design and Impact on the Character of the Area

6.3 The proposed shed would be a modest addition, sitting on a similar footprint to the existing shed. The Church is Locally Listed, therefore consideration is given to the potential impact of any development on the character and setting of the heritage asset in accordance with Policy Pc10 and Cf2 of the Borough Local Plan.



- 6.4 It is considered that the level of impact would be minimal and comparable to that of the existing shed. The Conservation Officer raises no objection given its' location to the rear of the church, where it is hidden from general view.

Impact on neighbouring residential amenity

- 6.5 The proposed shed would be sited away from neighbouring properties and would be sufficiently small to not impact on the amenity of any other properties.

**CONDITIONS**

1. The development hereby permitted shall be completed before the expiration of three years from the date of this permission.

Reason:

To comply with Section 91(1) of the Town and Country Planning Act 1990 as amended by Section 51 (1) of the Planning and Compulsory Purchase Act 2004.

2. The development hereby permitted shall be carried out in accordance with the following approved plans:

The development hereby permitted shall be carried out in accordance with the following approved plans:

<b>Plan Type</b>	<b>Reference</b>	<b>Version</b>	<b>Date Received</b>
Location Plan	SK01		10.09.2018
Combined Plan	001		10.09.2018
Site Layout Plan	SK002		10.09.2018
Location Plan	SK01		10.09.2018
Elevation Plan	01	A	21.09.2018
Site Layout Plan	SK003		21.09.2018
Elevation Plan	01	B	05.10.2018
Elevation Plan	02	A	05.10.2018

Reason:

To define the permission and ensure the development is carried out in accord with the approved plans and in accordance with National Planning Practice Guidance.

3. The materials and windows to be used in the development hereby approved shall be as specified in the application.

Reason: To ensure that the development hereby permitted is only constructed using the appropriate external facing materials or suitable alternatives in the interest of the visual amenities of the area with regard to Reigate and Banstead Borough Local Plan 2005 policy Cf2.

## INFORMATIVES

1. Your attention is drawn to the safety benefits of installing sprinkler systems as an integral part of new development. Further information is available at [www.firesprinklers.info](http://www.firesprinklers.info).
2. The applicant is encouraged to provide renewable technology within the development hereby permitted in order to reduce greenhouse gas emissions.
3. You are advised that the Council will expect the following measures to be taken during any building operations to control noise, pollution and parking:
  - (a) Work that is audible beyond the site boundary should only be carried out between 08:00hrs to 18:00hrs Monday to Friday, 08:00hrs to 13:00hrs Saturday and not at all on Sundays or any Public and/or Bank Holidays;
  - (b) The quietest available items of plant and machinery should be used on site. Where permanently sited equipment such as generators are necessary, they should be enclosed to reduce noise levels;
  - (c) Deliveries should only be received within the hours detailed in (a) above;
  - (d) Adequate steps should be taken to prevent dust-causing nuisance beyond the site boundary. Such uses include the use of hoses to damp down stockpiles of materials, which are likely to generate airborne dust, to damp down during stone/slab cutting; and the use of bowsers and wheel washes;
  - (e) There should be no burning on site;
  - (f) Only minimal security lighting should be used outside the hours stated above; and
  - (g) Building materials and machinery should not be stored on the highway and contractors' vehicles should be parked with care so as not to cause an obstruction or block visibility on the highway.

Further details of these noise and pollution measures can be obtained from the Council's Environmental Health Services Unit.

In order to meet these requirements and to promote good neighbourliness, the Council recommends that this site is registered with the Considerate Constructors Scheme - [www.ccscheme.org.uk/index.php/site-registration](http://www.ccscheme.org.uk/index.php/site-registration).

4. The Applicants attention is brought to the requirement of the Planning Enforcement notice that required the corrective works to be completed within eight months of the date the Enforcement Notice became effective. The Enforcement Notice is extant and the Council will expect, to avoid prosecution proceedings, the works to the clubhouse to be completed within 3 months of the date of this permission.

## REASON FOR PERMISSION

The development hereby permitted has been assessed against development plan policies Pc10 and Cf2 and other material considerations, including third party representations. It has been concluded that the development is in accordance with

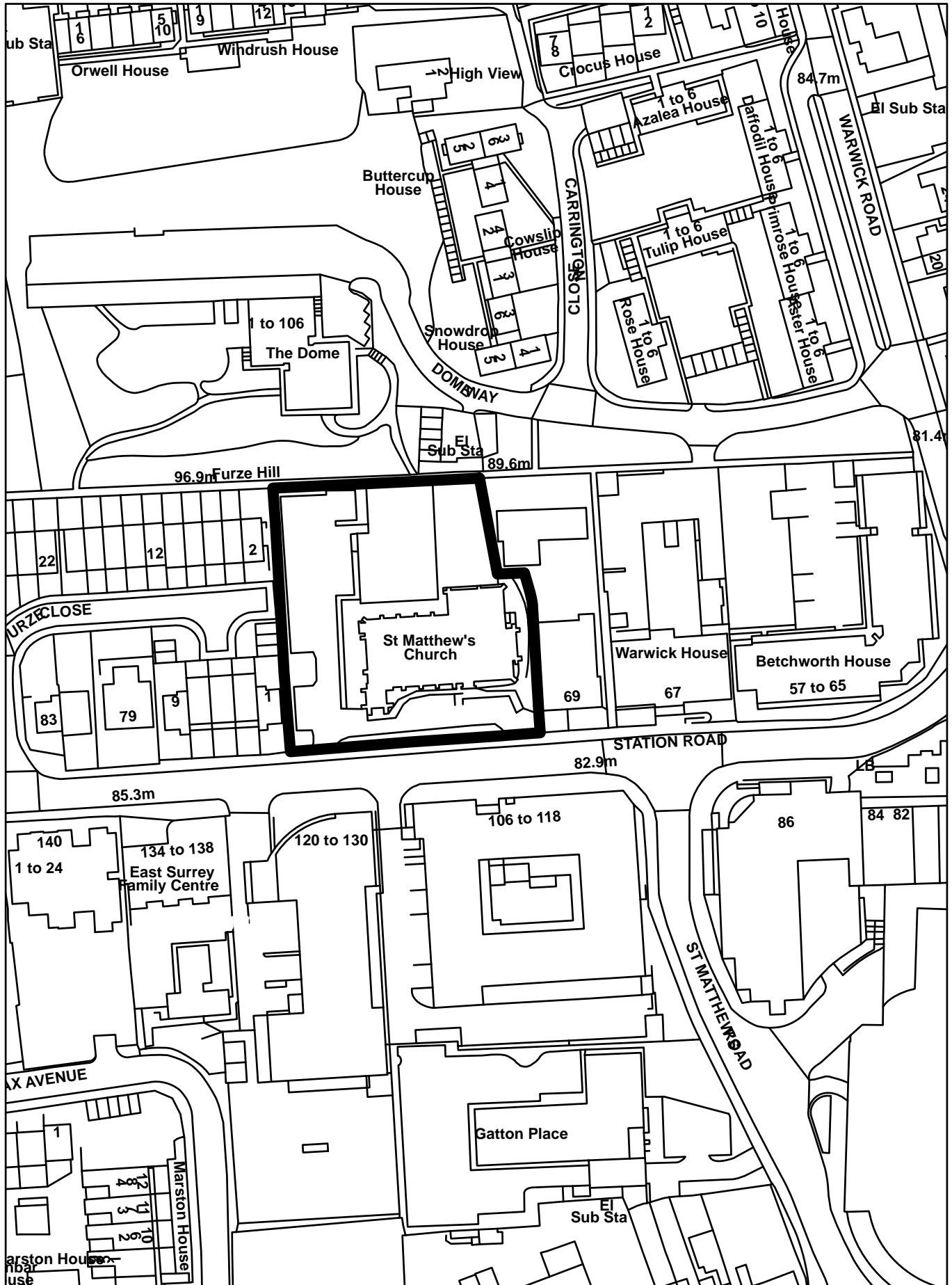
the development plan and there are no material considerations that justify refusal in the public interest.

### **Proactive and Positive Statements**

The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.



# 18/01901/F - St Matthews Church, Station Road, Redhill



**Michael Blacker** *Partnership*

Michael S. Blacker C.Eng. F.I.Struct.E.      Andrew Blacker B.Sc.(Hons) C.Eng. M.I.C.E.

**CONSULTING STRUCTURAL & CIVIL ENGINEERS**

No1 MARK STREET, REIGATE SURREY RH2 0BL

E-MAIL engs@blacker.co.uk

TELEPHONE 01737 244886

FACSIMILE 01737 224556

PROJECT

**St MATTHEWS CHURCH**  
**STATION ROAD**  
**REDHILL, RH1 1DL**

Job No.

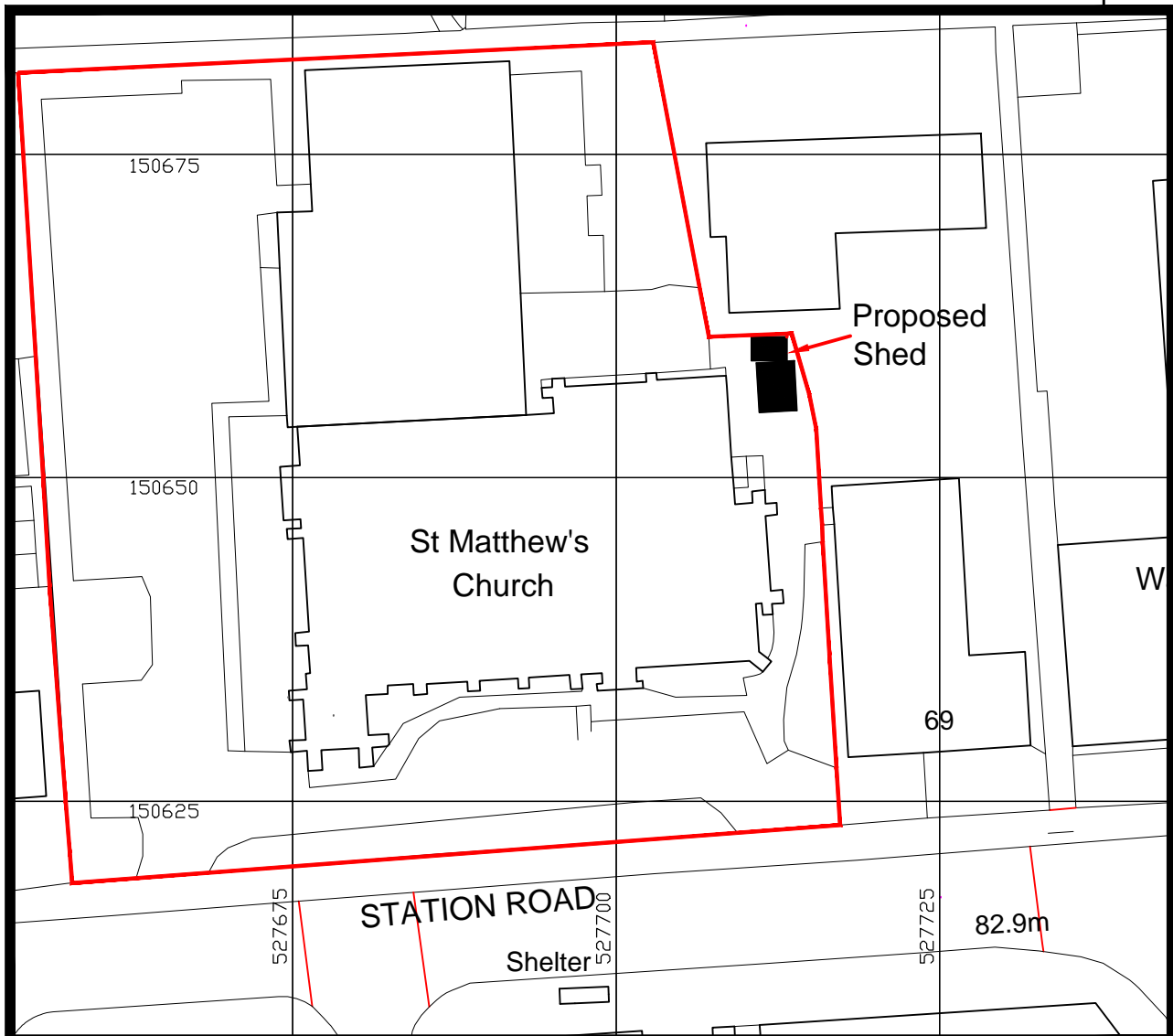
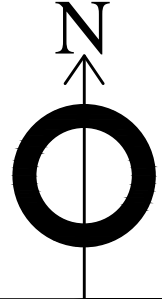
4490

Sht. No.

SK003

Date

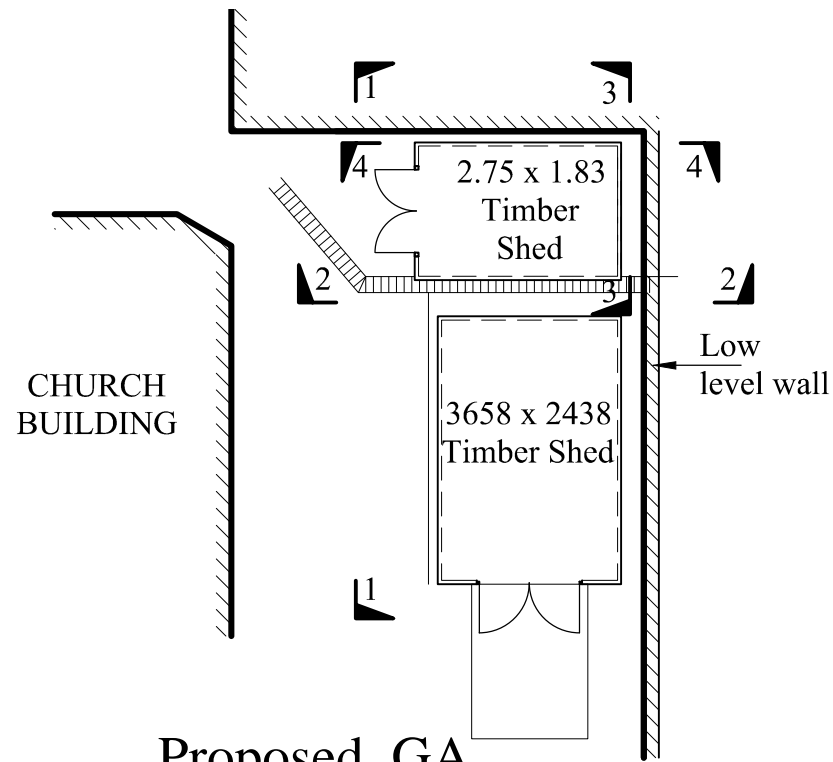
SEPT 2018



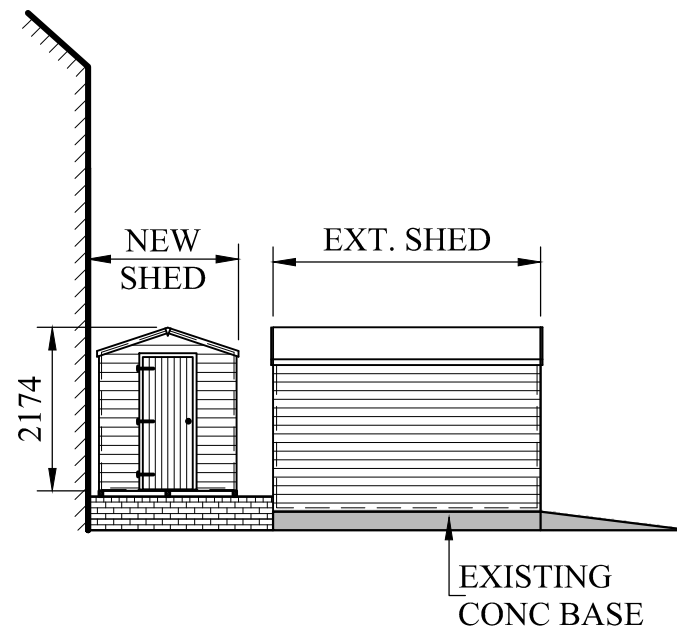
**PROPOSED SITE PLAN**



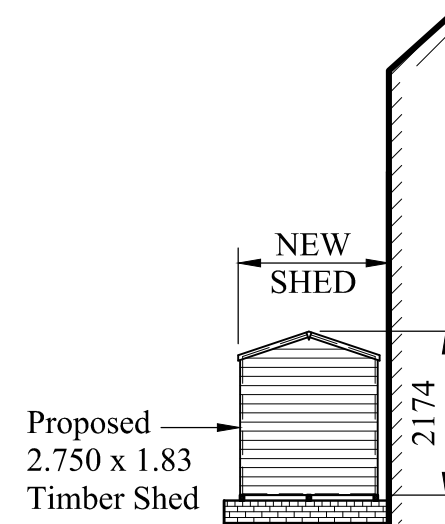
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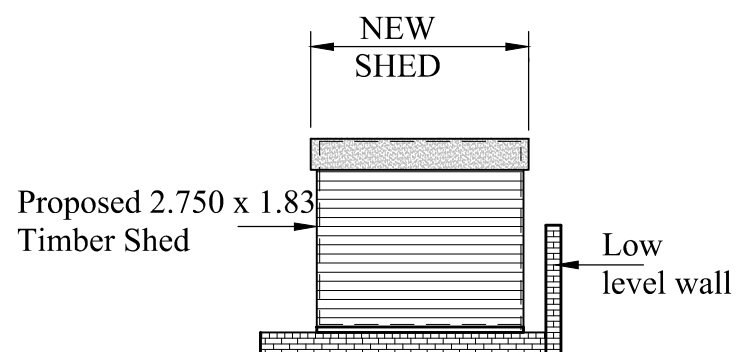
Proposed GA



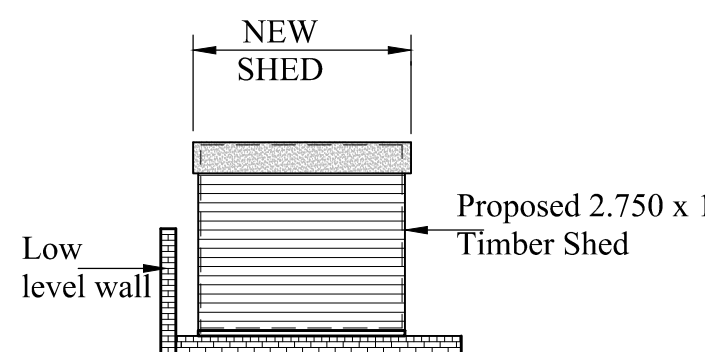
Front Elevation on New Shed  
Elevation 1-1



Rear Elevation on New Shed  
Elevation 3-3



Side Elevation on New Shed  
Elevation 2-2



Side Elevation on New Shed  
Elevation 4-4

**General Notes.**

1. All concrete to have a minimum cube crushing strength of:  
Mass Concrete = 25 N/mm<sup>2</sup>. at 28 days.  
Reinforced Concrete = 35 N/mm<sup>2</sup>. at 28 days.  
Nominal Aggregate size is to be 20mm.
2. All dimensions to be checked on site by the Contractor prior to construction and the Engineer to be informed of any discrepancies.
3. All new steelwork is to comply with B.S.449, 1969 and later amendments, or B.S.5950 1985 and later amendments as directed.
4. All new timber is to comply with B.S.5268, 1985, Grade SC4 and be treated.
5. All dimensions are in millimetres unless otherwise stated.
6. Fire casing to steelwork is to be two layers of 12.5mm Gypsum fireline board with joints taped & staggered. Finished with skim coat of gypsum plaster on Gypsum steel encasement system to achieve 1 hour fire resistance.
7. All welds are to be continuous 6mm fillet welds unless otherwise stated.
8. This drawing is to be read in conjunction with all relevant Architects and other specialists drawings.
9. All work to be carried out to the approval of the local Authority District Surveyor or Building Inspector.

149

REV	DATE	DESCRIPTION
A	05/10/18	SHED DIMENSIONS CONVERTED TO METRIC
Drawing		
PROPOSED NEW 2750 x 1830 SHED GA's & ELEVATIONS		
Project		
ST MATTHEWS CHURCH STATION RD, REDHILL		
Client		
Architect		
<b>Michael Blacker Partnership</b> <small>Michael S. Blacker CEng, FStructE, Andrew Blacker BSc(Hons) CEng, MICE</small> CONSULTING STRUCTURAL & CIVIL ENGINEERS <small>No 1 MARK STREET, REIGATE, SURREY RH2 0BL</small> <small>E-MAIL engs@blacker.co.uk</small> <small>TELEPHONE 01737 244886 FACSIMILE 01737 224556</small>		
<small>THE COPYRIGHT IN THIS DRAWING IS THE PROPERTY OF THE MICHAEL BLACKER PARTNERSHIP AND NEITHER THE DRAWING NOR ANY PART OF IT, NOR ANY INFORMATION CONTAINED THEREIN, SHALL BE REPRODUCED OR DISCLOSED TO ANY THIRD PARTY WITHOUT PRIOR CONSENT OF THE MICHAEL BLACKER PARTNERSHIP.</small>		
Scale	1:100 @ A3	Checked -
Date	Aug 2018	Rev A
Job No.	4490	Dwg. No. 02
Drawn	GG	Checked -




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# Agenda Item 10

Planning Committee  
31<sup>st</sup> October 2018

Agenda Item: 10  
18/01818/F

 <b>Reigate &amp; Banstead</b> BOROUGH COUNCIL Banstead   Horley   Redhill   Reigate	<b>TO:</b>	PLANNING COMMITTEE
	<b>DATE:</b>	31 <sup>st</sup> October 2018
	<b>REPORT OF:</b>	HEAD OF PLACES & PLANNING
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<b>AGENDA ITEM:</b>	10	<b>WARD:</b> Reigate Central

<b>APPLICATION NUMBER:</b>	18/01818/F	<b>VALID:</b>	4 <sup>th</sup> September 2018
<b>APPLICANT:</b>	Heddmara Ltd	<b>AGENT:</b>	Burgess Mean Architects
<b>LOCATION:</b>	<b>THE CROFT RESIDENTIAL HOME, BUCKLAND ROAD, REIGATE</b>		
<b>DESCRIPTION:</b>	The proposed demolition of a former nursing home building and daycare outbuilding, and the erection of a single replacement building, comprising 8no. Apartments (1no. 1Bed; 3no. 2Bed and 4no.3Bed units) with 16no. Surface car parking spaces, cycle and refuse storage enclosure, with associated hard + soft landscaping enhancements. (Building reduced in size with a gross internal area of 997.03 sqm).		
All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail.			

## SUMMARY

This is a full application for the demolition of the existing nursing home building and day care outbuilding and the erection of a replacement building, which would accommodate 8 apartments, car parking spaces and cycle and refuse storage.

The application site is located within the Metropolitan Green Belt and is within the Flanchford Road and Colley Lane Conservation Area. It is also adjacent to a locally listed building, a Historic Park and Garden, Area of Great Landscape Value and Site of Special Scientific Interest.

This proposal follows a previous application to redevelop the site for 8 dwellings (15/02920/F) which was approved earlier this year following completion of the legal agreement to secure affordable housing contributions. This application differs in that the building now proposed is slightly smaller than that previously approved and now falls under the Government's 1,000sqm "small sites" threshold. On this basis, the current application would no longer be required to contribute towards affordable housing.

The site is wholly within the Metropolitan Green Belt where the construction of new buildings is generally regarded as inappropriate. However, as a previously developed site, redevelopment can be permissible where it would not have a greater impact on the openness of the Green Belt and the purpose of including land within it than the existing

development: it was on this basis that the previous application (15/02920/F) was justified. This previous permission is a significant material consideration. The current proposal is, as above, for a building of slightly lesser floorspace but otherwise comparable in all other respects to that previously approved. Given there has been no material change in site circumstances or Green Belt policy since that approval, it is similarly concluded in this case that the redevelopment would not be inappropriate.

The building would be of traditional appearance, practically identical to that previously approved. Overall, it is considered to be well-designed, in keeping with local vernacular and would preserve the character of the Conservation Area and the setting of nearby locally listed buildings.

The siting of the building remains as per the approved scheme and is considered to achieve adequate separation distances between the proposed building and neighbouring properties such that no adverse harm is considered to occur to the amenity of these properties. A similar conclusion was reached in the previous case.

### **RECOMMENDATION(S)**

Planning permission is **GRANTED** subject to conditions.



## Consultations:

Highway Authority: The County Highway Authority has undertaken an assessment in terms of the likely net additional traffic generation, access arrangements and parking provision and are satisfied that the application would not have a material impact on the safety and operation of the adjoining public highway. The County Highway Authority therefore has no highway requirements subject to conditions.

Conservation Officer: No objection subject to conditions

Tree Officer: No objection subject to conditions

## Representations:

Letters were sent to neighbouring properties on 6<sup>th</sup> September 2018, a site notice was posted on 26<sup>th</sup> September 2018 and the application was advertised in the local press on 20<sup>th</sup> September 2018.

Two responses were received: one objected to the proposals and the other raised no objection but highlighted the need to consider inconvenience during construction:

Issue	Response
Inconvenience during construction	See paragraphs 6.13, 6.18 and condition 8
Harm to Conservation Area	See paragraphs 6.7-6.10 and conditions 4, 6, 7, 11 and 12
Hazard to highway safety	See paragraphs 6.15 to 6.17
Inadequate parking	See paragraphs 6.15 to 6.17
Out of character with the surrounding area	See paragraphs 6.7-6.10 and conditions 4, 6, 7, 11 and 12

## 1.0 Site and Character Appraisal

- 1.1 The application site consists of a large three storey building (second floor being in roof) together with a detached single storey coach house style outbuilding which was last in use as a residential care home but is now redundant and has been vacant for a number of years. The buildings are set within a very large, well landscaped plot and positioned well back from the highway frontage. The site situated within the Metropolitan Green Belt and the Flanchford Road and Colley Lane Conservation Area. Whilst the existing building on the site is Edwardian, it was originally part of the parkland of the locally listed mansion to the west called Colley Manor in the 19th Century.
- 1.2 The site is on the edge of Reigate, outside of the urban area boundary and the character reflects this. The immediate surroundings on this part of Buckland Road are characterised by low density residential development either large mansion buildings in substantial plots or detached estate housing also in generous plots. Further north, the character gives way to open countryside and on the southern side

of Buckland Road is Reigate Heath, an area of heathland which is designated as a Site of Special Scientific Interest and within the Area of Great Landscape Value.

1.3 The site extends to an area of 0.92ha.

## 2.0 Added Value

2.1 Improvements secured at the pre-application stage: Pre-application advice was not sought in relation to the current scheme.

2.2 Improvements secured during the course of the application: Minor amendments to some of the architectural details and fenestration now shown have been secured.

2.3 Further improvements could be secured: Conditions have been recommended regarding materials and landscaping to ensure a high quality development befitting the location. A condition securing compliance with the submitted Construction Transport Management Plan is also recommended.

## 3.0 Relevant Planning and Enforcement History

3.1 There is an extensive planning history on the site. The most recent and pertinent applications are shown below:

3.2	14/01351/F	Proposed demolition of a former nursing home building and day care outbuilding, and the erection of a single replacement building comprising of 14 No. residential apartments (4x1bed, 9x2bed and 1x3 bed units) with underground parking for 14 No. vehicles, cycle and refuse storage with associated hard and soft landscaping enhancements.	Withdrawn by applicant 06 October 2014
	15/00370/F	The proposed demolition of a former nursing home building and daycare outbuilding, and the erection of a single replacement building comprising of 12 No. apartments (3no.x1bed, 6no.x2bed and 3no. x3 bed units) with 14 No.surface car parking spaces, cycle and refuse storage enclosure with associated hard and soft landscaping enhancements.	Refused 10 July 2015
	15/02920/F	Amended proposal: the proposed demolition of a former nursing home building and daycare outbuilding, and the erection of a single replacement building, comprising 8 no. Apartments with 12 no. surface car parking spaces, cycle and refuse storage enclosure, with associated hard and soft landscaping enhancements.	Approved with conditions 8 June 2018

3.3 Note: application 15/02920/F was approved with conditions following completion of a legal agreement to secure affordable housing contributions. The replacement building (which had a floorspace of 1,129sqm) was not considered to be inappropriate development within the Green Belt.

#### 4.0 Proposal and Design Approach

4.1 This is a full application for the erection of a block of 8 apartments (1no. 1Bed; 3no. 2Bed and 4no.3Bed units) following demolition of the existing nursing home and ancillary building on site.

4.2 The grounds would be laid out to provide a total of 16 surface car parking spaces to the eastern side of the building - where there is already an existing area of hardstanding - alongside cycle and refuse storage.

4.3 The proposed building would be located slightly further forward than the existing building. Like the existing building it would be two storeys in scale with an additional floor of accommodation set within the roof, which would be served by dormer windows. The proposed building would have a traditional design with hipped roofs and gable projections.

4.4 The application differs from the scheme approved under 15/02920/F in that it slightly reduces the floorspace of the building to under 1,000sqm and reconfigures the internal layout to avoid 'superfluous' communal areas.

4.5 A design and access statement should illustrate the process that has led to the development proposal, and justify the proposal in a structured way, by demonstrating the steps taken to appraise the context of the proposed development. It expects applicants to follow a four-stage design process comprising:  
Assessment;  
Involvement;  
Evaluation; and  
Design.

4.6 Evidence of the applicant's design approach is set out below:

Assessment	The site lies on the northern side of Buckland Road (A25), a busy distributor road and faces Reigate Heath within an Area of Great Landscape Value. The surrounding area is characterised by a mix of private residential streets with a variety of architectural house styles. The site is surrounded by a large number of detached properties in large landscaped plots, predominantly two/three storeys in height. Buildings are in the typical Surrey vernacular style. To the eastern side of the site sits the Locally Listed building and garden at Colley House which is screened by dense planting.
	The site contains many mature trees and established



	shrub/hedgerow planting at the perimeter boundaries and within the body of the plot.
Involvement	No evidence of community consultation is identified or demonstrated within the submission documents.
Evaluation	The Statement explains how the scheme has evolved compared to the approved development, notably a slight reduction in building footprint and floorspace following a value engineering exercise on common/communal parts.
Design	The Statement sets out that the primary design consideration was a sympathetic treatment reflecting the Surrey vernacular and respecting the siting, bulk and building outline of the existing nursing homes whilst retaining established vegetation. As above, the current proposals represent a value engineering exercise on the communal areas of the approved scheme which allows for a reduction in the overall floorspace.

4.5 Further details of the development are as follows:

Site area	0.92ha
Existing use	Nursing home
Proposed use	Residential apartments (8 units)
Proposed parking spaces	16
Parking standard	14 (maximum)
Affordable housing contribution	Nil (see discussion below)
Proposed site density	9 dwellings per hectare

**5.0 Policy Context**

5.1 Designation

Metropolitan Green Belt  
Flanchford Road and Colley Lane Conservation Area  
Adjacent to Locally Listed Building  
Adjacent to Historic Park and Garden  
Adjacent to AGLV  
Adjacent to Reigate Heath SSSI

## 5.2 Reigate and Banstead Core Strategy 2014

CS1 (Sustainable Development)  
CS2 (Valued Landscapes and Natural Environment),  
CS3 (Green Belt)  
CS4 (Valued Townscapes and Historic Environment)  
CS10 (Sustainable Development)  
CS11 (Sustainable Construction),  
CS12 (Infrastructure Delivery)  
CS13 (Housing delivery)  
CS14 (Housing Needs)  
CS15 (Affordable Housing)  
CS17 (Travel options and accessibility)

## 5.3 Reigate & Banstead Borough Local Plan 2005

Landscape & Nature Conservation	Pc4
Conservation Areas	Pc10, Pc12, Pc13
Metropolitan Green Belt	Co1
Housing	Ho9
Movement	Mo5, Mo7, Mo13

## 5.4 Other Material Considerations

National Planning Policy Framework	
National Planning Practice Guidance	
Supplementary Planning Guidance	Local Distinctiveness Design Guide Householder Extensions and Alterations Affordable Housing SPD Developer Contributions SPD
Other	Human Rights Act 1998 Community Infrastructure Levy Regulations 2010 Planning (Listed Buildings and Conservation Areas) Act 1990 Conservation of Habitats and Species Regulations 2017 Public Sector Equality Duty

## 6.0 **Assessment**

6.1 The site is within the Metropolitan Green Belt and Flanchford Road/Colley Lane Conservation Area. The application follows a recent approval (15/02920/F) for the erection of a very similar building also containing 8 flats which is a material consideration in this case.

- 6.2 The main issues to consider are:
- development within the Metropolitan Green Belt
  - design and effect on the character of the area, including heritage assets
  - effects on the amenity of neighbouring properties
  - access, parking and highways
  - affordable housing and CIL
  - other issues

Development within the Metropolitan Green Belt

- 6.2 The application site is within the Metropolitan Green Belt where the construction of new buildings is generally regarded as inappropriate. The Framework (at paragraph 145) does however incorporate a number of exceptions to this including limited infilling or the partial or complete redevelopment of previously developed sites, which would not have a greater impact on the openness of the green belt than the existing.
- 6.3 As the application site comprises previously developed land, the principle of its redevelopment is acceptable. However, the proposed building must not have a greater impact on the openness of the Green Belt than the existing.
- 6.4 The proposal would see the demolition of the existing residential care home and a day care building and their replacement with a single block of 8 apartments. The residential care home is a two and a half storey building and the day care building is single storey. The combined existing gross internal area of the buildings is 884sqm compared to 997sqm now proposed, approximately a 12% increase. However, the proposals would reduce the overall footprint of built form and, by consolidating the floorspace in a single block, would reduce the spread of buildings across the site, allowing the parts of the site currently occupied by the footprint of The Croft Centre (separate outbuilding) to be opened up, increasing the sense and perception of space and openness around the building. Furthermore, the proposed building would have a very similar overall ridge height to the existing and thus would not be more visually prominent in the Green Belt. Taking these factors into account, it is concluded that the proposed development of this previously developed site would not give rise to a greater impact on openness than the current situation.
- 6.5 In coming to this view, the previous consent (15/02920/F) is also a significant material consideration. In that case, a building of slightly more floorspace (1,041sqm) but otherwise identical height, siting, design and number of apartments was concluded to be acceptable and not give rise to greater impact on openness. To reach a different view in this case on a smaller building, given there has been no material change in site circumstances or the overall policy position, would be inconsistent.
- 6.6 It is therefore concluded that the proposals would not constitute inappropriate development in the Metropolitan Green Belt and therefore would comply with the thrust of policy Co1 of the Local Plan, CS3 of the Core Strategy and the relevant provisions of the Framework.



Design and effect on the character of the area, including heritage assets

- 6.7 The application site is located within the Flanchford Road/Colley Lane Conservation Area and it is essential that any redevelopment must preserve or enhance the character and setting of the Conservation Area.
- 6.8 The proposed building would be of practically identical form and appearance to the scheme approved under 15/02920/F – that is, a building of traditional design reflecting the local Surrey vernacular. It would, as before, be a two and a half storey building with well-designed dormers serving the roof accommodation which would not be unduly obtrusive. The detailing, articulation (including gabled jettied projections, chimneys, etc.) and materials would achieve a good quality development befitting of the Conservation Area and the character of the area more generally. The Conservation Officer has confirmed no objection to the development subject to conditions.
- 6.9 The layout of the scheme would be identical to the approved scheme, providing 16 spaces in a surface car parking area to the eastern side of the building in an area already given over to landscaping. The proposals are shown to retain much of the existing established tree planting and mature hedging and shrubbery. This, supplemented with some new landscaping, would retain the landscape dominated, parkland character of the site. A landscaping proposal has been submitted with the application and is considered acceptable. A condition requiring the existing frontage trees, hedging and shrubbery to be retained and maintained is recommended in order to preserve the rustic backdrop to the adjoining heath.
- 6.10 It is therefore concluded that the design of the building and layout of the scheme would complement the character of the area, including the Conservation Area and setting of the adjoining locally listed building. It would therefore meet with the requirements of policies Ho9, Pc10, Pc12 and Pc13 of the Local Plan, CS4 and CS10 of the Core Strategy and the provisions of the Framework regarding well-designed places.

Effects on the amenity of neighbouring properties

- 6.11 The proposed development has been considered with regards to its impact on the amenity of neighbouring properties. The siting, scale and design of the building remains as per the approved scheme (15/02920/F) and in that case it was concluded that there would not be an adverse impact on neighbour amenity.
- 6.12 The circumstances in this case are considered to be the same as before. Given the very generously sized plot, the residual separation distances between the proposed building and the boundaries with neighbouring properties are significant (c.11m min to the west and c.14m to the east). As a result of this no harmful overlooking, overshadowing or overbearing impacts are considered to occur as a result of the proposed development. There is significant and mature landscaping intervening between the proposed building and neighbours which assists in providing screening (although it is not relied up in reaching a conclusion that the scheme is acceptable).

The proposal would not result in significant increases in noise and disturbance above what would have previously existed on the site.

- 6.13 Concern has been raised from residents regarding inconvenience that may occur during the construction process if the application were to be granted. Whilst this is acknowledged, such impacts would be temporary and would not constitute a sustainable reason for refusal. Other legislative regimes, including statutory nuisance legislation, exist to protect neighbours and surrounding residents should significant unacceptable events and disturbance occur. The applicant has supplied an acceptable Construction Transport Management Plan which gives details of how the construction process will be managed, including hours of working; a condition requiring compliance with this is recommended.
- 6.14 While giving rise to a degree of change in the relationship between buildings, it is therefore concluded that the proposed scheme would not adversely affect the amenity of neighbouring properties, and thus complies with policy Ho9 in this regard.

#### Access, parking and highways

- 6.15 The proposal would utilise the existing site access onto Buckland Road (A25) with a reconfigured driveway and parking area within the site. Space for a total of 16 vehicles to be parked is shown on the plans. The access arrangements would be identical to the approved scheme and the provision of effectively 2 parking spaces per unit would exceed the standards advised in the 2005 Borough Local Plan (which is considered acceptable given the location of the site).
- 6.16 Compared to the lawful care home use, the number of movements associated with the 8 apartments is unlikely to be materially different so as to give rise to unacceptable impacts in terms of intensification of the access or congestion.
- 6.17 The application has been reviewed and assessed by the County Highway Authority which has raised no objection in terms of the likely net additional traffic generation, access arrangements and parking provision. A number of conditions are however recommended.
- 6.18 As the application is accessed from the A25, a busy main route into Reigate, it is important that the construction process, and in particular vehicle movements associated with it, are appropriately managed to avoid unacceptable disruption to the operation or safety of the A25. The applicant has provided a Construction Transport Management Plan (CTMP) within the application which includes details of timing and routing of construction vehicles to the site in order to avoid conflicts with peak times and minimise the need to go through Reigate Town Centre. Furthermore, the CTMP contains a site logistics plan which allows for space for 17 parking spaces for site operatives/contractors on site. This CTMP has been reviewed by the County Highway Authority and is considered to be adequate. A condition requiring compliance with the details and measures set out within the document is therefore recommended to avoid unacceptable highway or amenity impacts.

### Affordable housing and CIL

- 6.19 The proposal would result in the creation of additional residential units and accordingly would be liable for the Community Infrastructure Levy. The exact amount of liability would be determined and collected after the grant of planning permission; however, based on the information provided by the applicant to date, CIL liability in this case is estimated to be approximately £200,000 (before indexation and subject to any reliefs which the applicant may seek).
- 6.20 Legislation (Regulation 122 of the CIL Regulations) and national policy requires that only contributions that are directly required as a consequence of development can be secured through planning obligations. Requests of this nature must be fully justified with evidence including costed spending plans to demonstrate what the money requested would be spent on. In this case, no such site specific contributions have been requested.
- 6.21 In terms of affordable housing, the previous scheme (15/02920/F) was subject to a legal agreement to secure affordable housing contributions. This is due to the fact that, whilst the scheme was under the 10 unit threshold introduced by the Government, it exceeded the associated 1,000sqm floorspace threshold. In this case, as above, the applicant has “value engineered” the scheme to reduce superfluous communal areas which enables a reduction in the floorspace to 997sqm, therefore below the Government’s threshold which is now included
- 6.22 On this basis, and mindful of the resolution of the Planning Committee in November 2016, greater weight is therefore given in these circumstances to the national policy position in the WMS than the Council’s adopted policy. For this reason, it is not considered justified to seek contributions towards affordable housing in this case and the absence of an agreed undertaking does not therefore warrant a reason for refusal in this case.

### Other matters

- 6.23 The proposal would make a positive contribution towards meeting the housing needs and requirements of the borough, with associated social and economic benefits. This attracts a limited amount of additional weight in favour of the application. Representations received note a preference for 2-3 family homes over a flatted development; however, the principle of a flatted development on the site has already been established through 15/02920/F and is not considered to be objectionable – each case must be assessed on its own merits.
- 6.24 The site is within Flood Zone 1 according to the Environment Agency Flood Maps and is not therefore considered to be at particular risk of fluvial flooding. Given the number of units proposed, a SUDS strategy is not required by policy. Detailed issues of sewerage, etc would be addressed through Building Regulations and Building Control approvals.



- 6.25 The application was supported by a Sustainability and Energy Statement which identifies the measures and technologies to be employed in order to minimise carbon emissions from the development. A condition requiring compliance with the details in this statement is recommended in order to comply with policies CS10 and CS11 of the Core Strategy.
- 6.26 Whilst there is no local policy resisting the loss of care home accommodation, the proposal was nonetheless supported by a statement setting out the reasons for the decline and ultimate closure of the previous care home business, including increased standards and challenges with the configuration and layout of the existing building.

## CONDITIONS

1. The development hereby permitted shall be carried out in accordance with the following approved plans:

<b>Plan Type</b>	<b>Reference</b>	<b>Version</b>	<b>Date Received</b>
Location Plan	001045239		23.12.2015
Block Plan	S/01		23.12.2015
Floor Plan	S/02		23.12.2015
Floor Plan	S/03		23.12.2015
Elevation Plan	S/04		23.12.2015
Combined Plan	S/05		23.12.2015
Site Layout Plan	P/11	D	13.04.2016
Floor Plan	P/12	B	13.04.2016
Floor Plan	P/13	B	13.04.2016
Elevation Plan	P/14	B	13.04.2016
Elevation Plan	P/15	B	13.04.2016
Section Plan	P/16	B	13.04.2016
Other Plan	P/10	B	13.04.2016

Reason:

To define the permission and ensure the development is carried out in accord with the approved plans and in accordance with National Planning Practice Guidance.

Note: Should alterations or amendments be required to the approved plans, it will be necessary to apply either under Section 96A of the Town and Country Planning Act 1990 for non-material alterations or Section 73 of the Act for minor material alterations. An application must be made using the standard application forms and you should consult with us, to establish the correct type of application to be made.

2. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason:

To comply with Section 91(1) of the Town and Country Planning Act 1990 as amended by Section 51 (1) of the Planning and Compulsory Purchase Act 2004.

3. No development shall commence including demolition or any groundworks preparation until a detailed, scaled Tree Protection Plan (TPP) and the related Arboricultural Method Statement (AMS) is submitted to and approved in writing by

the Local Planning Authority (LPA). These shall include details of the specification and location of exclusion fencing, ground protection and any construction activity that may take place within the Root Protection Areas of trees (RPA) shown to scale on the TPP, including the installation of service routings. The AMS shall also include a pre commencement meeting with the LPA, supervisory regime for their implementation and monitoring with an agreed reporting process to the LPA. All works shall be carried out in strict accordance with these details when approved.

Reason:

To ensure good arboricultural practice in the interests of the maintenance of the character and appearance of the area and to comply with British Standard 5837:2012 'Trees in Relation to Design, demolition and Construction – Recommendations' and policies Pc4 and Ho9 of the Reigate and Banstead Borough Local Plan.

4. Notwithstanding the drawings, the proposed external finishing materials and details shall be carried out using the external facing materials and details specified below and there shall be no variation without the prior approval in writing of the Local Planning Authority;
- a) All tiles and tile hanging shall be of Wealden handmade sandfaced plain clay tiles and the tile hanging shall be lighter in colour than the roof tile
  - b) All external joinery shall be of painted timber with architraved bargeboards with no box ends
  - c) All dormers shall have an ogee cornice
  - d) All windows shall be of white painted timber exposed box vertically sliding sash windows with architraved surrounds whilst the ground floor sashes in the masonry shall be set back behind the reveal at one brick depth, and glazing bars shall be of traditional profile
  - e) All fascias shall be no more than two bricks depth (15cm)
  - f) This permission does not purport to grant consent for the French windows shown on the approved drawings, revised details for which shall be submitted to and approved in writing by the Local Planning Authority prior to their installation within the building
  - g) This permission does not purport to grant consent for the external entrance door and canopy/porch in the front elevation shown on the approved drawings, revised details for which shall be submitted to and approved in writing by the Local Planning Authority prior to their installation within the building
  - h) This permission does not purport to grant consent for the juliet balconies shown on the approved drawings, revised details for which shall be submitted to and approved in writing by the Local Planning Authority prior to their installation. The ground floor balcony within the front elevation of the building shall be omitted.
  - i) All rainwatergoods shall be of painted cast metal or cast metal profile.
  - j) All timber framing shall be of painted timber.
  - k) The photovoltaic or solar panels within the crown roof shall be lower than the lowest ridge point at their highest point.

Reason:

To ensure that a satisfactory external appearance is achieved of the development with regard to Reigate and Banstead Borough Local Plan 2005 policies Ho9 and Pc13.

5. The development shall be carried out in strict accordance with the site ground levels and finished floor levels specified on the approved drawings and there shall be no variation without the prior written consent of the Local Planning Authority.

Reason:

To ensure the Local Planning Authority are satisfied with the details of the proposal and its relationship with adjoining development and to safeguard the visual amenities of the locality with regard to Reigate and Banstead Borough Local Plan 2005 policy Ho9.

6. All hard and soft landscaping shall be completed in full accordance with the scheme as detailed on the approved Planting Plan 742-L-02 by Petrow Harley prior to occupation or within the first planting season following completion of the development.

Notwithstanding the approved plans, the driveway and parking areas shall be of pea shingle gravel, fixed where required.

All new tree planting shall be positioned in accordance with guidelines and advice contained in the current British Standard 5837: Trees in relation to construction.

Any trees shrubs or plants planted in accordance with this condition which are removed, die or become damaged or become diseased within five years of planting shall be replaced within the next planting season by trees, and shrubs of the same size and species.

Reason:

To ensure good arboricultural and landscape practice in the interests of the maintenance of the character and appearance of the area and the Conservation Area in order to comply with policies Pc4, Pc13 and Ho9 of the Reigate and Banstead Borough Local Plan 2005.

7. Notwithstanding the approved drawings and landscaping scheme, no pruning, removal or other works to the retained trees, hedges and shrubbery located between the front boundary fence and the footway of Buckland Road shall be carried out during the development without the prior written approval of the Local Planning Authority.

Thereafter, the aforementioned frontage planting shall be retained on an ongoing basis and any losses through death or disease shall be remedied with replacement matching planting, to current landscape standards, within 1 year to maintain this feature.

Reason:

To ensure good arboricultural and landscape practice in the interests of the maintenance of the character and appearance of the area and the Conservation Area in order to comply with policies Pc4, Pc13 and Ho9 of the Reigate and Banstead Borough Local Plan 2005.

8. The development shall be carried out in strict accordance with the Construction Transport Management Plan and associated Site Logistics Plan both by Stanton Construction.

Reason:



In order that the development should not prejudice highway safety nor cause inconvenience to other highway users to satisfy policies Mo5 and Mo7 of the Reigate and Banstead Borough Local Plan 2005 and the objectives of the NPPF

9. The development hereby approved shall not be first occupied unless and until facilities for the secure parking of a minimum of 8 bicycles and for the storage of refuse and recycling bins have been provided in accordance with a scheme to be submitted to and approved in writing by the Local Planning Authority. Such a scheme shall include details of the appearance, scale and materials of any such structures/facilities.

Reason:

To ensure that a satisfactory external appearance is achieved of the development and in order that the development should facilitate sustainable modes of transport with regard to Reigate and Banstead Borough Local Plan 2005 policies Ho9 and Pc13 and Reigate and Banstead Core Strategy policies CS4 and CS17.

10. The development hereby approved shall not be first occupied unless and until space has been laid out within the site in accordance with the approved plans for vehicles to be parked and for vehicles to turn so that they may enter and leave the site in forward gear. Thereafter the parking /turning areas shall be retained and maintained for their designated purposes.

Reason:

In order that the development should not prejudice highway safety nor cause inconvenience to other highway users to satisfy policies Mo5 and Mo7 of the Reigate and Banstead Borough Local Plan 2005 and the objectives of the NPPF.

11. The development shall not be occupied until a plan indicating the positions, design, materials and type of boundary treatment to be erected has been submitted to and approved in writing by the Local Planning Authority. All new or replacement fencing shall be of vertically boarded timber with timber posts.

The boundary treatment shall be completed before the occupation of the development hereby permitted.

Reason:

To preserve the visual amenity of the area and protect neighbouring residential amenities with regard to the Reigate and Banstead Borough Local Plan 2005 policies Ho9, Pc4 and Pc13.

12. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015 (or any order revoking and re-enacting that Order with or without modification), no a gate, fence, wall or other means of enclosure forward of the principal elevation of the building shall be erected, constructed or altered unless expressly authorised by this permission.

Reason:

To preserve the visual amenity of the area and protect neighbouring residential amenities with regard to the Reigate and Banstead Borough Local Plan 2005 policies Ho9, Pc4 and Pc13.

## INFORMATIVES

1. The development hereby approved is liable to pay the Community Infrastructure Levy (CIL). Payment of CIL is mandatory. You are advised to familiarise yourself with CIL, its implications and your responsibilities. More information about CIL is available at [www.reigate-banstead.gov.uk/cil](http://www.reigate-banstead.gov.uk/cil).
2. If you have not already done so, you should submit an Assumption of Liability Form as soon as possible to notify the Council who will be responsible for paying CIL for the development. This will ensure that the CIL Liability Notice, and any subsequent correspondence associated with CIL, is issued to the correct party. Responsibility to pay CIL will default to the landowner unless another party has assumed liability. All relevant forms can be found on the Planning Portal website at: <http://www.planningportal.co.uk/planning/applications/howtoapply/whattosubmit/cil>
3. Your attention is drawn to the safety benefits of installing sprinkler systems as an integral part of new development. Further information is available at [www.firesprinklers.info](http://www.firesprinklers.info).
4. The applicant is advised that prior to the initial occupation of any individual dwelling or communal dwelling/flat hereby permitted, appropriate bins and recycling boxes should be provided for the use of the occupants of that dwelling. Refuse storage areas and collection points should meet the standards set out in the Council's Making Space for Waste in New Developments Guidance document [http://www.reigate-banstead.gov.uk/downloads/file/2579/making\\_space\\_for\\_waste](http://www.reigate-banstead.gov.uk/downloads/file/2579/making_space_for_waste). You are advised to make contact with the Council's Refuse & Recycling department in preparing the plans for the refuse store required by the above conditions.
5. You are advised that the Council will expect the following measures to be taken during any building operations to control noise, pollution and parking:
  - (a) Work that is audible beyond the site boundary should only be carried out between 08:00hrs to 18:00hrs Monday to Friday, 08:00hrs to 13:00hrs Saturday and not at all on Sundays or any Public and/or Bank Holidays;
  - (b) The quietest available items of plant and machinery should be used on site. Where permanently sited equipment such as generators are necessary, they should be enclosed to reduce noise levels;
  - (c) Deliveries should only be received within the hours detailed in (a) above;
  - (d) Adequate steps should be taken to prevent dust-causing nuisance beyond the site boundary. Such uses include the use of hoses to damp down stockpiles of materials, which are likely to generate airborne dust, to damp down during stone/slab cutting; and the use of bowsers and wheel washes;
  - (e) There should be no burning on site;
  - (f) Only minimal security lighting should be used outside the hours stated above; and
  - (g) Building materials and machinery should not be stored on the highway and contractors' vehicles should be parked with care so as not to cause an obstruction or block visibility on the highway.

Further details of these noise and pollution measures can be obtained from the Council's Environmental Health Services Unit.

In order to meet these requirements and to promote good neighbourliness, the Council recommends that this site is registered with the Considerate Constructors Scheme - [www.ccscheme.org.uk/index.php/site-registration](http://www.ccscheme.org.uk/index.php/site-registration).

6. The use of a suitably qualified arboricultural consultant is essential to provide acceptable supervision and monitoring in respect of the arboricultural issues in respect of the above conditions. All works shall comply with the recommendations and guidelines contained within British Standard 5837.
7. The developer is reminded that it is an offence to allow materials to be carried from the site and deposited on or damage the highway from uncleaned wheels or badly loaded vehicles. The Highway Authority will seek, wherever possible, to recover any expenses incurred in clearing, cleaning or repairing highway surfaces and prosecutes persistent offenders. (Highways Act 1980 Sections 131, 148, 149).
8. The developer is reminded that it is an offence to damage verge land within the highway. Any damage caused as a result of the development, or by construction activities associated with the development, should be repaired to a standard to be agreed with the Highway Authority. The Highway Authority will seek to recover any expenses incurred in repairing highway surfaces or verges as a result of development activity and prosecute persistent offenders. (Highways Act 1980 Sections 131, 148, 149).
9. The applicant is advised that the Borough Council is the street naming and numbering authority and you will need to apply for addresses. This can be done by contacting the Address and Gazetteer Officer prior to construction commencing. You will need to complete the relevant application form and upload supporting documents such as site and floor layout plans in order that official street naming and numbering can be allocated as appropriate. If no application is received the Council has the authority to allocate an address. This also applies to replacement dwellings.

If you are building a scheme of more than 5 units please also supply a CAD file (back saved to 2010) of the development based on OS Grid References. Full details of how to apply for addresses can be found [http://www.reigate-banstead.gov.uk/info/20277/street\\_naming\\_and\\_numbering](http://www.reigate-banstead.gov.uk/info/20277/street_naming_and_numbering)

## **REASON FOR PERMISSION**

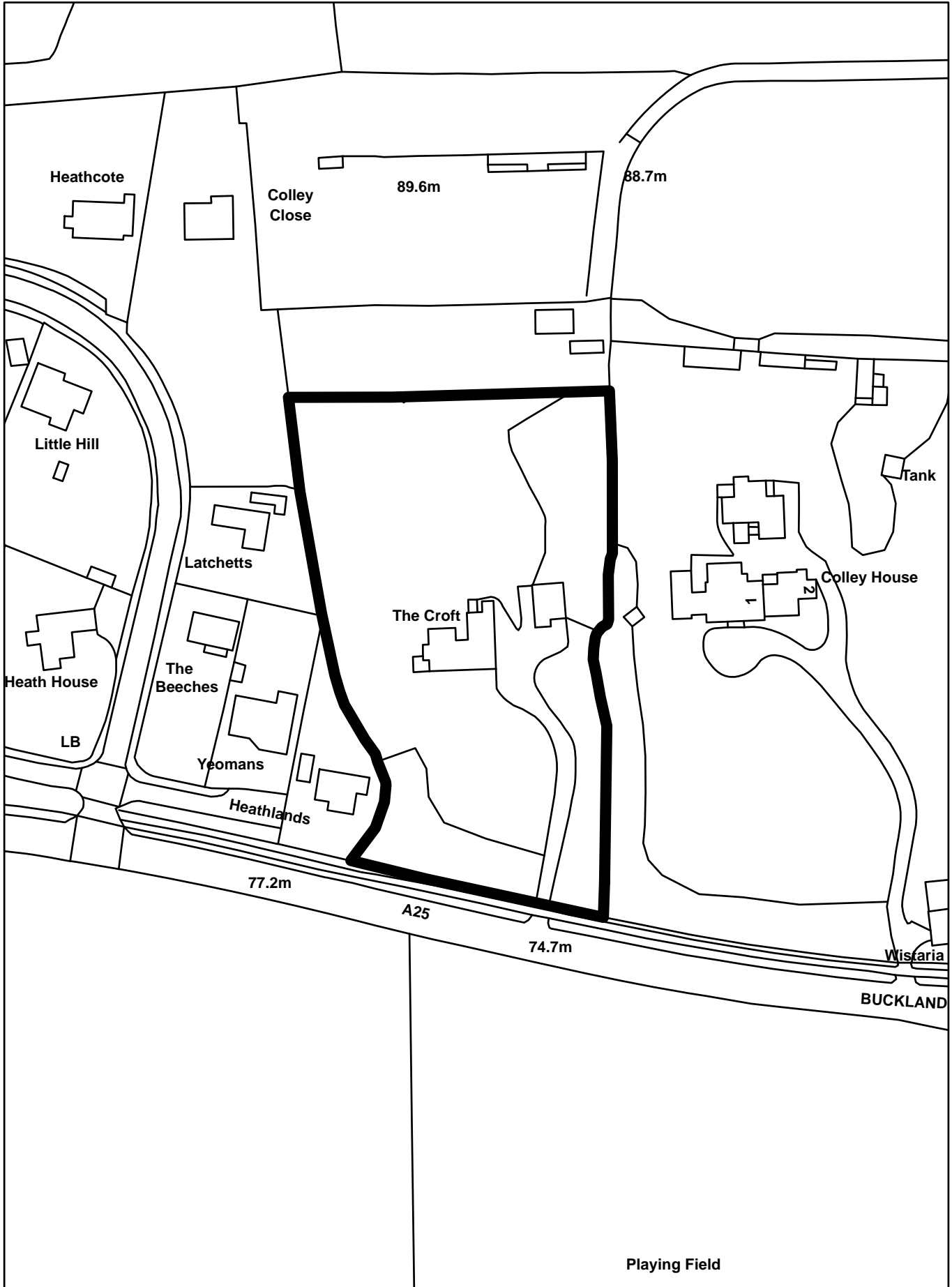
The development hereby permitted has been assessed against development plan policies Co1, Pc4, Pc10, Pc12, Pc13, Ho9, Mo5, Mo7, Mo13, CS1, CS3, CS4, CS10, CS11, CS12, CS13, CS14, CS15 and CS17 and material considerations, including third party representations. It has been concluded that the development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.

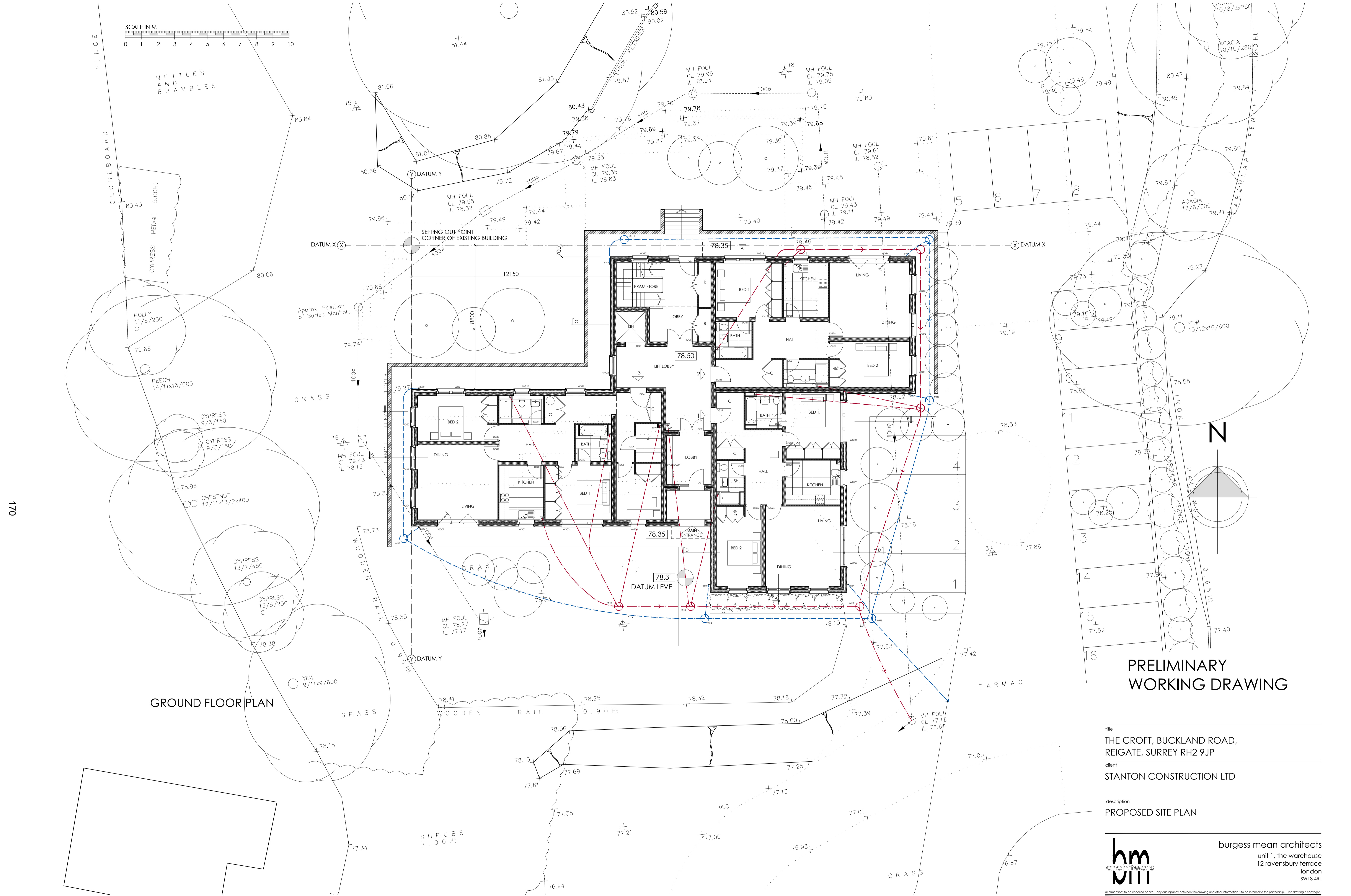
The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including



planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.

# 18/01818/F - The Croft Residential Home, Buckland Road, Reigate





GROUND FLOOR PLAN

PRELIMINARY WORKING DRAWING

title  
 THE CROFT, BUCKLAND ROAD,  
 REIGATE, SURREY RH2 9JP  
 client  
 STANTON CONSTRUCTION LTD

description  
 PROPOSED SITE PLAN

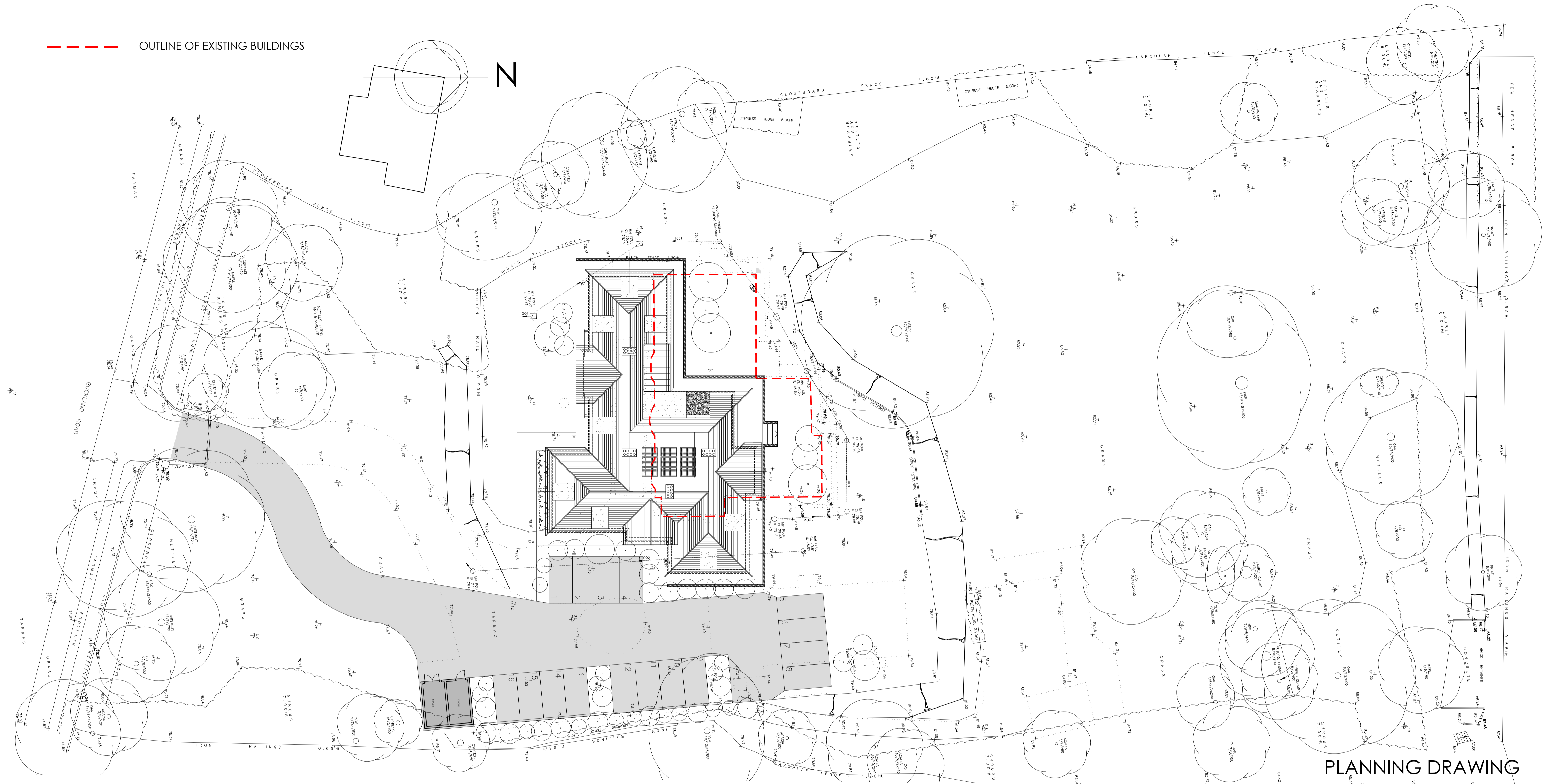
**bm** architects  
 burgess mean architects  
 unit 1, the warehouse  
 12 ravenbury terrace  
 london  
 SW18 4RL

t: 020 8944 8050 f: 020 8946 9506 e: bma@burgessmean.co.uk

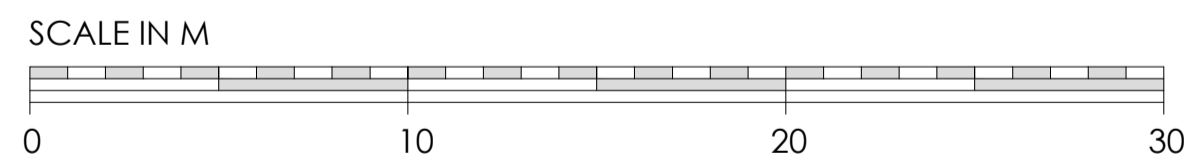
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 drawn by: MS  
 file name:  
 checked:  
 scale: 1:100  
 project: 120601  
 drawing: 01/001  
 revision: (A1)



OUTLINE OF EXISTING BUILDINGS



171



PLANNING DRAWING

Rev F: 16/08/2018: Client name changed and common parts reduced in size to achieve GIA of 997.03 sqm.  
 Rev E: 12/05/2017: Proposed development reduced to 8 N<sup>o</sup> apartments and 16 N<sup>o</sup> car parking spaces to suit clients comments.  
 Rev D: 11/04/2016: Amended to suit Conservation Officer comments.  
 Rev C: 05/04/2016: Amended to suit Planners comments (18 car parking spaces).  
 Rev B: 05/04/2016: Amended to increased car park spaces.  
 Rev A: 12/11/2015: Reduced to nine flat scheme.

title  
**THE CROFT, BUCKLAND ROAD, REIGATE, SURREY RH2 9JP**

client  
**HEDDMARA LIMITED**

description  
**RESIDENTIAL DEVELOPMENT  
 SITE PLAN AS PROPOSED**

**bm** architects  
 burgess mean architects  
 unit 1, the warehouse  
 12 ravenbury terrace  
 london  
 SW18 4RL

t: 020 8944 8050 f: 020 8946 9506 e: bma@burgessmean.co.uk

date: APRIL 2014  
 drawn by: SA  
 checked: (A1)  
 scale: 1:200  
 project: 120601  
 drawing: P/11  
 revision: F



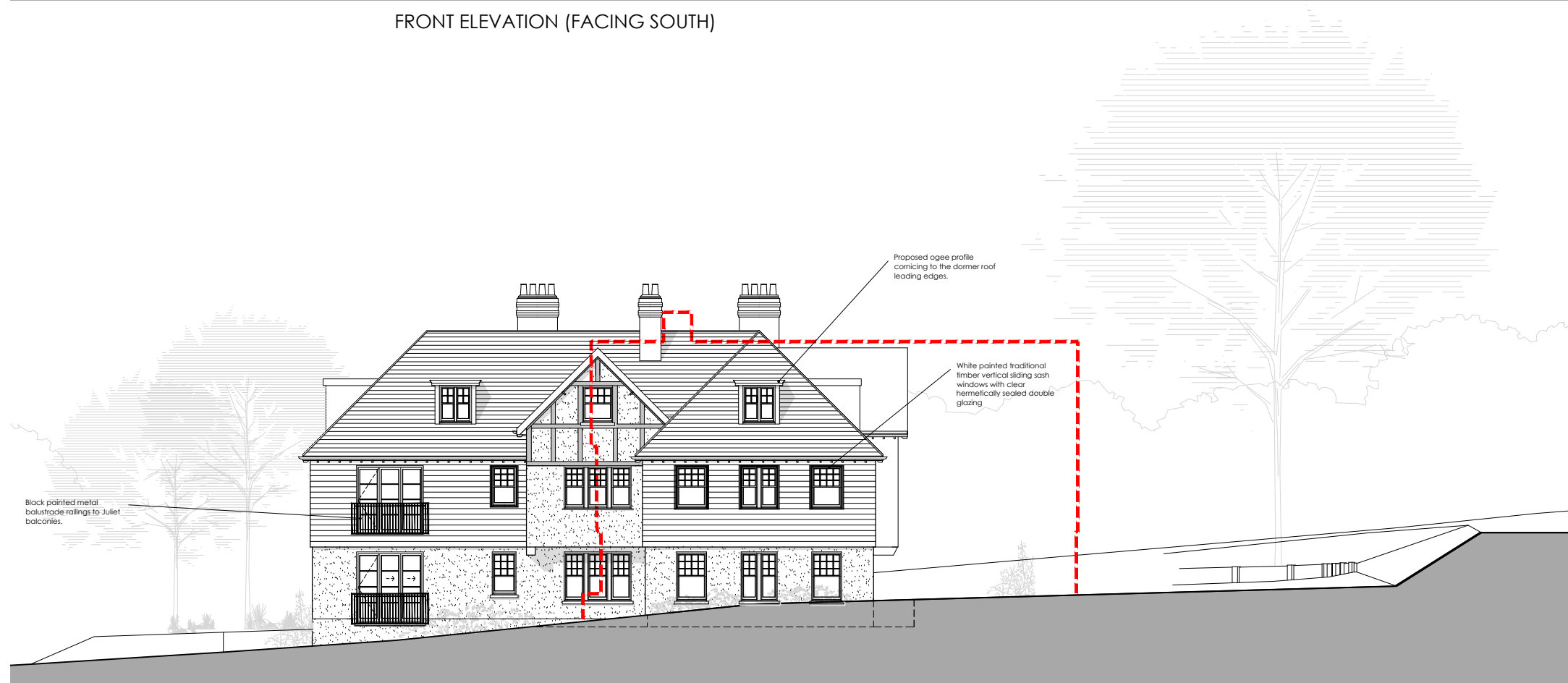






FRONT ELEVATION (FACING SOUTH)

-  VERTICAL TILE HANGING
-  PAINTED RENDER
-  KEYMER TRADITIONAL - ANTIQUE ROOF TILE
-  KEYMER TRADITIONAL - WEALDEN RED VERTICAL HANGING TILE
-  IVORY PAINTED RENDER
-  OUTLINE OF EXISTING BUILDINGS



SIDE ELEVATION (FACING EAST)

## PLANNING DRAWING

Rev E: 25/09/2018: Amended to suit planners comments.  
 Rev D: 16/08/2018: Client name changed and common parts reduced in size to achieve GIA of 997.03 sqm.  
 Rev C: 12/05/2017: Proposed development reduced to 8 N<sup>o</sup> apartments to suit clients comments.  
 Rev B: 11/04/2018: Amended to suit Conservation Officer comments.  
 Rev A: 12/11/2015: Reduced to nine flat scheme.

Title  
**THE CROFT, BUCKLAND ROAD,  
 REIGATE, SURREY RH2 9JP**  
 Client  
**HEDDMARA LIMITED**

Description  
**RESIDENTIAL DEVELOPMENT  
 FRONT + SIDE ELEVATIONS AS PROPOSED**

**bm** architects  
 burgess mean architects  
 unit 1, the warehouse  
 12 ravenbury terrace  
 london  
 SW18 4RL


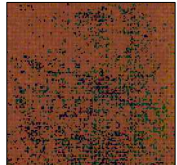
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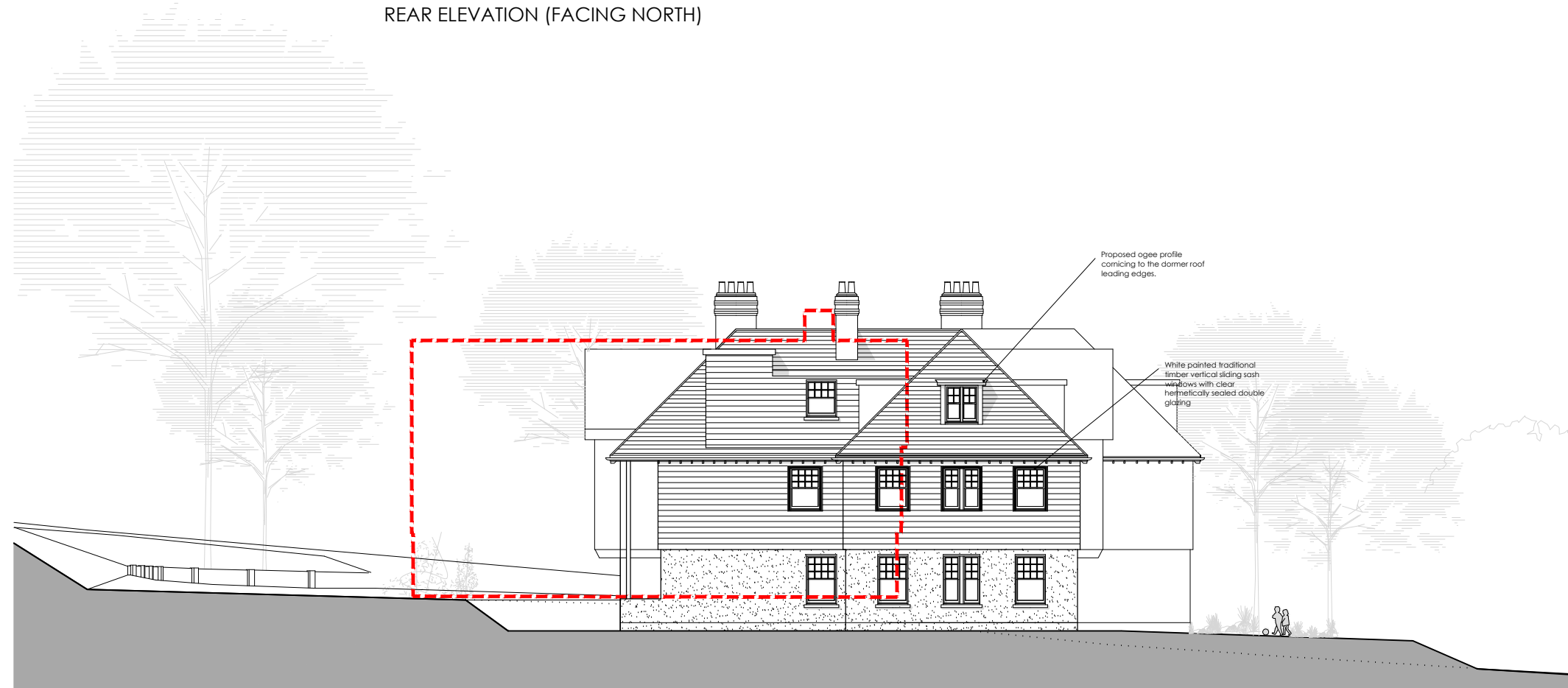
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 checked:





REAR ELEVATION (FACING NORTH)

- 
VERTICAL TILE HANGING
- 
PAINTED RENDER
- 
KEYMER TRADITIONAL - ANTIQUE ROOF TILE
- 
KEYMER TRADITIONAL - WEALDEN RED VERTICAL HANGING TILE
- 
IVORY PAINTED RENDER
- 
OUTLINE OF EXISTING BUILDING



SIDE ELEVATION (FACING WEST)



## PLANNING DRAWING

Rev E: 25/09/2018: Amended to suit planners comments.  
 Rev D: 16/08/2018: Client name changed and common parts reduced in size to achieve GIA of 997.03 sqm.  
 Rev C: 12/05/2017: Proposed development reduced to 8 N<sup>o</sup> apartments to suit clients comments.  
 Rev B: 11/04/2018: Amended to suit Conservation Officer comments.  
 Rev A: 12/11/2015: Reduced to nine flat scheme.

Title  
**THE CROFT, BUCKLAND ROAD,  
 REIGATE, SURREY RH2 9JP**  
 Client  
**HEDDMARA LIMITED**

Description  
**RESIDENTIAL DEVELOPMENT  
 REAR+ SIDE ELEVATIONS AS PROPOSED**

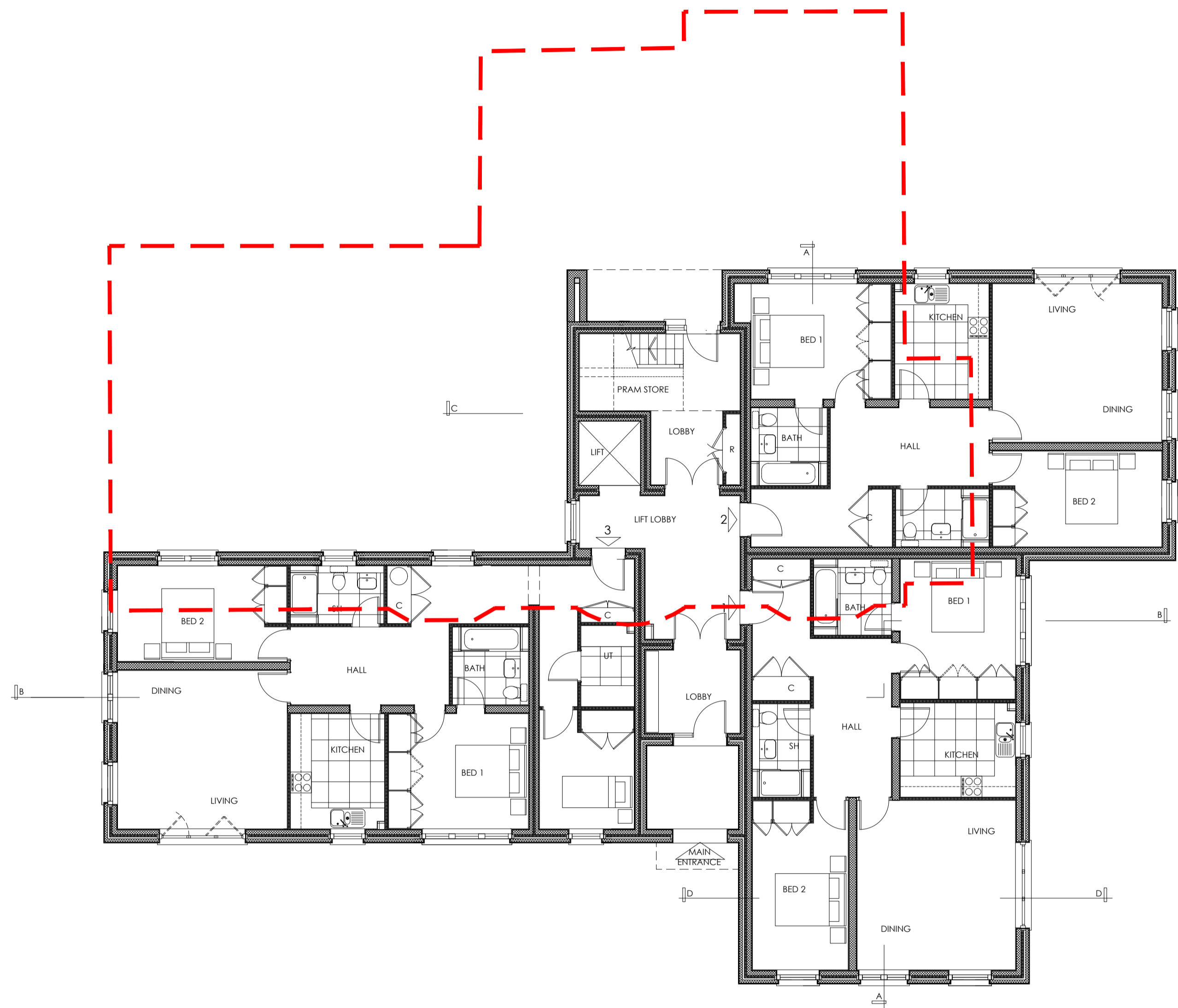


**burgess mean architects**  
 unit 1, the warehouse  
 12 ravenbury terrace  
 london  
 SW18 4RL

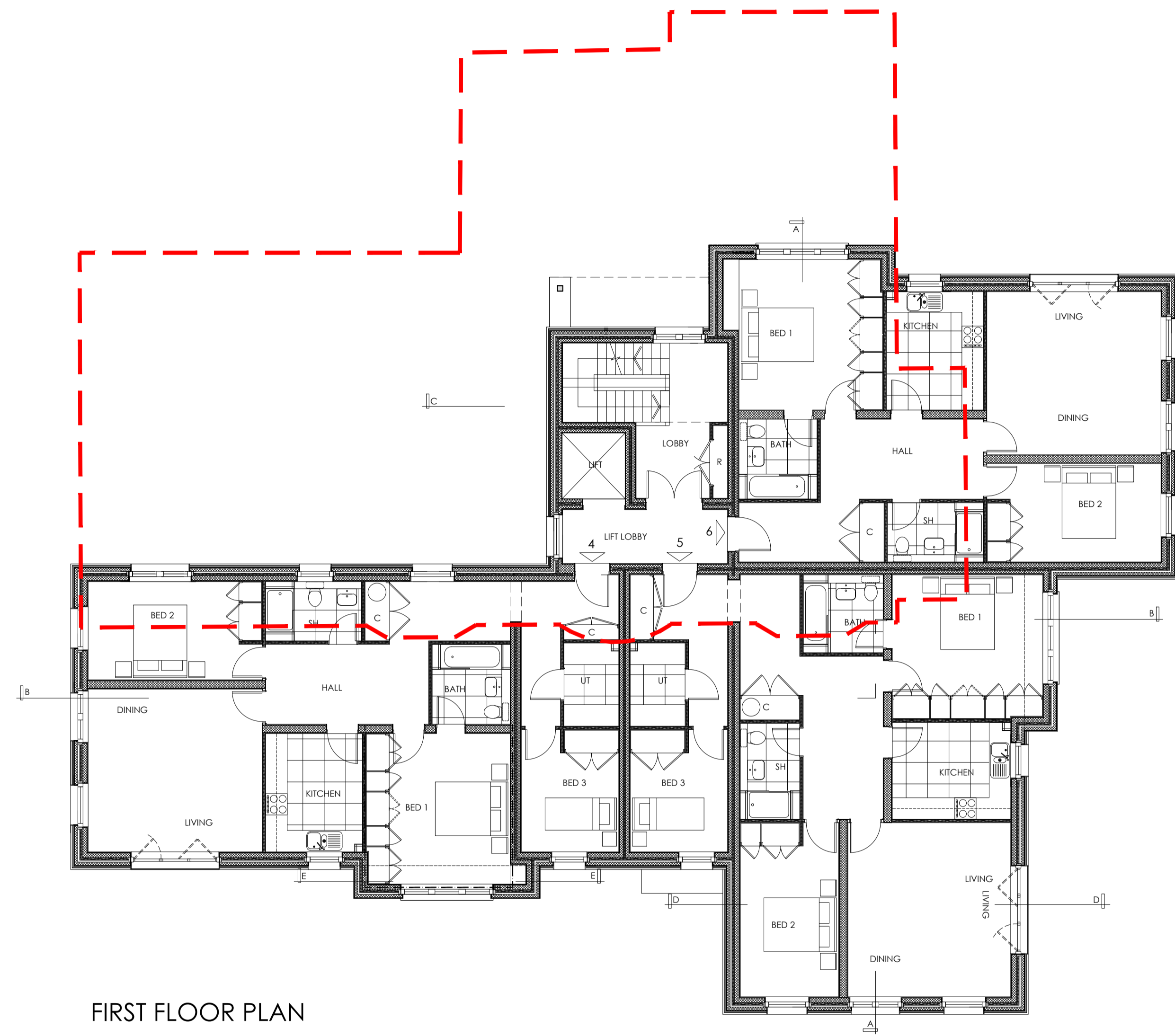
All dimensions to be checked on site. Any discrepancy between this drawing and other information is to be referred to the architect. This drawing is copyright.

t: 020 8944 8050 f: 020 8946 9506 e: bma@burgessmean.co.uk

date: APRIL 2014  
 drawn by: SA  
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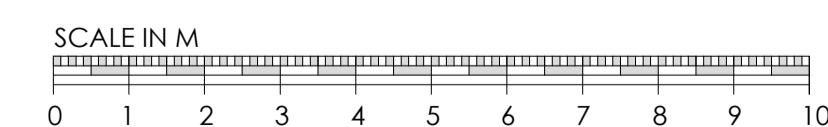
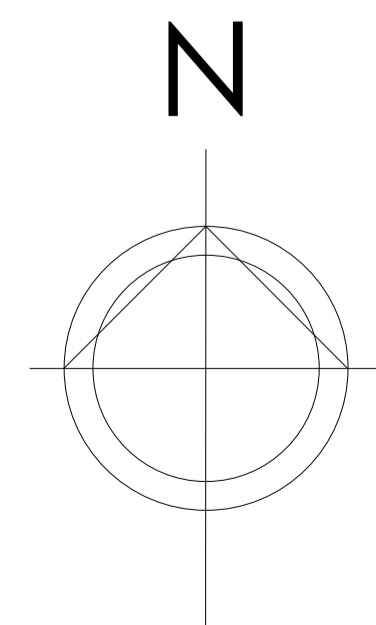


GROUND FLOOR PLAN



FIRST FLOOR PLAN

--- OUTLINE OF EXISTING BUILDING



### PLANNING DRAWING

Rev D: 16/08/2018: Client name changed and common parts reduced in size to achieve GIA of 997.03 sqm.  
 Rev C: 12/05/2017: Proposed development reduced to 8 N° apartments to suit clients comments.  
 Rev B: 11/04/2016: Amended to suit Conservation Officer comments.  
 Rev A: 12/11/2015: Reduced to nine flat scheme.

title  
**THE CROFT, BUCKLAND ROAD,  
 REIGATE, SURREY RH2 9JP**

client  
**HEDDMARA LIMITED**

description  
**RESIDENTIAL DEVELOPMENT  
 GROUND + FIRST FLOOR PLANS AS PROPOSED**



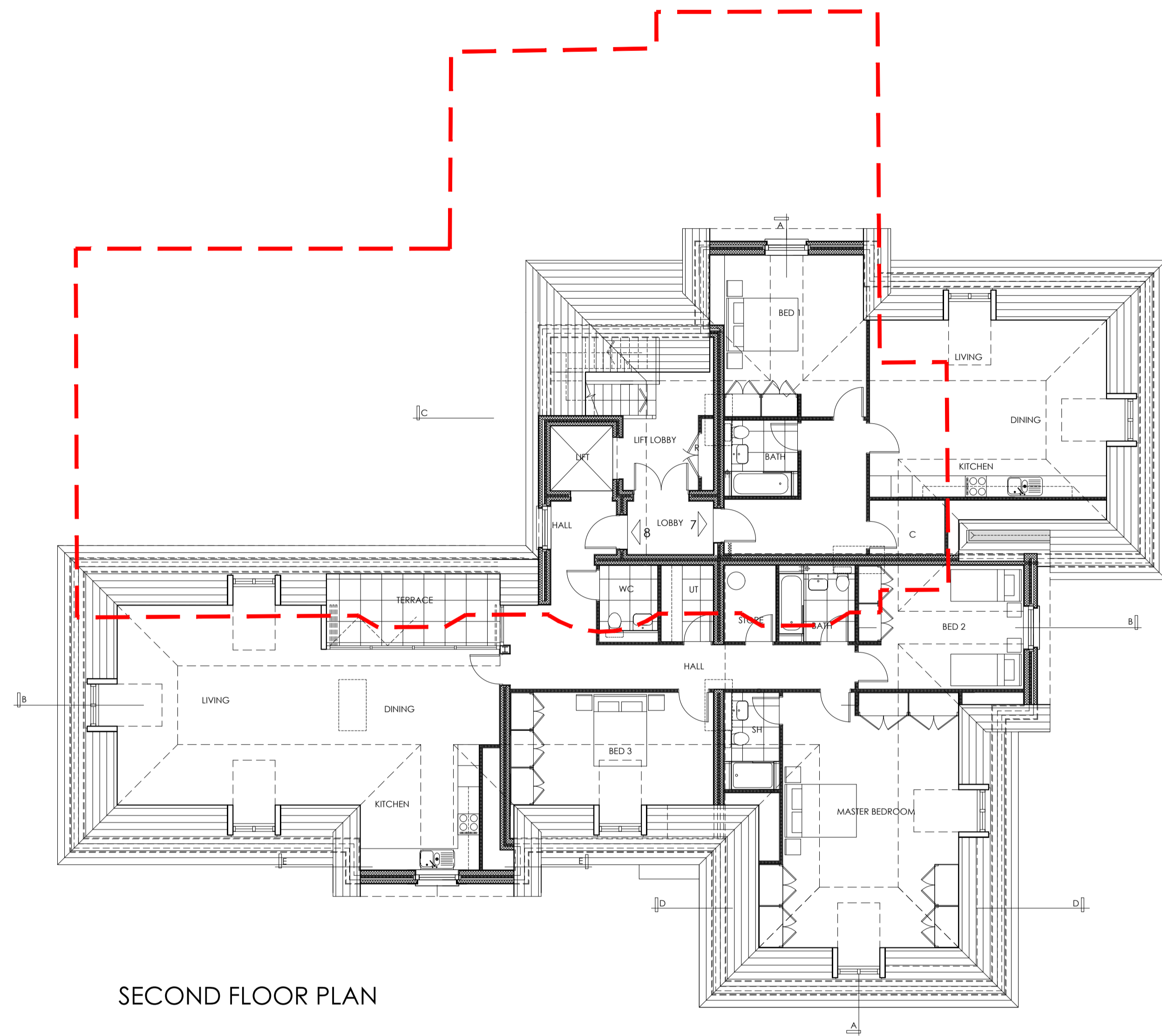
**burgess mean architects**  
 unit 1, the warehouse  
 12 ravenbury terrace  
 london  
 SW18 4RL

all dimensions to be checked on site. any discrepancy between this drawing and other information is to be referred to the partners. this drawing is copyright.

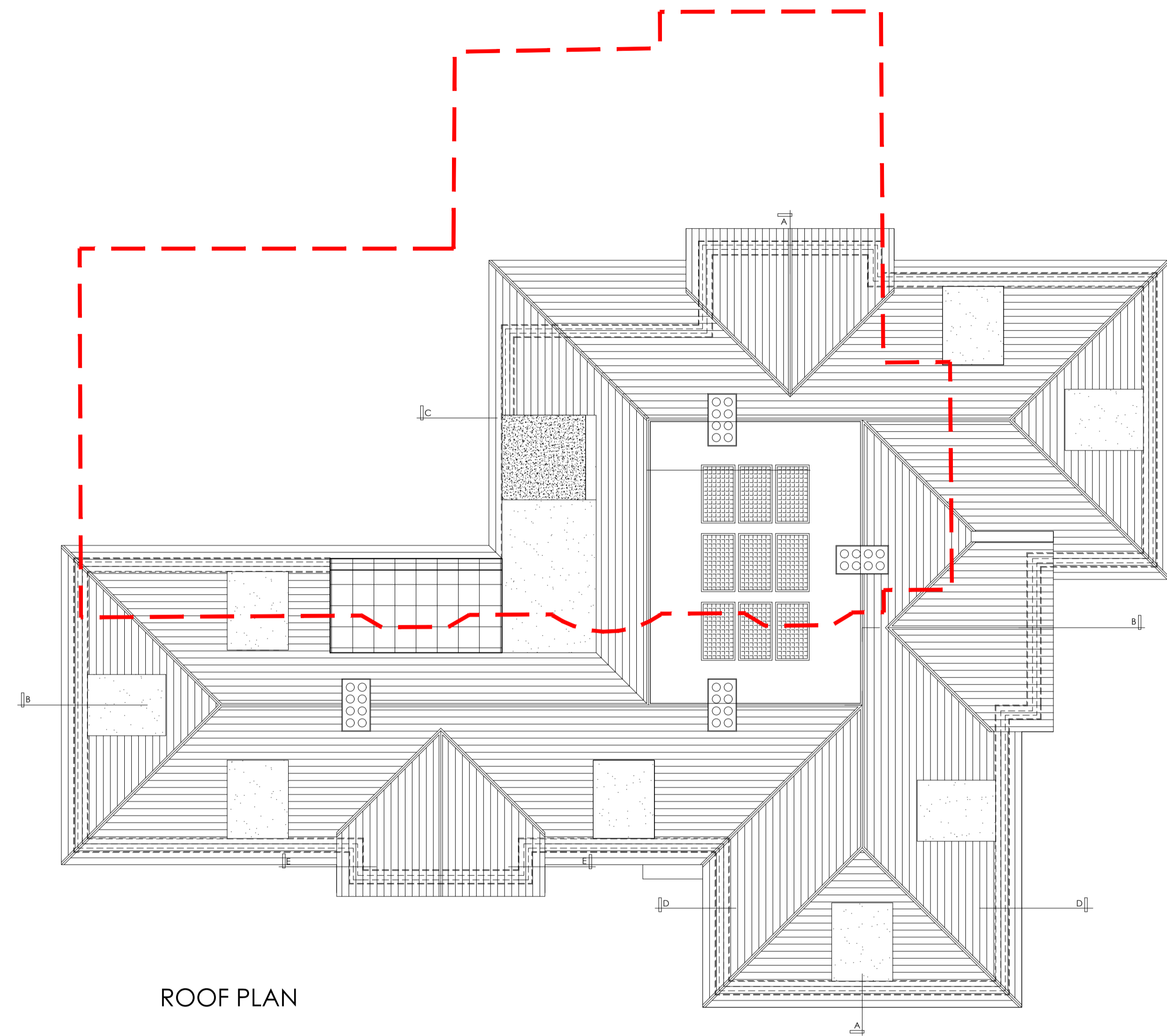
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file name:	(A1)			
checked:				



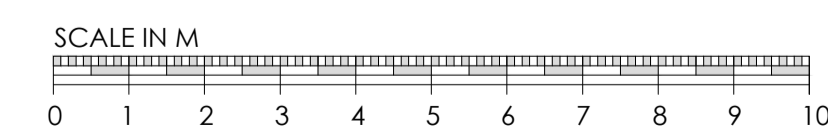
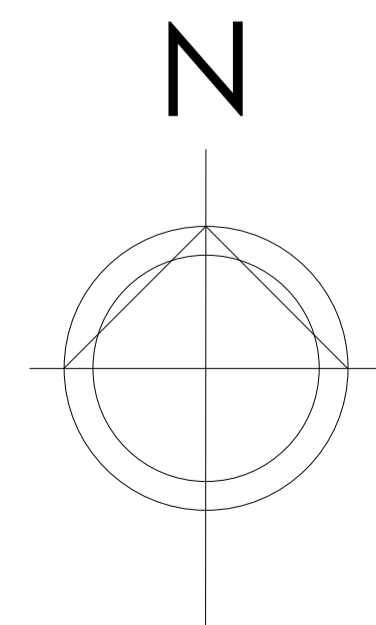


SECOND FLOOR PLAN



ROOF PLAN

--- OUTLINE OF EXISTING BUILDING



### PLANNING DRAWING

Rev D: 16/08/2018: Client name changed and common parts reduced in size to achieve GIA of 997.03 sqm.  
 Rev C: 12/05/2017: Proposed development reduced to 8 N° apartments to suit clients comments.  
 Rev B: 11/04/2016: Amended to suit Conservation Officer comments.  
 Rev A: 12/11/2015: Reduced to nine flat scheme.

title  
 THE CROFT, BUCKLAND ROAD,  
 REIGATE, SURREY RH2 9JP

client  
 HEDDMARA LIMITED

description  
 RESIDENTIAL DEVELOPMENT  
 SECOND FLOOR + ROOF PLANS AS PROPOSED



burgess mean architects  
 unit 1, the warehouse  
 12 ravenbury terrace  
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 SW18 4RL

f: 020 8944 8050 f: 020 8946 9506 e: bma@burgessmean.co.uk


date: APRIL 2014 scale: 1:100 project: 120601 drawing: P/13 revision: D  
 drawn by: SA  
 file name: (A1)  
 checked:



# Agenda Item 11

Planning Committee  
31<sup>st</sup> October 2018

Agenda Item: 11  
DM Performance Q2 2018/19

 <b>Reigate &amp; Banstead</b> BOROUGH COUNCIL Banstead   Horley   Redhill   Reigate	TO:	PLANNING COMMITTEE	
	DATE:	31 October 2018	
	REPORT OF:	HEAD OF PLANNING	
	AUTHORS:	Andrew Benson	
	TELEPHONE:	01737 276175	
	EMAIL:	<a href="mailto:Andrew.benson@reigate-banstead.gov.uk">Andrew.benson@reigate-banstead.gov.uk</a>	
<b>AGENDA ITEM:</b>	11	WARD:	All

<b>SUBJECT:</b>	<b>DEVELOPMENT MANAGEMENT Q2 PERFORMANCE</b>
<b>PURPOSE OF REPORT:</b>	To inform members of the 2018/19 Q2 Development Management performance against a range of indicators
<b>RECOMMENDATION:</b>	<b>To note the performance of Q2 of 2018/19</b>

Planning Committee has authority to note the above recommendation

## BACKGROUND

1. Development Management encompasses a wide range of planning activities including pre-application negotiations and engagement; decision making on planning applications through to compliance and enforcement.
2. It puts the Council's locally adopted development plan policies into action and seeks to achieve sustainable development.
3. It is a non-political, quasi-judicial system with all Development Management functions falling under the responsibility of the Planning Committee in the Council's Constitution. As such it is a non-Executive function falling outside the scope of the quarterly corporate performance reports that are presented to the Executive and Overview and Scrutiny Committee.
4. Development Management performance has always been monitored and reviewed in line with statutory and local targets with quarterly reports sent to the Department for Communities and Local Government. However, given that all functions of the Council as Local Planning Authority fall under the responsibility of the Planning Committee, the performance information has also been shared with the Planning Committee Chairman. This report enables the performance indicators to be noted by the Planning Committee itself.
5. This report is the second quarterly report of the 2017/18 municipal year and provides the quarterly performance at Table 1. Also provided at Table 2 is the requested performance measure, relating to the time taken in total days from receipt of a valid application to its registration.

**PERFORMANCE**

	<b>Performance measure</b>	<b>Target %</b>	<b>Q1 18/19</b>	<b>Q2 18/19</b>
	<b>Applications determined (in 8/13 weeks or agreed ext of time)</b>			
1	Major applications	60%	100%	90%
2	Non-major applications	65%	95%	91%
3	Average days to decision	73	73	82
	<b>Appeals</b>			
4	Appeals Received	-	15	37
5	Major Appeals Decided	-	1	3
6	Major Appeals Dismissed	70%	0%	0%
7	Non-major appeals Decided	-	18	10
8	Non-major appeals Dismissed	70%	44%	60%
	<b>Enforcement</b>			
7	Reported Breaches Received		115	118
8	Cases Closed		111	135
9	On hand at end of period		165	147
10	Cases over 6 months old (no notice)		23	25
11	Priority 1 Enforcement cases investigated within 24 hours	100%	100%	100%
	<b>Application Workload</b>			
12	On hand at beginning		345	350
13	Received		381	309
14	Determined		360	343
15	On hand at end of period		353	305

**Table 1 - Development Management performance**

Apr	May	Jun	Jul	Aug	Sep	Oct	Nov	Dec	Jan	Feb	Mar	Apr	May	Jun	Jul	Aug
16.6	10.8	5.7	5.4	4.9	5.3	7.3	6.5	6.5	7.8	6.0	5.6	8	6.2	5.8	2.3	2.9

**Table 2 – Time taken from receipt to registration (days)**

**Planning applications**

- The Town and Country Planning Development Management Procedure Order 2015 sets the statutory period for the determination of planning applications at 8 weeks for non-major applications and 13 weeks for major applications (10+ dwellings or 1,000+ sqm floorspace). This statutory period is relaxed where an extension of time is agreed between the applicant and local planning authority. In order to monitor the performance of local planning authorities, the Government sets targets for the determination of major and non-major planning applications within the statutory period or agreed extension of time. For major developments, this target is 60% and for non-major developments it is 70%. This Council's local performance target is slightly different at 60% and 65% respectively.
- In this Quarter 90% of major applications were determined within the statutory period or within agreed extension of time and 91% of non-major applications were determined within the statutory period or agreed extension of time. This compares favourably against the Government and local performance targets. This also compares favourably to the national picture where 87% of majors and 84% of minors were determined within the relevant periods in Quarter 1.

8. The average days to decision for Q1 was 82 days, exceeding the target of 73 days. However, this was due to one outlier which skewed the overall figure, without which the average days to decision would have been 69 days, within target. It is not therefore reflective of a wider trend.

### Planning appeals

9. 37 appeals were submitted in the last quarter which is relatively high and likely to be due to the Planning Inspectorate working through some of their backlog of appeals awaiting validation.
10. Alongside the Government performance measure based on speed of determination of planning applications, is the other performance criteria set for local planning authorities aimed at assessing the 'quality' of decision making. This is measured as a percentage of total applications which result in an appeal allowed, broken down between major and non-major development proposals. The relevant target for both types of application is that not more than 10% of applications should be allowed at appeal.  
For example –  
If 100 major applications are determined by the authority over the qualifying two-year period and 9 are allowed at appeal that would result in a figure of 9% which is acceptable. However, if 100 major applications were determined and 11 of these ended up being appealed and the appeals allowed, this would result in a figure of 11% which fails the 10% target.  
  
The assessment is made over a 2-year period, with the current period concluding 31st December 2018.
11. So far within the current period, we have determined 77 major applications, 5 of which have been allowed at appeal. This equates to 6.5% which is within target and it is unlikely that a further 3 major appeals will be allowed to the period to 31<sup>st</sup> December 2018 (when considering the major appeals pending) which would result in the poorly performing categorisation. However, 4 of those 5 major appeals allowed came in the current year (with 3 in the last quarter) and so will roll forward and continue to be counted in the assessment at end December 2019.
12. This does therefore pose a risk of the 10% target of major applications being allowed at appeal being exceeded in next year's performance assessment given it is likely that 3 or 4 major appeals allowed between now and 31<sup>st</sup> December 2019 would trigger the 10% target across the two years being exceeded.
13. If the target across the two-year period is not achieved then legislation gives rise for the designation of the local planning authority as 'poorly performing.' In such circumstances applicants have the potential to bypass the local planning authority for determining their planning applications, instead submitting them directly to the Planning Inspectorate thus taking the potential to determine planning applications within its area out of the local authority's hands.
14. Therefore, whilst planning applications must always be determined as the decision maker sees fit, with regard to policies in the development plan and other material considerations, it is important for all decision makers to understand the potential consequences of their decisions. This is particularly so



for major applications given the relatively low numbers and ability for a small number of allowed appeals to skew the figures and given both the high proportion of these that are appealed and the higher chances of appeals being allowed for such schemes. Designation based on the non-major performance is extremely unlikely given the larger application base to be assessed against, which generally gives a figure of around 5% allowed at any one time with little variation year to year. The focus therefore is on major schemes.

15. This Quarter, 3 major applications were allowed at appeal. They were:

17/00762/F - 130-138 Great Tattenhams, Epsom Downs KT18 5SF  
*Demolition of existing buildings and redevelopment to form 34 one and two bed retirement apartments for the elderly*



140-142 Great Tattenhams Proposed Churchill Retirement Development Footpath 138 Great Tattenhams

CONTEXTUAL ELEVATION A-A (GREAT TATTENHAMS)



ELEVATION A-A

This application was refused at the 26<sup>th</sup> July 2017 Planning Committee for two technical reasons which were overcome during the appeal stage and a further reason on character grounds added by the Committee. In this respect the appeal Inspector commented *“The appeal proposal would be larger than the dwellings that it would replace. It would have a greater site coverage, depth and bulk. However, it would be set back from the street, on a similar line to other*

*dwellings in the locality, behind a landscaped frontage. Due to the slope of the land to the rear of the site, it would have a similar overall height to other buildings in the locality. Whilst it would be wide it would provide some separation from existing dwellings and the bulk of the front element would be broken up by deeply recessed and lower 'link' elements. As a result it would have the appearance of a number of separate elements, such that it would generally accord with the layout and rhythm of dwellings in the street."*

17/00539/F - Nutley Dean Business Park, Small Hill Road, Horley RH6 0HR

*Removal of industrial buildings and the erection of 10 dwellings*



This was a delegated refusal on grounds that the proposed development would be inappropriate in the green belt and unsustainably located.

The Inspector, in allowing the appeal, agreed that the height of proposed dwellings would exceed current buildings on site but that the overall site coverage and volume of buildings would be reduced. He therefore considered there would be no overall harm to openness and therefore appropriate redevelopment of previously developed land.

With regards the site's location, the Inspector agreed that the proposal would not be sustainably located and so conflict with Policy CS17 of the Core Strategy but considered overall, the proposed residential development would result in fewer trips than the lawful industrial use.

17/00870/F - Cornerways, Smugglers, Mountfield & 266 Chipstead Way, Outwood Lane, Chipstead, CR5 3NH

*Demolition of existing buildings and redevelopment to form 25 retirement living apartments for older persons*



This was another delegated refusal, primarily on character grounds but also noise impact to neighbours associated with the steep access drive to the side of the development.

The appeal Inspector disagreed with the Council’s concerns regarding the building’s depth, appearing cramped, prominent and out of keeping with the domestic character. Instead it was concluded *“Rather than causing harm to the character and appearance of the local area it would have a beneficial effect in townscape terms by more clearly marking an important corner site and providing better definition to the road junction and the approaches to it.”*

With regards the noise issue the appellants (McCarthy & Stone) produced technical evidence to demonstrate that there would be limited harm to the neighbouring properties. Whilst such evidence is often disregarded by Inspectors in favour of a qualitative assessment, in this case that was given considerable weight leading to the conclusion that there would be no harmful noise impact.

- 16 The other appeal decision of note, relates to a non-major decision but one which was determined by the Planning Committee. It was:

17/01061/F - Mount Pleasant, Coppice Lane, Reigate RH2 9JF

*Demolition of existing residential dwelling (Use Class C3) and erection of replacement buildings comprising 6 no. 182 flats and 1 no. 5 bedroom dwelling house*





The main issue was whether the proposed development of 6 flats and 1 house would be more harmful to the openness of the green belt compared to the current buildings on site.

The Inspector considered the impacts of both the built form and the intensity of use associated with the flats but concluded this would not be materially more harmful to openness of the green belt than the lawful use of the building so found the proposal to be acceptable.

### Planning Enforcement

- 17 The enforcement performance statistics for Quarter 2 show a similar number of cases received but with higher number of cases closed than the previous quarter so bringing down the number of open cases. The number of cases over six months old is also comparable with previously so being kept well under control. Last quarter was the first to consider the number of Priority 1 cases investigated within 24 hours and this remains at 100%.

### Registration/Other

- 18 Table 2 shows the continued efficiency of the registration team, with applications taking on average less than 3 days from receipt before they are registered. The team has recently lost two members of staff and so efforts will be made including recruitment to ensure that performance is maintained despite this.

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